

# BRUCE VENTO REGIONAL TRAIL PHASE 2 PRELIMINARY DESIGN

RAMSEY COUNTY, MN



OCTOBER 6, 2022

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# ACKNOWLEDGMENTS

The Ramsey County Parks & Recreation Department gratefully acknowledges the team who contributed to this report. Reviewing agencies included the City of White Bear Lake, White Bear Township, ISD 624, Rice Creek Watershed District, Vadnais Lake Area Water Management Organization, and staff from Ramsey County Parks & Recreation and Public Works Departments.



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# TABLE OF CONTENTS

<b>Introduction .....</b>	<b>7</b>
<b>Traffic Study .....</b>	<b>9</b>
<b>Bald Eagle Boulevard Cross Section Analysis .....</b>	<b>11</b>
<b>Highway 96 Crossing Alternatives .....</b>	<b>15</b>
<b>Drainage Study .....</b>	<b>17</b>
<b>Plans and Sections .....</b>	<b>18</b>
<b>Cost Estimates .....</b>	<b>61</b>
<b>Engagement Summary .....</b>	<b>66</b>
<b>Next Steps .....</b>	<b>76</b>
<b>Appendix .....</b>	<b>77</b>

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# INTRODUCTION

## Project Goal

Provide Design Development Documents for the integration of a regional trail extension within an urban corridor of varied road types utilizing the preferred trail route from Whitaker Street to County Road J to remove significant barriers for northern communities in Ramsey County in addition to completing a major gap in the National US Bike Route 41 (USBR 41).

## Project Objectives

Basis for final design and construction documents and future grant requests.

- To facilitate public involvement with meaningful engagement to obtain feedback representative of the population living within the project corridor for regional trail improvements.
- Provide high-quality community spaces that promote an enriched culture for all people, of all ages, and of all abilities.
- To provide a regional trail facility, amenities, and site components that complement the natural resource base and incorporate sustainability and green infrastructure for long-term resiliency.
- Provide trail connections to Rush Line Bus Rapid Transit (BRT) and downtown White Bear Lake.

## Project Precedent and Background

The Bruce Vento Regional Trail is 13.3 miles in length and extends from the east side of downtown Saint Paul northwestward to the north County line in White Bear Township. The trail is located on the former right of way of the Burlington Northern Santa Fe (BNSF) Railroad. The trail passes through the cities of Saint Paul, Maplewood, Vadnais Heights, Gem Lake, White Bear Lake, and White Bear Township. Although the designated trail corridor extends the entire length of the BNSF Railroad right of way, only the southern 7 miles have been developed for public use between the east side of downtown Saint Paul to Buerkle Road in White Bear Lake.

A major planning effort was initiated in 2014 to determine an alternative three-mile trail alignment out of the railway right-of-way from Buerkle Road to Highway 96 in White Bear Lake for reducing the remaining gap for the Bruce Vento Regional Trail. In efforts to extend the regional trail from Buerkle Road to County Road J, the Parks department initiated three regional trail projects in 2018:

## Buerkle Road to Intersection of Hoffman Road/Highway 61 Trail Extension

This project will complete the final construction plans for the Phase 1 section between Buerkle Road to the intersection of Hoffman Boulevard. A 2020 regional solicitation application was submitted in 2020 for federal funds; however, the Parks department was not successful in receiving funds. Another regional solicitation application was submitted in 2022 for future federal funds.

## Highway 96 to County Road J Trail Feasibility Study

This project assessed the downtown of White Bear Lake and portions of White Bear Townships for an alternate regional trail corridor alignment. The evaluation analyzed design intent, usability, and effectualness for both bikers and walkers, impacts, connections to downtown, and trail user experience. At the completion of the feasibility study the finding/outcomes of the study were incorporated in to the Bruce Vento Regional Trail Master Plan Amendment.

## Bruce Vento Regional Trail Master Plan amendment – Larpenteur Avenue to County Road J

The master plan amendment includes the regional trail corridor from Larpenteur Avenue to County Road J. This document addresses several changes and projects planned throughout the corridor including amenity improvements, trailhead development, and enhanced access to the Rush Line Bus Rapid Transit (BRT) improvements. This master plan amendment was approved in 2021.

## Preferred Route for Phase 2 of the Bruce Vento Regional Trail

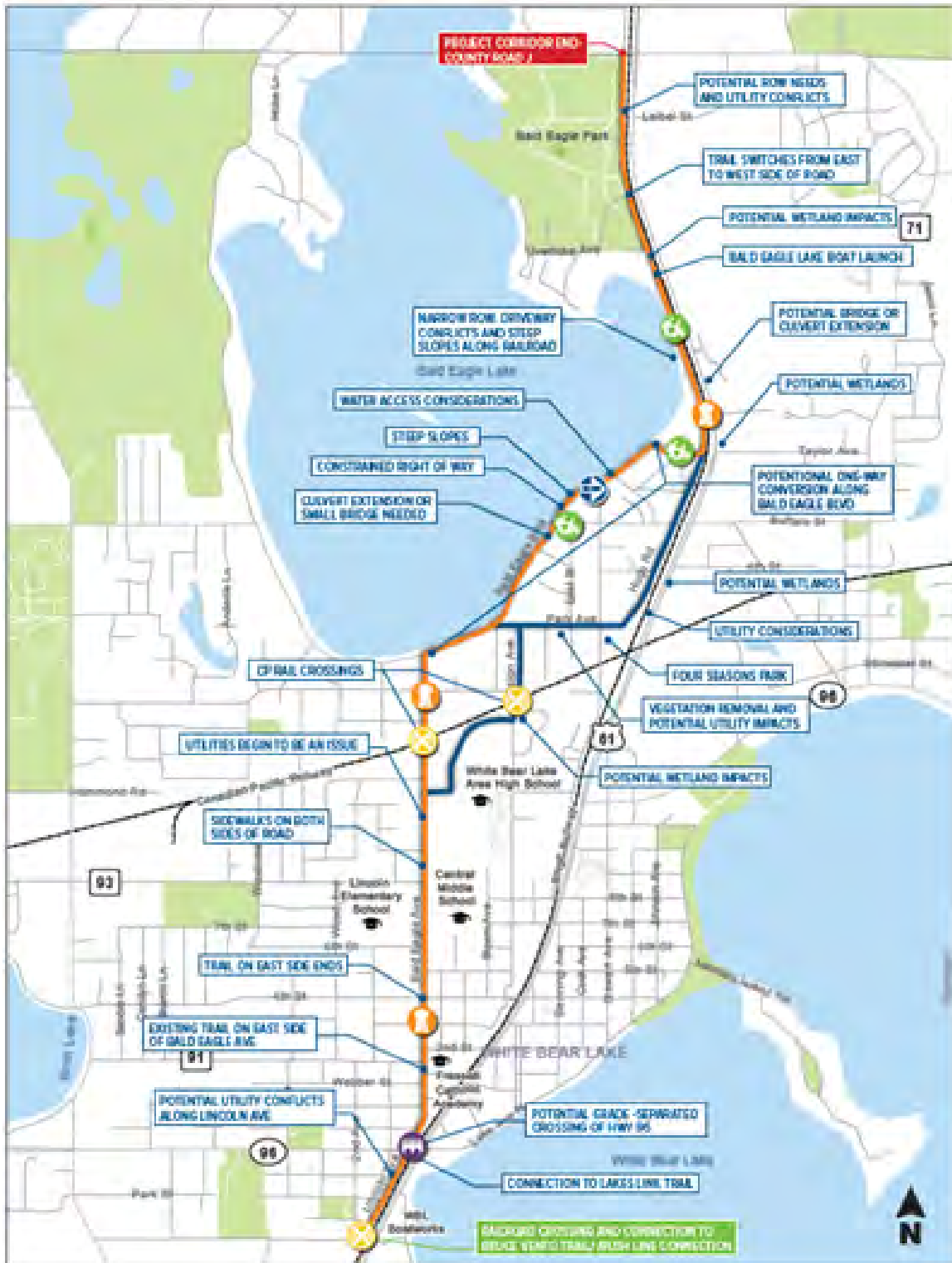
The preferred route shown in Figure 1 became the starting point for this study which initiates preliminary engineering to better define the trail design, impacts, right-of-way needs, and costs.

The evaluation of multiple alternatives for the location of the trail within the preferred corridor identified in the original Feasibility Study and Master plan amendment will provide the County with more detailed information on the cost and impacts of trail construction in the preferred corridor. The Preliminary Design study is also helpful in efforts to secure State and Federal grants for further design and construction.

Additional trail design will affirm the location of the trail and associated infrastructure and will better define

approximate grading limits, vegetation impacts and needs for any additional right-of-way. The study also provides more detailed cost estimates for the trail and associated infrastructure implementation.

Please refer to Appendix L for a general project timeline and additional information regarding the Feasibility Study and Master Plan Amendment.



**FIGURE 1: Bruce Vento Trail Phase 2 Corridor**



## TRAFFIC STUDY

A traffic evaluation of the area was completed to determine the traffic impacts of constructing an extension of the Bruce Vento Trail on the preferred alignment including Bald Eagle Avenue, Bald Eagle Boulevard, Taylor Avenue, and Hugo Road in White Bear Township along the southeast shore of Bald Eagle Lake. The high-level traffic study was conducted to evaluate the impacts of the various trail designs on daily traffic volumes and services to the surrounding neighborhood. The report also considers information from the White Bear Lake High School traffic study in developing these evaluations.

Multiple roadway designs for Bald Eagle Boulevard were considered for adding the bike trail extension. The designs include:

- Keeping both lanes of vehicle traffic on Bald Eagle Boulevard and using right-of-way to add a trail on either the east or west side of the roadways.
- Closing the northbound/eastbound vehicle lane of Bald Eagle Boulevard and adding the trail on either the east or west side of the roadways.
- Closing the southbound/westbound vehicle lane of Bald Eagle Boulevard and adding a bike lane on either the east or west side of the roadways.



**FIGURE 2: Existing Traffic Conditions - Two-way traffic on Bald Eagle Boulevard, Taylor Avenue, and Hugo Road (numbers indicate Average Daily Automobile Traffic)**

Year 2016 and 2019 daily traffic volumes were provided by the Minnesota Department of Transportation (MnDOT) for roadways within the study area and are shown in Figure 2. Various impacts on driver behavior, civil services, and/or private properties are associated with each design.

Partially or wholly converting Bald Eagle Boulevard and Taylor Avenue to one-way will have a limited impact on traffic in the area. The forecasted impacts on the vehicle traffic in a one-way northbound scenario are shown in Figure 3. As shown, the bulk of the traffic is expected to divert to Park Avenue and Hugo Road. It is not expected

that there will be any significant traffic operational impacts from the lane closure as daily volumes are expected to be well below the daily thresholds for two-lane roadways.

Recommend that one-way vehicular traffic in a north/east direction along Bald Eagle Boulevard and Taylor Avenue be considered as this configuration would lessen the need to relocate mailboxes and trash pickup along Bald Eagle Boulevard and Taylor Avenue to comply with the postal and waste service vehicles.

The full Traffic Memorandum is included in Appendix A.



**FIGURE 3: Proposed Traffic Conditions - One-way traffic north/east-bound on Bald Eagle Boulevard east of Park Avenue and Taylor Avenue and Two-way traffic on Hugo Road and Bald Eagle Avenue west of Park Avenue. (numbers indicate Average Daily Automobile Traffic)**



## BALD EAGLE BOULEVARD CROSS SECTION ANALYSIS

The design team looked at a variety of combinations of cross-section design scenarios along Bald Eagle Boulevard to determine the preferred roadway and trail section for further development of plan alignment and profile. The team considered several factors in developing the Bald Eagle Boulevard sections including the following:

- Hold lake-side curb and adjust house-side curb
- Hold house-side curb and adjust lake-side curb
- One-way roadway
- Two-way roadway
- Minimum roadway/trail section meeting minimum State Aid Standards
- Ideal roadway/trail section matching previous Bruce Vento Trail segment designs

Combining these factors, the team developed eight representative sections to evaluate further.

These cross-section design scenarios along Bald Eagle Boulevard include the following:

- Two-way ideal, trail right/house side, hold lake edge of pavement
- Two-way ideal, trail left lake side, hold house edge of pavement
- Two-way minimum, trail right/house side, hold lake edge of pavement
- Two-way minimum, trail left/house side, hold house edge of pavement
- One-way ideal, trail right/house side, hold lake edge of pavement
- One-way ideal, trail left/lake side, hold house edge of pavement
- One-way minimum, trail right/house side, hold lake edge of pavement
- One-way minimum, trail left/lake side, hold house edge of pavement

Representative sections and design alternatives are shown in Figures 4, 5 and 6 and Table 1 (next page).



**FIGURE 4: One-way roadway with trail on right.**



**FIGURE 5: Two-way roadway with trail on right.**



**FIGURE 6: One-way roadway with trail on left.**

The cross-section evaluation looked at factors such as grading and constructibility, right-of-way impacts, need for retaining walls, and need for permanent or temporary easements. It also looked at the number of driveway, road, and walk crossings. A summary of this evaluation is shown in Figure 7. The full evaluation matrix is included in Appendix B.

Conclusions derived from the analysis suggest the following:

- Bald Eagle Boulevard between Bald Eagle Avenue and Park Avenue has the most flexibility for alternative typical sections.
- Bald Eagle Boulevard north of Park Avenue is not necessarily limited to a one-way roadway configuration and works best with the trail on the lake side.
- Two-way alternative for Bald Eagle Boulevard north of Park Avenue would have a significant impact on adjacent properties including potential for additional grading, tree removal, additional retaining walls and property acquisitions.
- Small retaining walls, within the road right of way, along Bald Eagle Boulevard may be needed in a one-way configuration to minimize grading.
- For the one-way alternatives temporary grading/

construction easements may be needed on the lake side to accommodate lake access. However, most grading should occur within the road right of way.

From the public engagement process, the team noted that avoiding permanent easements and additional right-of-way acquisition could likely be achieved with a one-way alternative on Bald Eagle Boulevard North of Park Avenue.

Further input from the public suggested an interest in Taylor Avenue also being a one-way configuration from Bald Eagle Boulevard to Hugo Road.

The suggested design scenario for Bald Eagle Boulevard was identified as a two-way roadway to Park Avenue and then one-way to the north and along Taylor Avenue to Hugo Road.

The study also looked at the potential for a two-way alternative for Bald Eagle Boulevard east of Park Avenue. This alternative resulted in significant impacts to vegetation, existing retaining walls and driveways/other points of property access. In addition there is an existing bridge that would need to be replaced to accommodate the trail and likely need for substantial property acquisitions.

Other sections of the proposed trail corridor (Lincoln Avenue, Bald Eagle Avenue and Hugo Road) required less detailed evaluation and analysis to determine the preferred trail location. These segments are described in the following paragraphs.

**TABLE 1: Representative Road/Trail Sections**

<b>TWO-WAY IDEAL</b>	<b>TWO-WAY MINIMUM</b>
6-foot berm	6-foot berm
B618 curb (face at existing pavement edge)	B618 curb (face at existing pavement edge)
2 x 10-foot lanes	2 x 10-foot lanes
B618 curb	B618 curb
6-foot turf boulevard (from curb face)	3-foot paved boulevard (from curb face)
10–12-foot trail	8-foot trail + 2-foot paved clear zone
2-foot clear	2-foot clear
<b>ONE - WAY IDEAL</b>	<b>ONE - WAY MINIMUM</b>
6-foot berm	6-foot berm
B618 curb (face at existing pavement edge)	B618 curb (face at existing pavement edge)
10-foot lane	10-foot lane
B618 curb	B618 curb
6-foot turf boulevard (from curb face)	3-foot paved boulevard (from curb face)
10–12-foot trail	8-foot trail+ 2-foot paved clear zone
2-foot clear	2-foot clear

Bruce Vento Phase 2  
 Bald Eagle Boulevard - Bald Eagle Avenue to Hugo Road  
 Alternative Cross Sections Analysis  
 #14110  
 By SBP 6/27/22

**Alternatives**

Typical Section 1- TS1 2 way ideal road, Lake side trail, Hold house side edge of pavement  
 Typical Section 2- TS2 2 way ideal road, House side trail, Hold lake side edge of pavement  
 Typical Section 3- TS3 2 way minimum road, Lake side trail, Hold house side edge of pavement  
 Typical Section 4- TS4 2 way minimum road, House side trail, Hold lake side edge of pavement  
 Typical Section 5- TS5 1 way ideal road, House side trail, Hold lake side edge of pavement  
 Typical Section 6- TS6 1 way minimum road, House side trail, Hold lake side edge of pavement  
 Typical Section 7- TS7 1 way minimum road, Lake side trail, Hold house side edge of pavement

**Blocks**

B1	Bald Eagle Avenue to Park Avenue	189+00	200+00
B2	Park Avenue to Short Street	201+00	213+00
B3	Short Street to Buffalo Street	214+00	219+00
B4	Buffalo Street to Eagle Street	220+00	226+00
B5	Eagle Street to Williams Avenue	227+00	229+00
B6	Williams Avenue to Shadyside Lane	230+00	238+00
B7	Shadyside Lane to Hoxie Avenue	239+00	241+00
B8	Hoxie Avenue to Hugo Road	242+00	244+00

**Questions**

Feasible? Constructable as drawn? Does grading work?  
 Right of Way? Is trail (with CZ) contained within existing ROW?  
 Wall? Could a wall make trail feasible?  
 Permanent Easement (PE)? Would PE be required to build XS?  
 Temporary Easement (TE)? Would TE be required to build XS?

Y = reasonable cross section	N = excessive grading (into lake or house)
Y = Trail in ROW	N = Extends beyond Ex ROW
Y = Makes trail feasible	N = wall not necessary
Y = PE required for trail + CZ	N = PE not required
Y = TE required for grading	N = TE not required

Station	TS1 - TWO WAY IDEAL, LAKE SIDE					TS2 - TWO WAY IDEAL, HOUSE SIDE					TS3 - TWO WAY MIN, LAKE SIDE					TS4 - TWO WAY MIN, HOUSE SIDE					TS5 - ONE WAY IDEAL, HOUSE SIDE					TS6 - ONE WAY MIN, HOUSE SIDE					TS7 - ONE WAY MIN, LAKE SIDE				
	Feas?	R/W?	Wall?	PE?	TE?	Feas?	R/W?	Wall?	PE?	TE?	Feas?	R/W?	Wall?	PE?	TE?	Feas?	R/W?	Wall?	PE?	TE?	Feas?	R/W?	Wall?	PE?	TE?	Feas?	R/W?	Wall?	PE?	TE?	Feas?	R/W?	Wall?	PE?	TE?
B1	N	N	Y	Y	Y	Y	Y	N	Y	Y	N	N	Y	Y	Y	Y	Y	N	N	N	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
B2	N	N	Y	Y	Y	Y	N	N	Y	Y	N	Y	Y	Y	Y	Y	N	N	Y	Y	Y	Y	N	N	Y	Y	Y	N	N	N	Y	Y	N	N	Y
B3	N	N	Y	Y	Y	Y	N	N	Y	Y	N	Y	Y	Y	Y	Y	Y	N	Y	Y	Y	Y	N	Y	Y	Y	Y	N	Y	Y	Y	Y	N	N	N
B4	N	N	Y	Y	Y	Y	N	N	Y	Y	N	Y	Y	N	N	Y	Y	N	N	N	Y	Y	N	N	N	Y	Y	N	N	N	Y	Y	N	N	N
B5	N	N	Y	Y	Y	Y	Y	N	Y	Y	N	Y	Y	N	N	Y	Y	N	N	N	Y	Y	N	N	N	Y	Y	N	N	N	N	Y	Y	N	N
B6	N	Y	Y	Y	Y	Y	N	N	Y	Y	N	Y	Y	N	N	Y	Y	N	N	Y	Y	Y	N	N	N	Y	Y	N	N	N	N	Y	Y	N	N
B7	Y	N	N	Y	Y	Y	N	N	Y	Y	Y	Y	N	N	N	Y	Y	N	N	N	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
B8	Y	N	N	Y	Y	Y	N	N	Y	Y	Y	Y	N	N	N	Y	N	N	Y	Y	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A

**Summary**

- TS1 Walls required majority of segment, difficult to meld this with existing lake access, however no impacts on the house side
- TS2 Eliminates most walls from TS1, permanent and temporary easements still required, no impacts on the lake side
- TS3 Only a slight improvement from TS1
- TS4 A slight improvement from TS2, easements still required, benefits don't outweigh loss of grassy boulevard
- TS5 Better fit in ROW than TS1, temporary easements still required, implements one way and jurisdictional transfer
- TS6 No improvement from TS5, temporary easements still required, no benefits for loss of grassy boulevard
- TS7 Some improvement from TS1 and TS3 but same issues largely remain, only limited temporary easements required

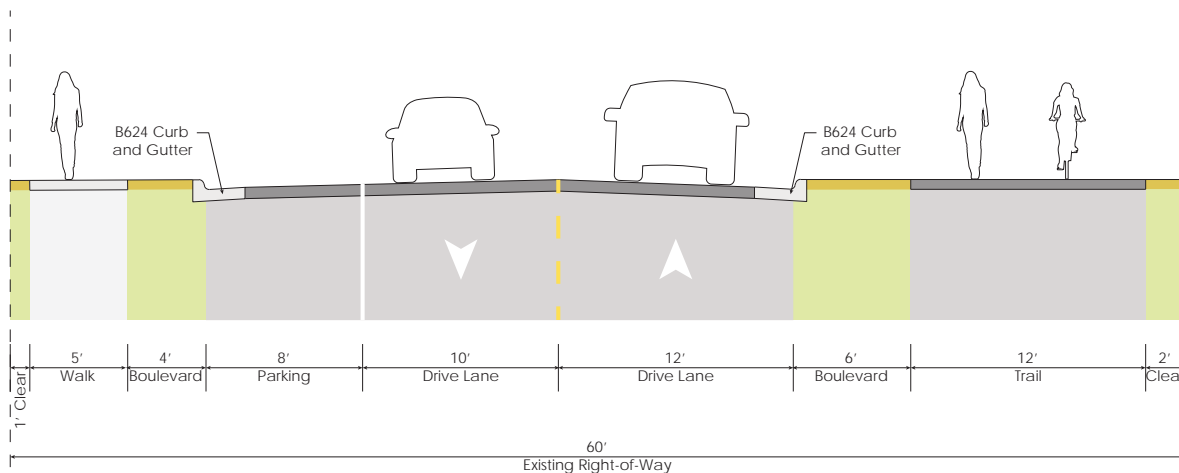
FIGURE 7: Bald Eagle Boulevard Alternatives Analysis

For the segment along Lincoln Avenue (Figure 8), the trail is proposed to be constructed along the east side of the road between the roadway and the adjacent railroad right of way. This will require reconstruction of Lincoln Avenue to narrow/shift the roadway east and is touched on in the Highway 96 crossing discussion in the next chapter. Further evaluation is also needed for the proposed cul-de-sac at the north end of Lincoln Avenue to avoid property impacts and ensure adequate public safety access.

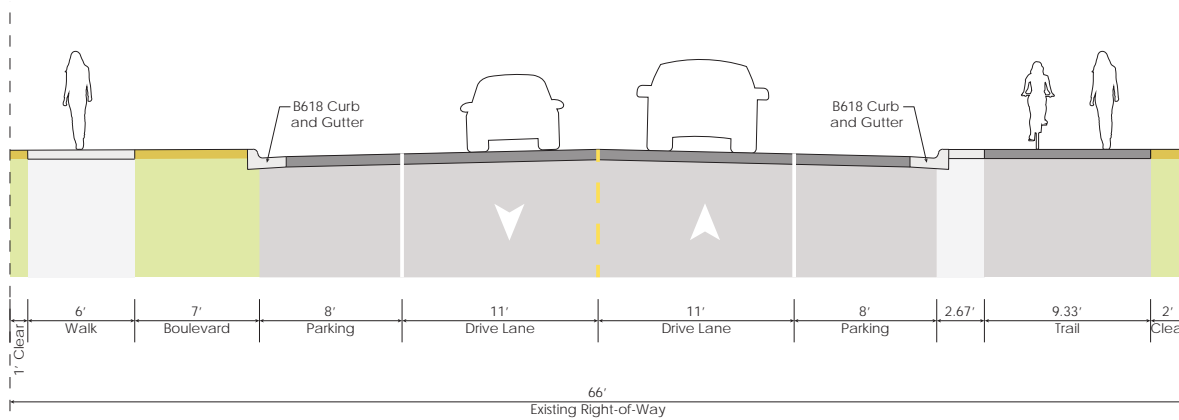
Along most of Bald Eagle Avenue (Figure 9), there is an existing trail/sidewalk on the east side of the roadway. This facility will be upgraded to provide a consistent trail experience. At the north end of Bald Eagle Avenue where there is not an existing sidewalk/trail. The trail will continue

to be located on the east side of the roadway. The east shoulder of the roadway will be removed, and a new curb placed at the edge of the existing travel lane to provide more space within the existing right of way for the trail and to avoid trees and landscaping impacts.

Based on community input and a challenging right-of-way situation both one-way and two-way alternatives were evaluated. Bald Eagle Boulevard (Bald Eagle Avenue to Park Avenue) maintains two-way with the trail on the lake side. From Park Avenue to Taylor Avenue, the scenario is a one-way with the trail on the lake side. Taylor Avenue is one-way with trail on north side. Hugo Road in both design scenarios has the trail located on the east side between Hugo Road and the railroad tracks.



**FIGURE 8: Lincoln Avenue Proposed Section.**



**FIGURE 9: Bald Eagle Avenue, Highway 96 to White Bear Lake High School Proposed Section.**

## HIGHWAY 96 CROSSING ALTERNATIVES

Most of the roadway crossings in the corridor are minor (low volume, low speed, and two lanes). However, the crossing of Highway 96 is a bit more challenging due to high speed, high volume, and multiple lanes. For this reason, additional evaluation of this crossing was provided. The team looked at three alternatives:

- Underpass
- Pedestrian Bridge
- At-grade Crossing

The underpass alternative is challenged by a high-water table and the proximity of the BNSF Railroad corridor and was quickly dismissed.

Several alternatives were developed for a pedestrian bridge over Highway 96.

Another alternative was looked at for an at-grade crossing of Highway 96 at the intersection with Highway 61.

The pedestrian bridges do provide a safe alternative for crossing Highway 96. However, they also are very expensive and have the following specific constraints:

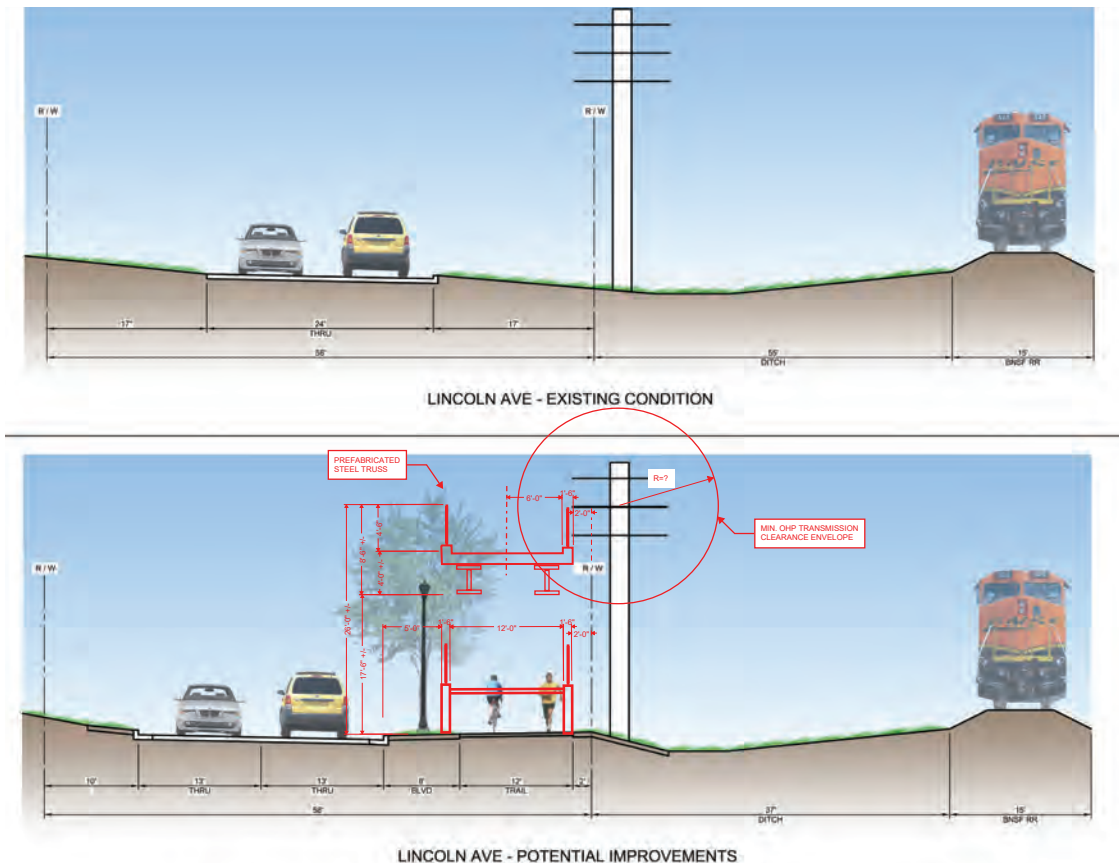
- Creates circuitous route for access to the bridge from existing trails and sidewalks along Highway 96
- Conflict with existing overhead utilities (see Figure 10)
- Would require the acquisition of business property on the south side of Highway 96

Layouts of the pedestrian bridge alternatives are included in Appendix C.

An at-grade crossing can be made relatively safe and is generally much less expensive than a pedestrian bridge. The specific crossing here presents the following opportunities.

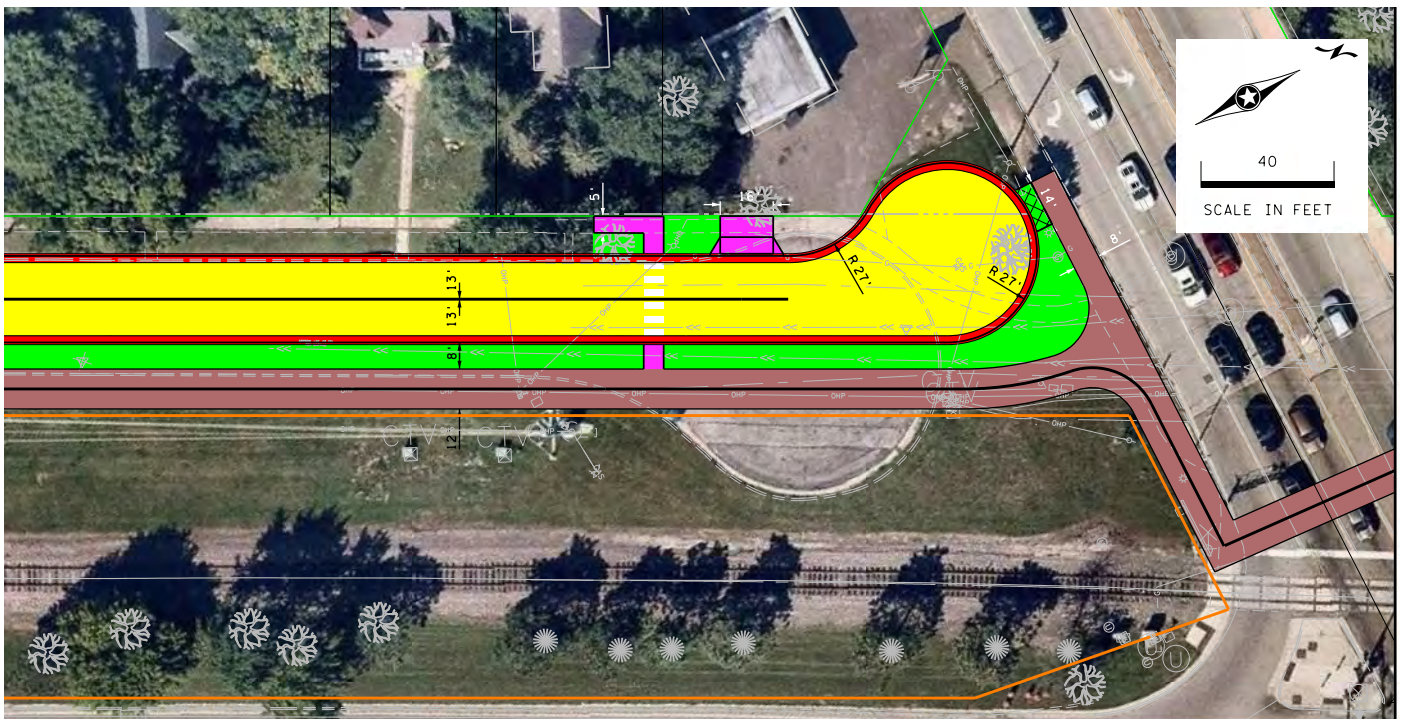
- Makes use of an existing traffic signal
- MnDOT initiating intersection improvements
- Minimal conflicts with utilities

For these reasons, it was determined to go forward with an at-grade crossing approach until trail use warrants the construction of a pedestrian bridge. While the at-grade alternative is proposed at this time, additional evaluation is needed to minimize impacts to adjacent properties and facilitate adequate public safety access (see Figures 11 and 12 next page).

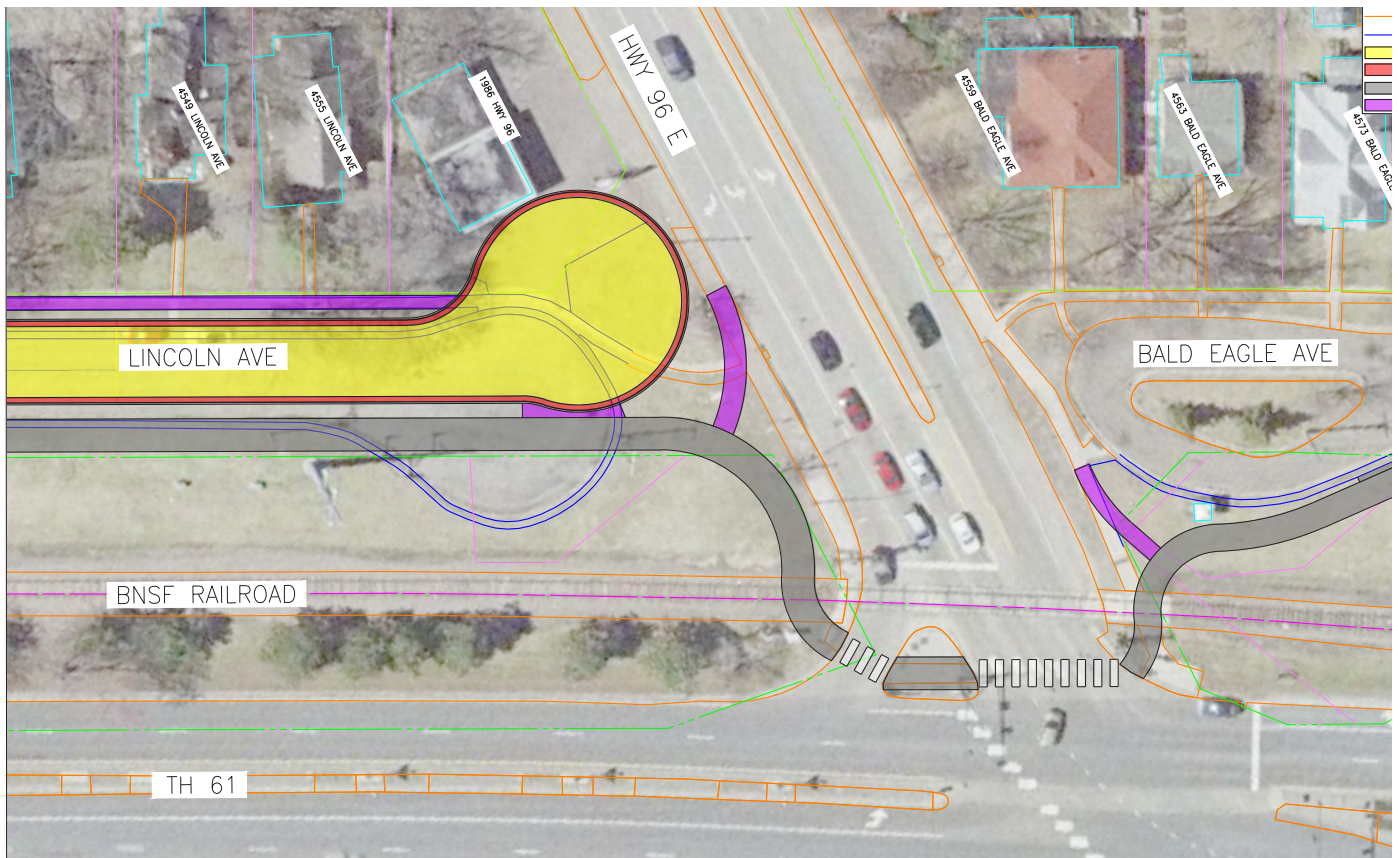


**FIGURE 10: Representative Utility Conflicts with Pedestrian Bridge at Lincoln Avenue**





**FIGURE 11: Concept Trail and Roadway Configuration at Lincoln Avenue from Master Plan Amendment**



**FIGURE 12: Proposed Highway 96 At-Grade Crossing**



## DRAINAGE STUDY

The Bruce Vento Regional Trail Phase 2 project is located in two watershed organizations (Rice Creek Watershed District and Vadnais Lake Area Watershed Management Organization). This makes addressing stormwater requirements somewhat challenging as requirements between jurisdictions often differ.

The design team looked at the requirements of all jurisdictions to determine the most restrictive requirements.

To address Permitting requirements of various agencies the study looked at the following and identified potential locations for mitigation facilities:

- Rate Control
- Volume Control
- Water Quality
- Groundwater
- Floodplain impacts
- Wetland impacts

In the VLAMO jurisdiction the plan identifies the following potential locations:

- Basin South of Lincoln Avenue
- Basin just north of Highway 96
- Basin near White Bear Lake High School
- Basin near Canadian Pacific Railroad Tracks

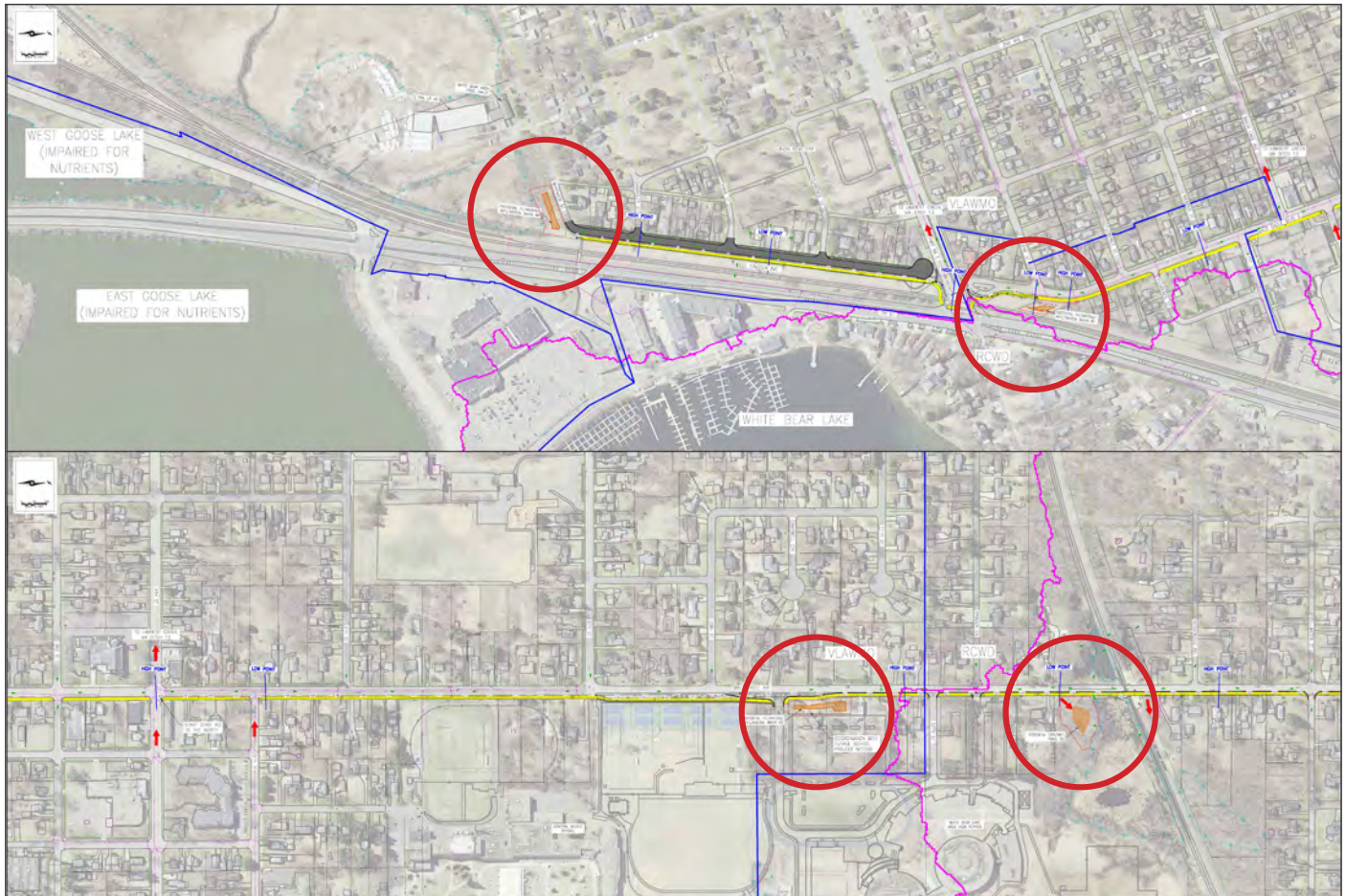
The locations are indicated in red circles in Figure 13.

In the RCWD jurisdiction the plan identifies the following potential locations:

- Basin near White Bear Lake High School
- Basin near Canadian Pacific Railroad Tracks
- No BMPs proposed along Bald Eagle Blvd segment
- Potential for structural pretreatment at existing discharge points
- Two options for basins at Bald Eagle Boat Launch

These locations are indicated by the circles and squares in Figure 14.

The full drainage study is attached as Appendix D.



**FIGURE 13: Suggested Best Management Plan (BMP) locations in the Vadnais Lake Area Water Management Organization (VLAWMO)**



**FIGURE 14: Suggested Best Management Practices (BMP) Locations in the Rice Creek Watershed District.**

## PLANS AND SECTIONS

Based on the information and analysis presented previously, the design team developed several alternate layouts. All the designs are the same for each alternate until they reach Hugo Road and include:

- Trail on the east side of Lincoln Avenue and Bald Eagle Avenue.
- Two-way roadway and trail along the lake side of Bald Eagle Boulevard to Park Avenue.
- One-way roadway and trail along the lake side of Bald Eagle Boulevard from Park Avenue to Taylor Avenue.
- One-way roadway and trail along the north side of Taylor Avenue from Bald Eagle Boulevard to Hugo Road.

In addition, a two-way roadway along the entire segment was also analyzed, but would have greatly impacted the corridor.

The alternates differ in their approach to Hugo Road.

Alternate A – Two-way roadway for Hugo Road; Sidewalk on the east side; No parking.

Alternate B – Two-way roadway for Hugo Road; Trail on the east side; No parking.

Alternate C – One-way roadway for Hugo Road; Trail on the east side; Parking bay.

Alternate D – Two-way roadway for Hugo Road; Trail on the east side, No parking; railroad impacts.

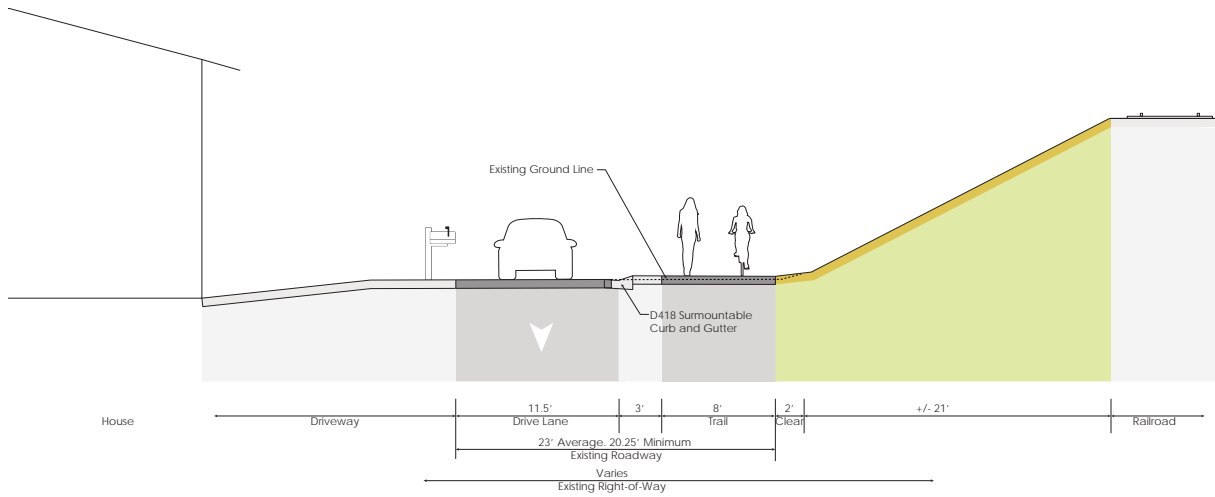
Plans and cross-sections were prepared for Alternates C and D which suggest a one-way and two-way section at the pinch point along Hugo Road.

In addition, the study also looked at the potential costs and impact of a two-way trail from Park Avenue to Hugo Road.

Plan sheets are included in the following pages. Cross-sections are included in Appendix E.

## Preferred Alignment with One-Way Roadway on Hugo Road at Pinch Point

The following 35 pages include concept plans for trail concept with a one-way roadway on Hugo Road and a one-way roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue. Figure 15 shows a conceptual section of the proposed trail placement along Hugo Road at the pinch point location.



**FIGURE 15: One-Way Roadway on Hugo Road at Pinch Point Section.**



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



**LEGEND**

- EXISTING RIGHT OF WAY
- EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
- PARCEL BOUNDARY
- BUILDING OUTLINE
- EXISTING TOPOGRAPHY (LIDAR)
- EXISTING TOPOGRAPHY (SURVEYED)
- PROPOSED ROAD RECONSTRUCTION
- PROPOSED CURB AND/OR GUTTER
- PROPOSED BITUMINOUS TRAIL
- PROPOSED CONCRETE SIDEWALK

MATCHLINE STA. 105+60  
SEE SHEET NO. 2

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NO	DATE	BY	CKD	APPR

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
 Print Name: \_\_\_\_\_  
 Date \_\_\_\_\_ License # \_\_\_\_\_

STATE AID PROJECT NO.	DRAWN BY
STATE PROJECT NO.	DESIGNED BY
COUNTY PROJECT NO.	CHECKED BY
CITY PROJECT NO.	COMM. NO. 14110



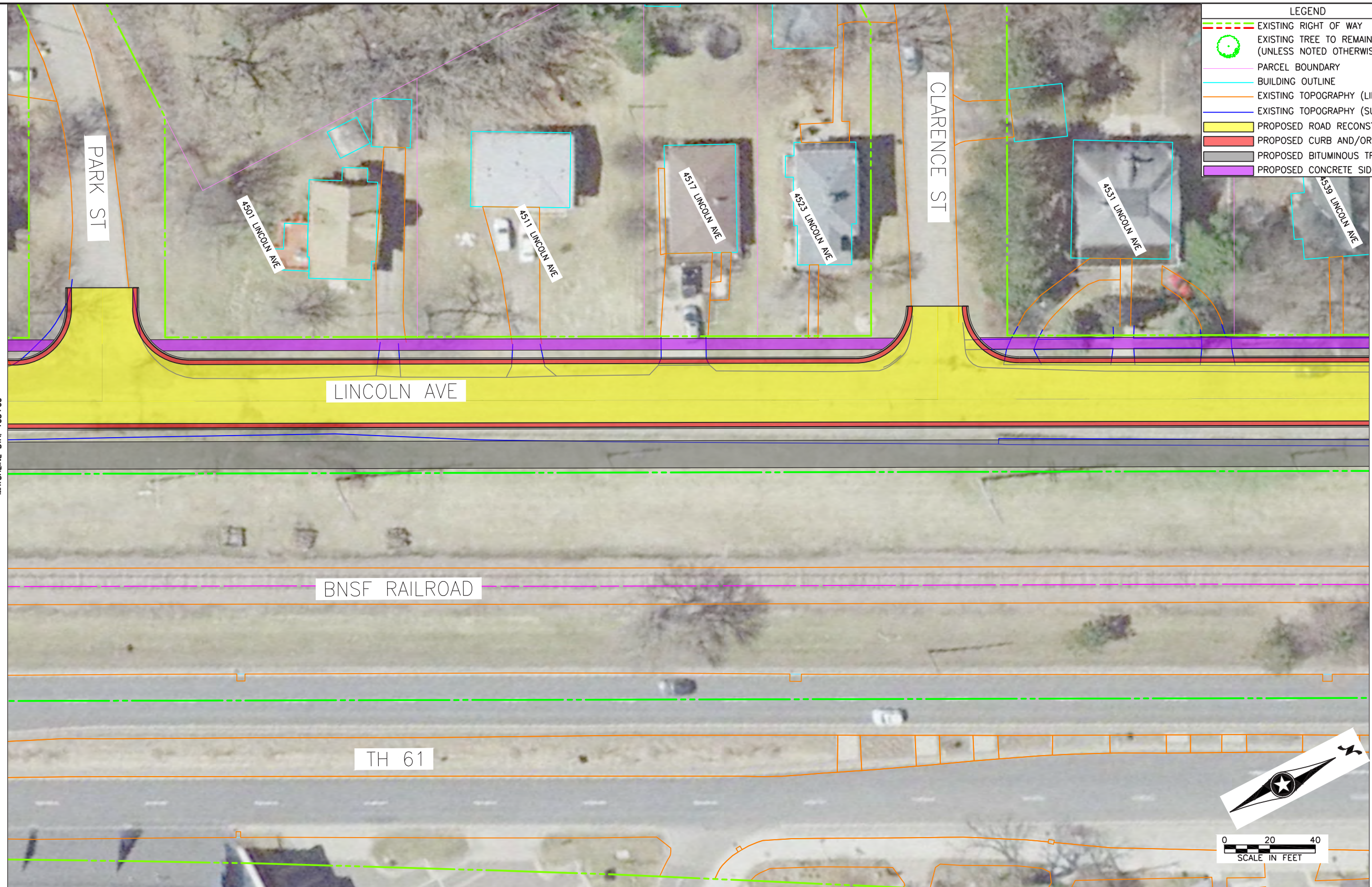
RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
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 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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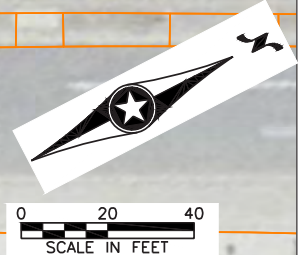
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CITY PROJECT NO.	COMM. NO. 14110



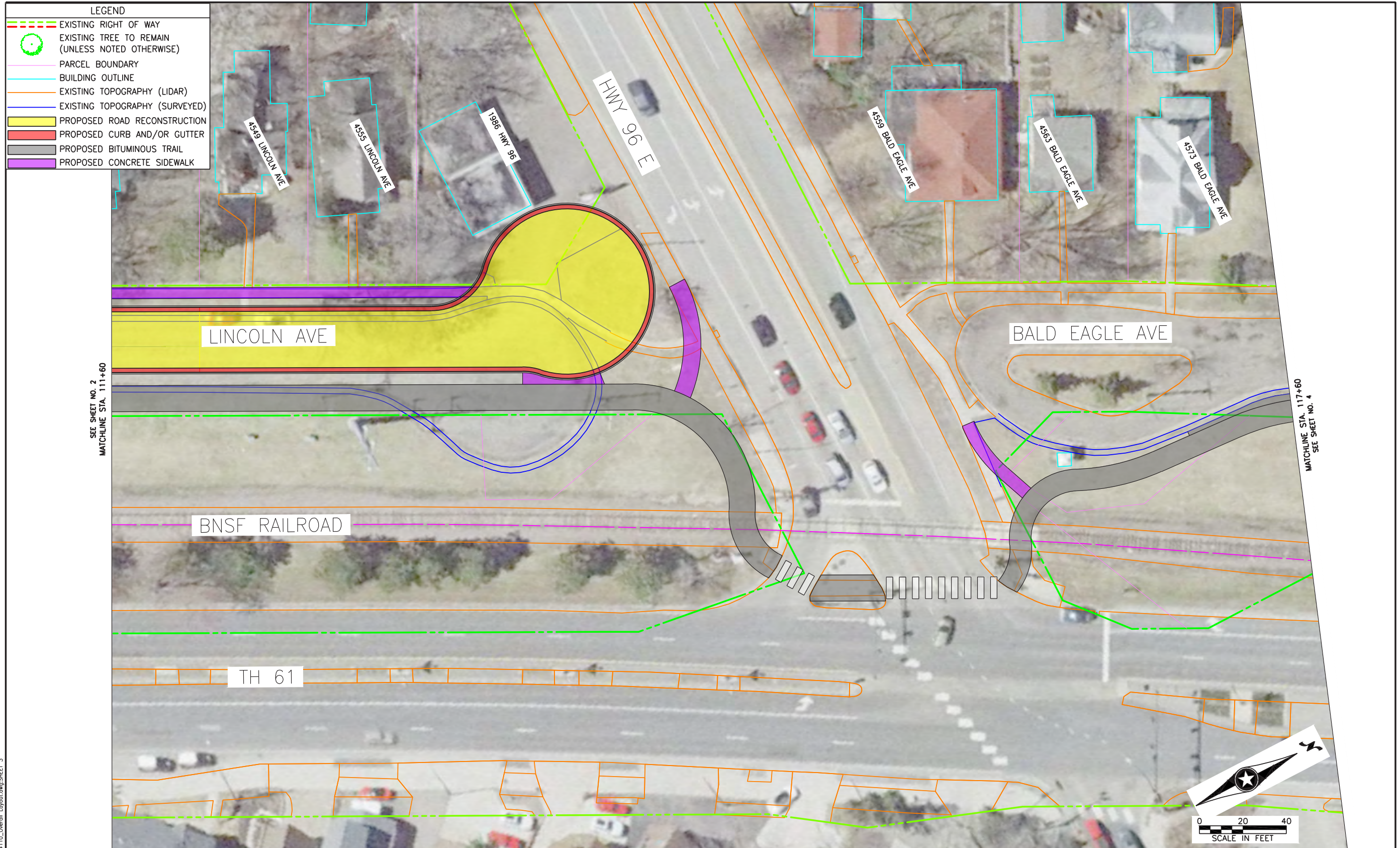
RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 2  
 OF  
 35





Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



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RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 3  
 OF  
 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



SEE SHEET NO. 3  
MATCHLINE STA. 117+60

EXISTING TREE TO BE REMOVED (4)

MATCHLINE STA. 123+60  
SEE SHEET NO. 5

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CITY PROJECT NO.	COMM. NO. 14110

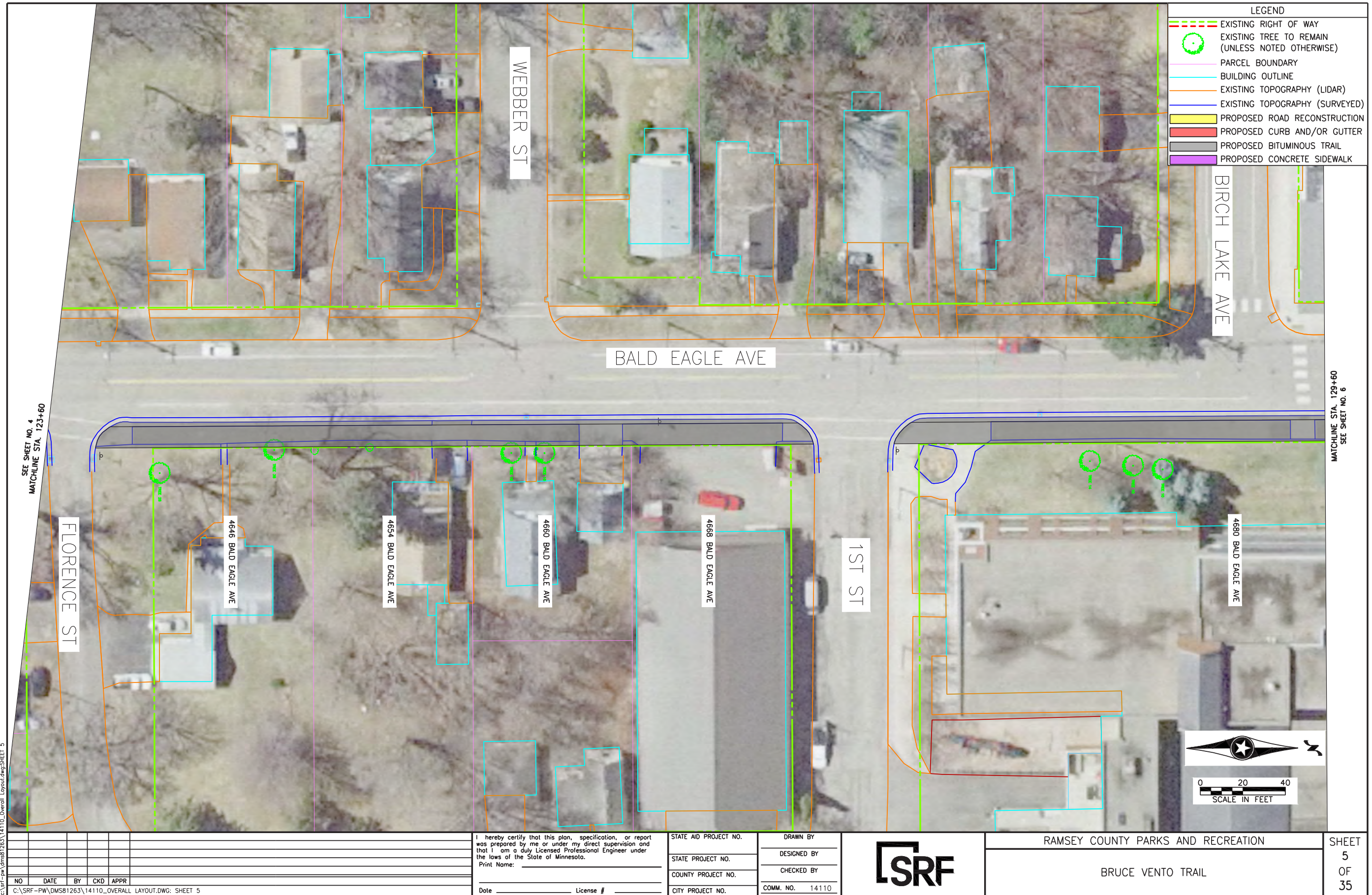


RAMSEY COUNTY PARKS AND RECREATION  
BRUCE VENTO TRAIL

SHEET  
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OF  
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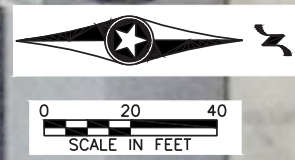


Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



**LEGEND**

- EXISTING RIGHT OF WAY
- EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
- PARCEL BOUNDARY
- BUILDING OUTLINE
- EXISTING TOPOGRAPHY (LIDAR)
- EXISTING TOPOGRAPHY (SURVEYED)
- PROPOSED ROAD RECONSTRUCTION
- PROPOSED CURB AND/OR GUTTER
- PROPOSED BITUMINOUS TRAIL
- PROPOSED CONCRETE SIDEWALK



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SEE SHEET NO. 4  
 MATCHLINE STA. 123+60

MATCHLINE STA. 129+60  
 SEE SHEET NO. 6

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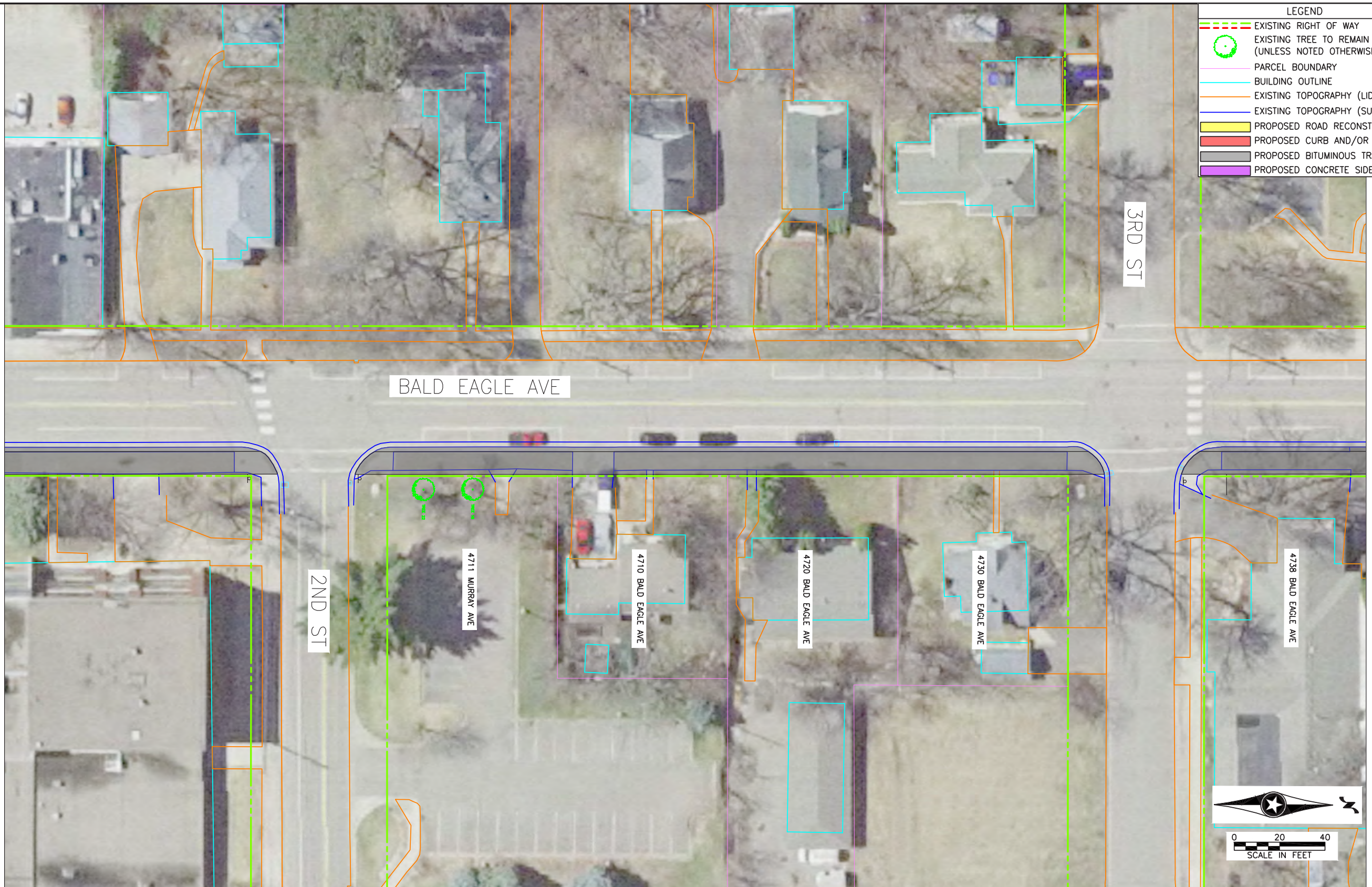
RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 5  
 OF  
 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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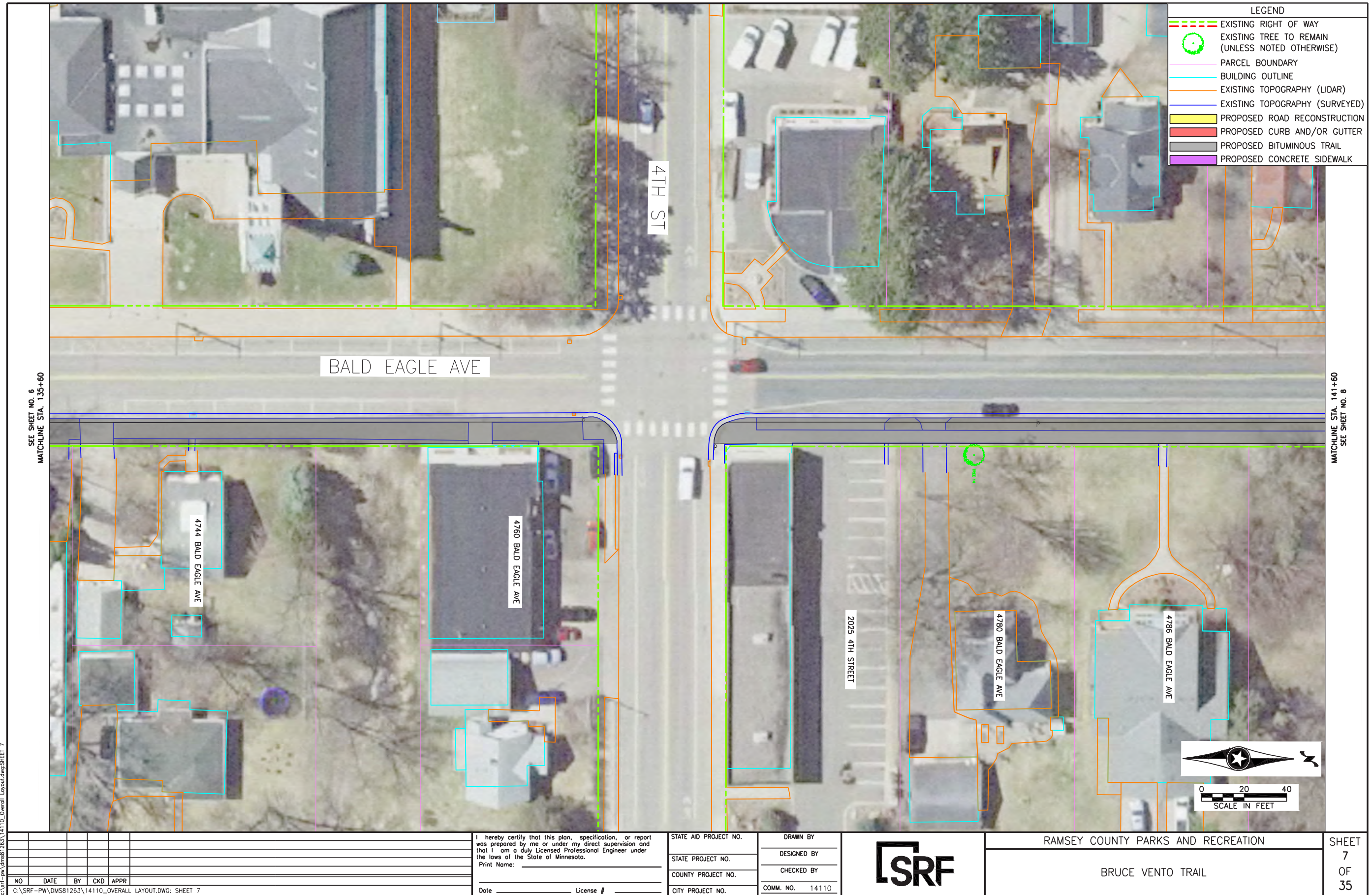


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 BRUCE VENTO TRAIL

SHEET  
 6  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



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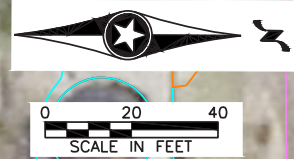
RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 7  
 OF  
 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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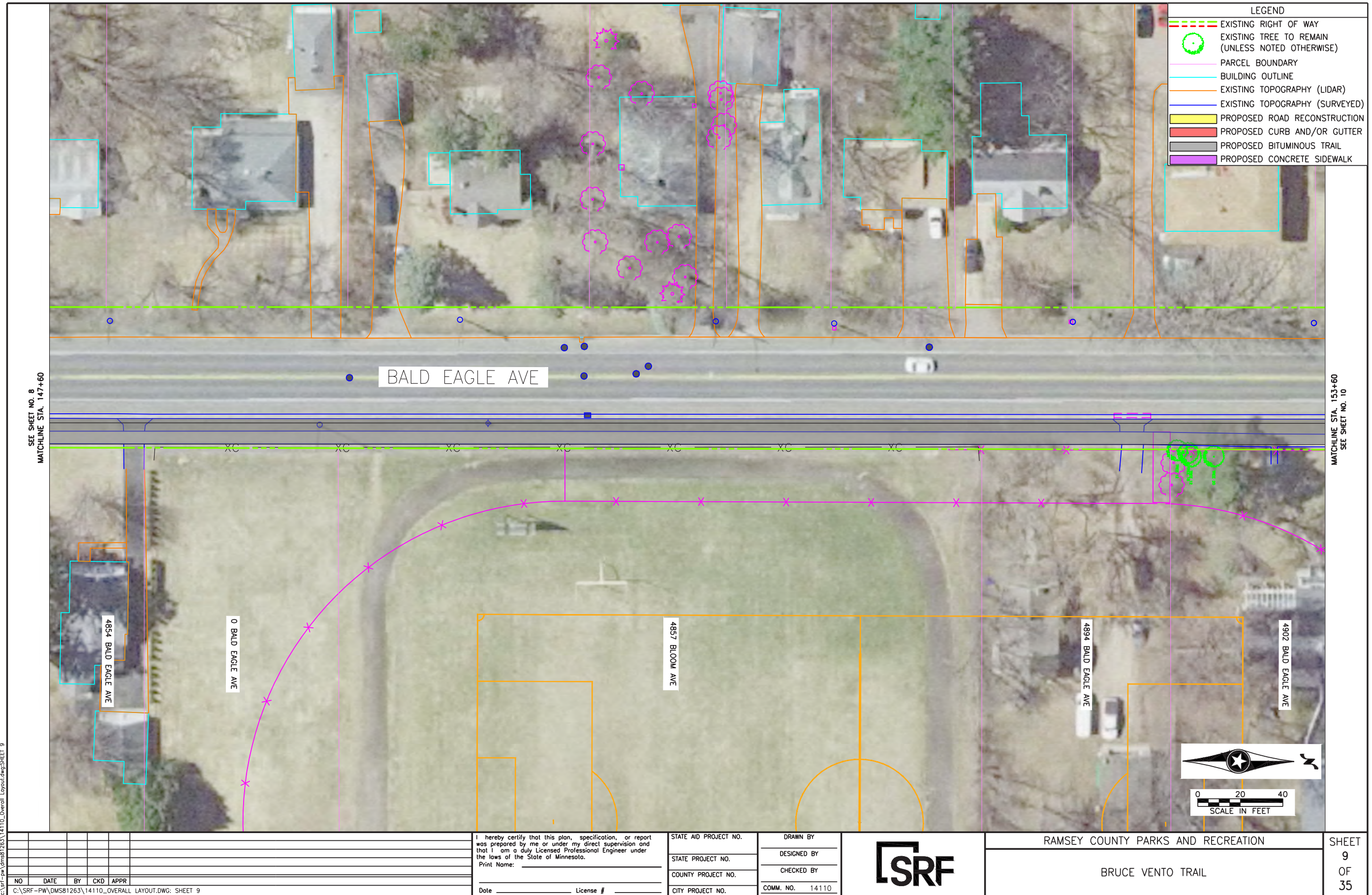


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 BRUCE VENTO TRAIL

SHEET  
 8  
 OF  
 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



**LEGEND**

- EXISTING RIGHT OF WAY
- EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
- PARCEL BOUNDARY
- BUILDING OUTLINE
- EXISTING TOPOGRAPHY (LIDAR)
- EXISTING TOPOGRAPHY (SURVEYED)
- PROPOSED ROAD RECONSTRUCTION
- PROPOSED CURB AND/OR GUTTER
- PROPOSED BITUMINOUS TRAIL
- PROPOSED CONCRETE SIDEWALK

SEE SHEET NO. 8  
MATCHLINE STA. 147+60

MATCHLINE STA. 153+60  
SEE SHEET NO. 10



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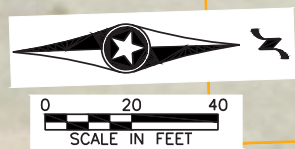
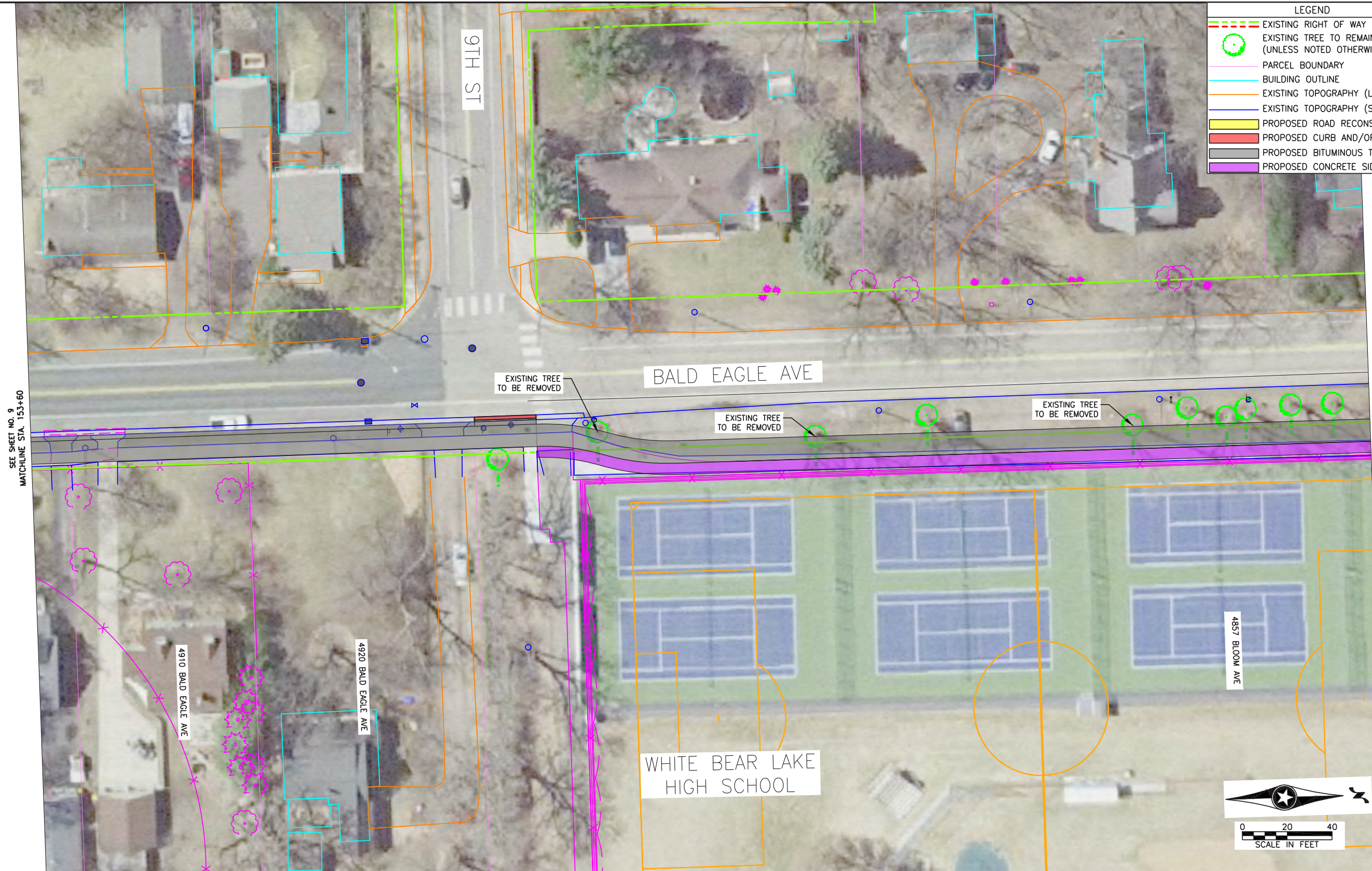
RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 9  
 OF  
 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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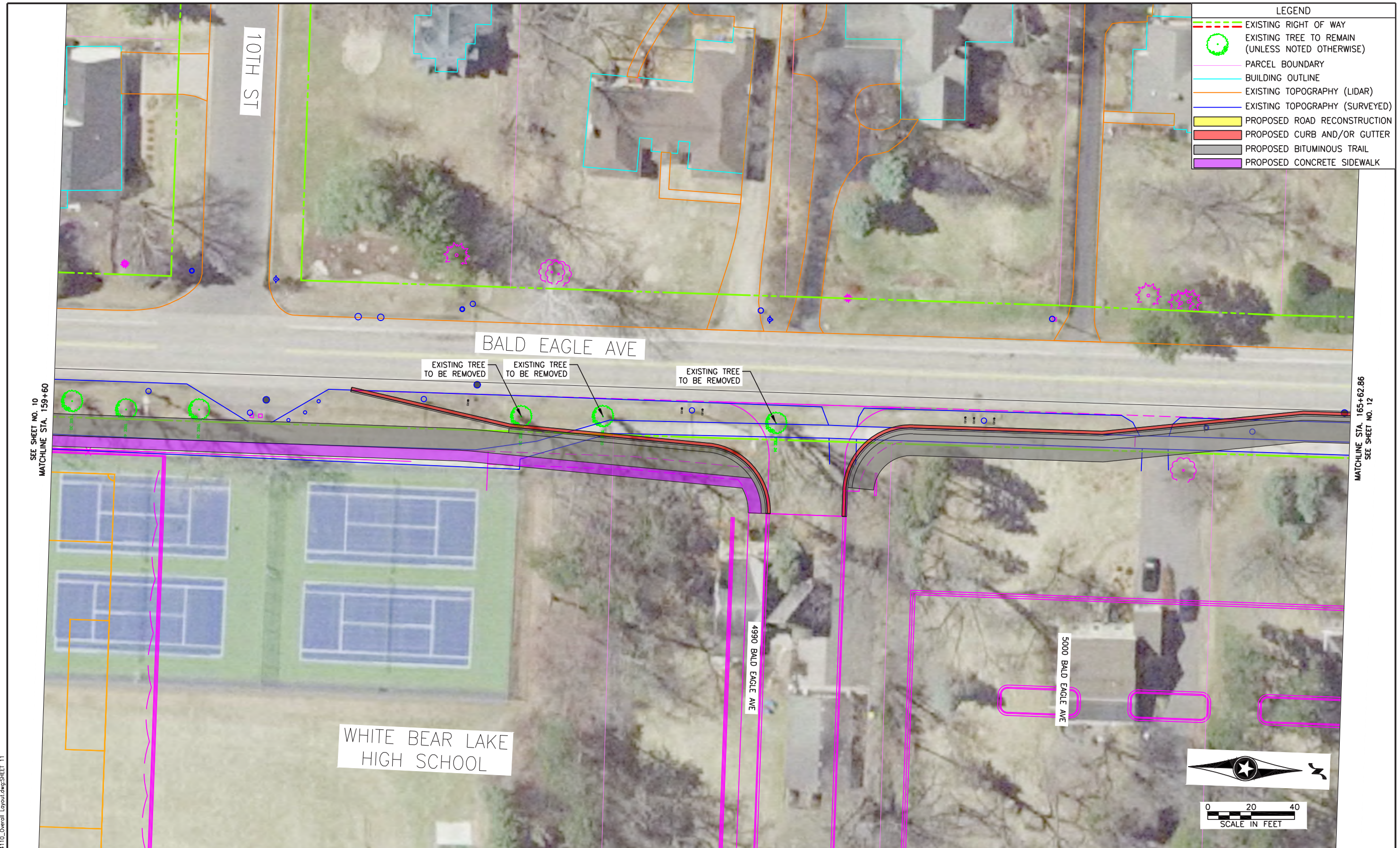


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 BRUCE VENTO TRAIL

SHEET  
 10  
 OF  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



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







RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 11  
 OF  
 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



SEE SHEET NO. 11  
MATCHLINE STA. 165+62.86

MATCHLINE STA. 171+62.86  
SEE SHEET NO. 13

5014 BALD EAGLE AVE

5018 BALD EAGLE AVE

5028 BALD EAGLE AVE

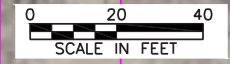
5038 BALD EAGLE AVE

12TH ST

5045 DIVISION AVE

5050 BALD EAGLE AVE

5060 BALD EAGLE AVE



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RAMSEY COUNTY PARKS AND RECREATION  
BRUCE VENTO TRAIL

SHEET  
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OF  
35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



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 STATE PROJECT NO. \_\_\_\_\_  
 COUNTY PROJECT NO. \_\_\_\_\_  
 CITY PROJECT NO. \_\_\_\_\_

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RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 13  
 OF  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



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CITY PROJECT NO.	COMM. NO. 14110

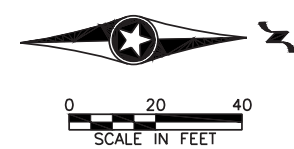
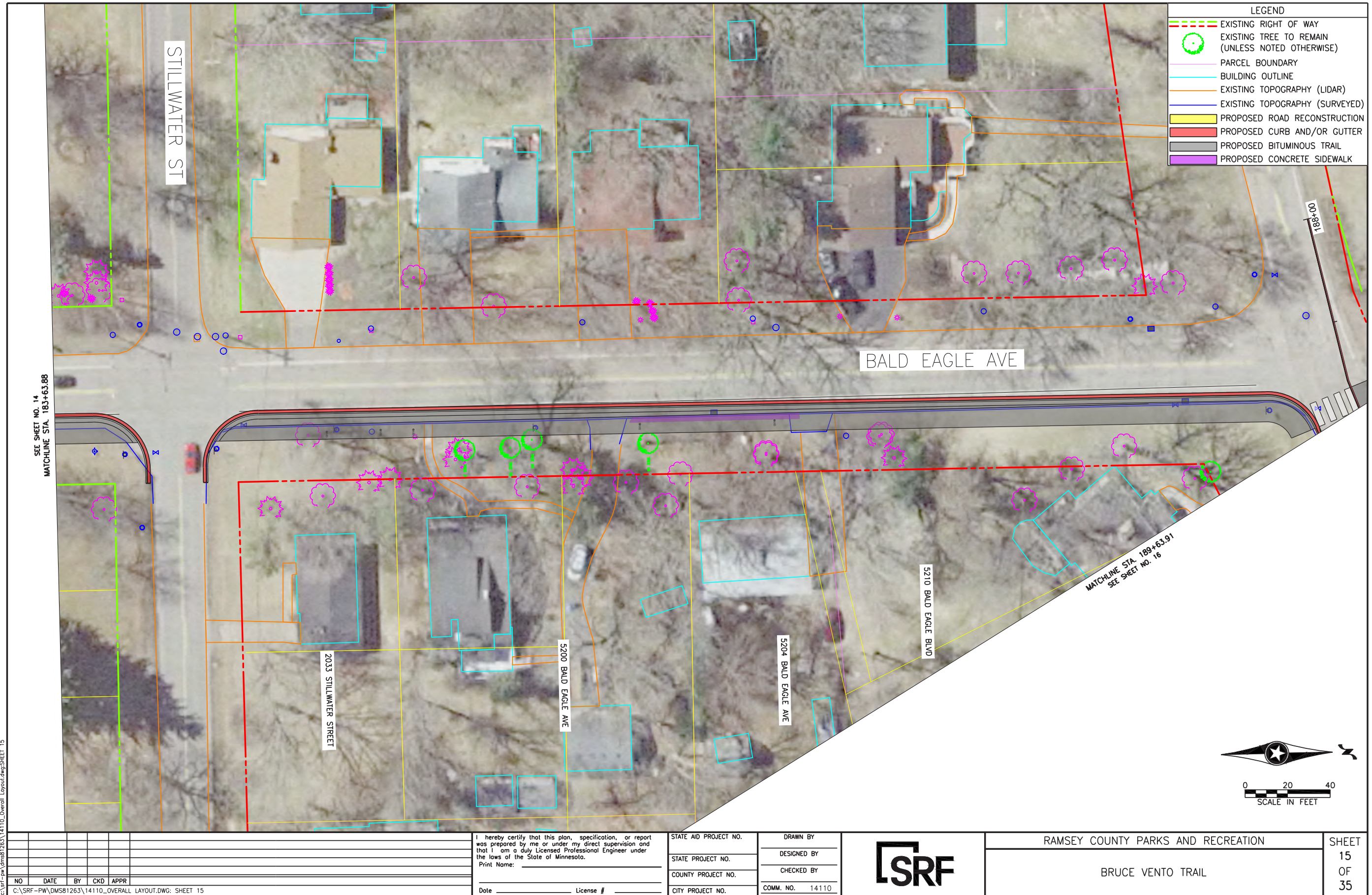


RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 14  
 OF  
 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



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RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 15  
 OF  
 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



SEE SHEET NO. 15  
MATCHLINE STA. 189+63.91

MATCHLINE STA. 195+63.91  
SEE SHEET NO. 17

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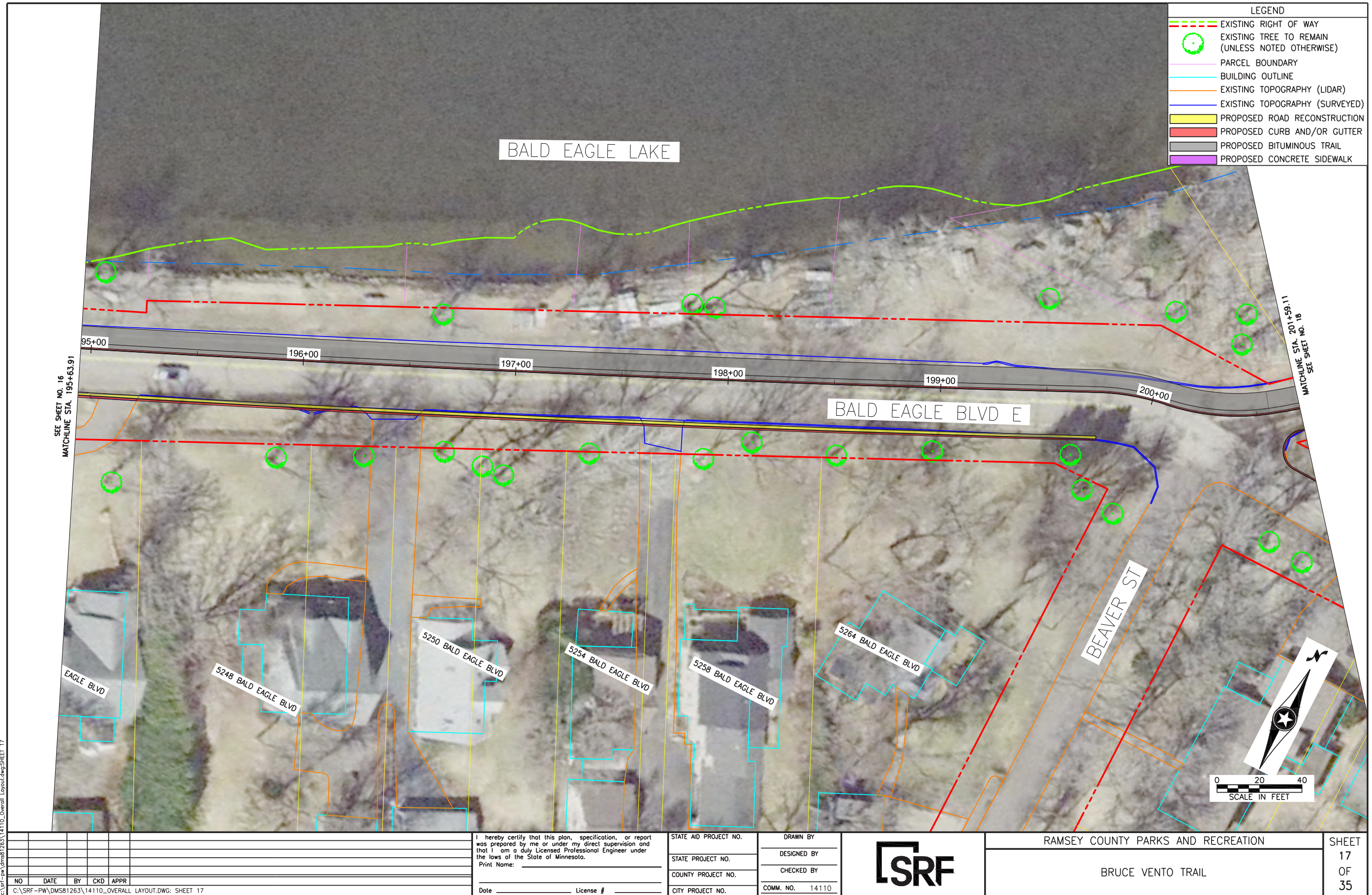


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SHEET  
16  
OF  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK

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RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET 17 OF 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



SEE SHEET NO. 17  
MATCHLINE STA. 201+59.11

MATCHLINE STA. 207+61.35  
SEE SHEET NO. 19

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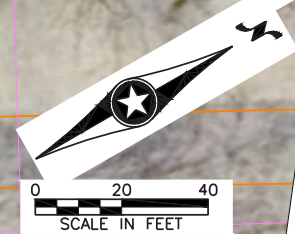
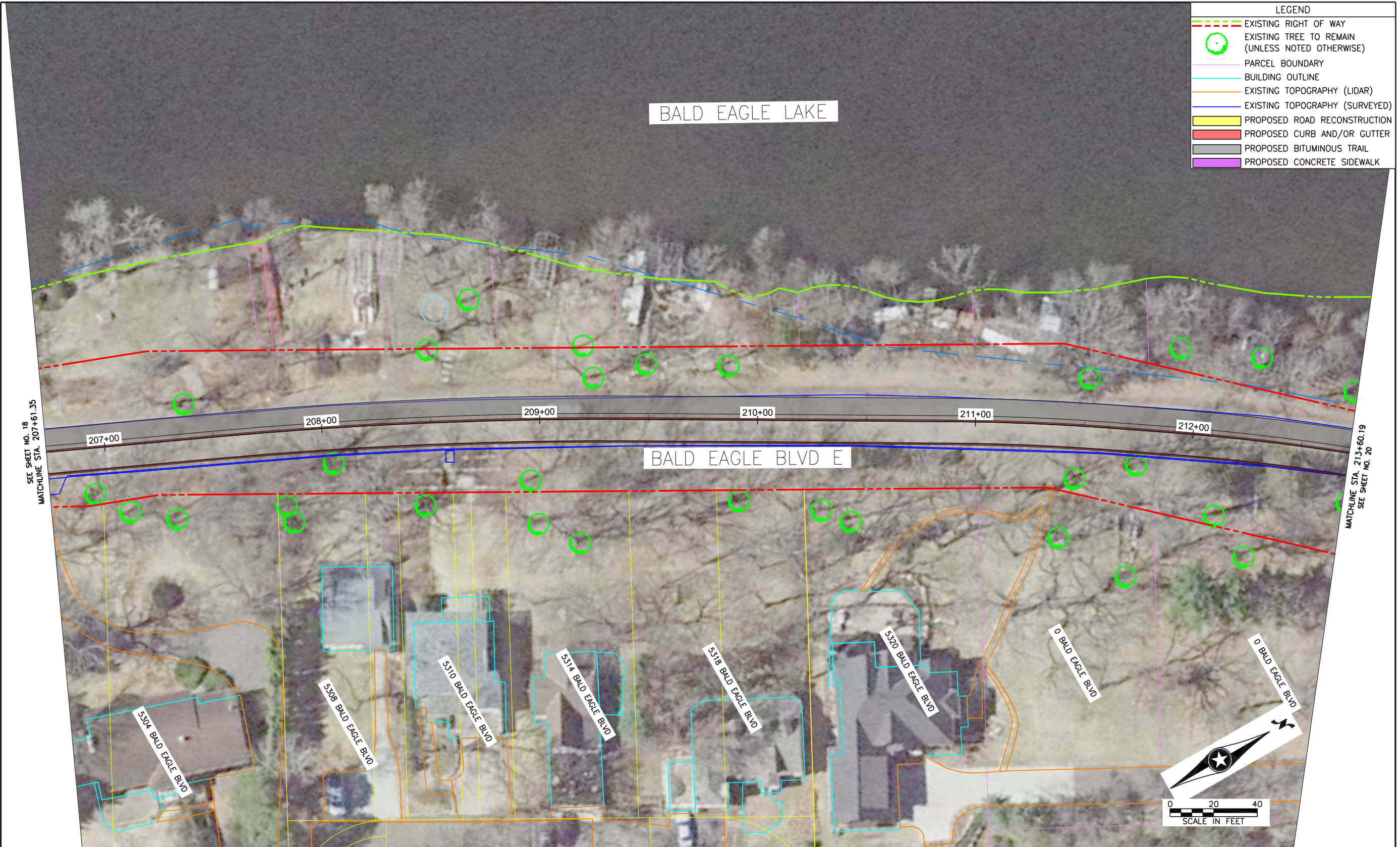
RAMSEY COUNTY PARKS AND RECREATION  
BRUCE VENTO TRAIL

SHEET  
18  
OF  
35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 19  
 OF  
 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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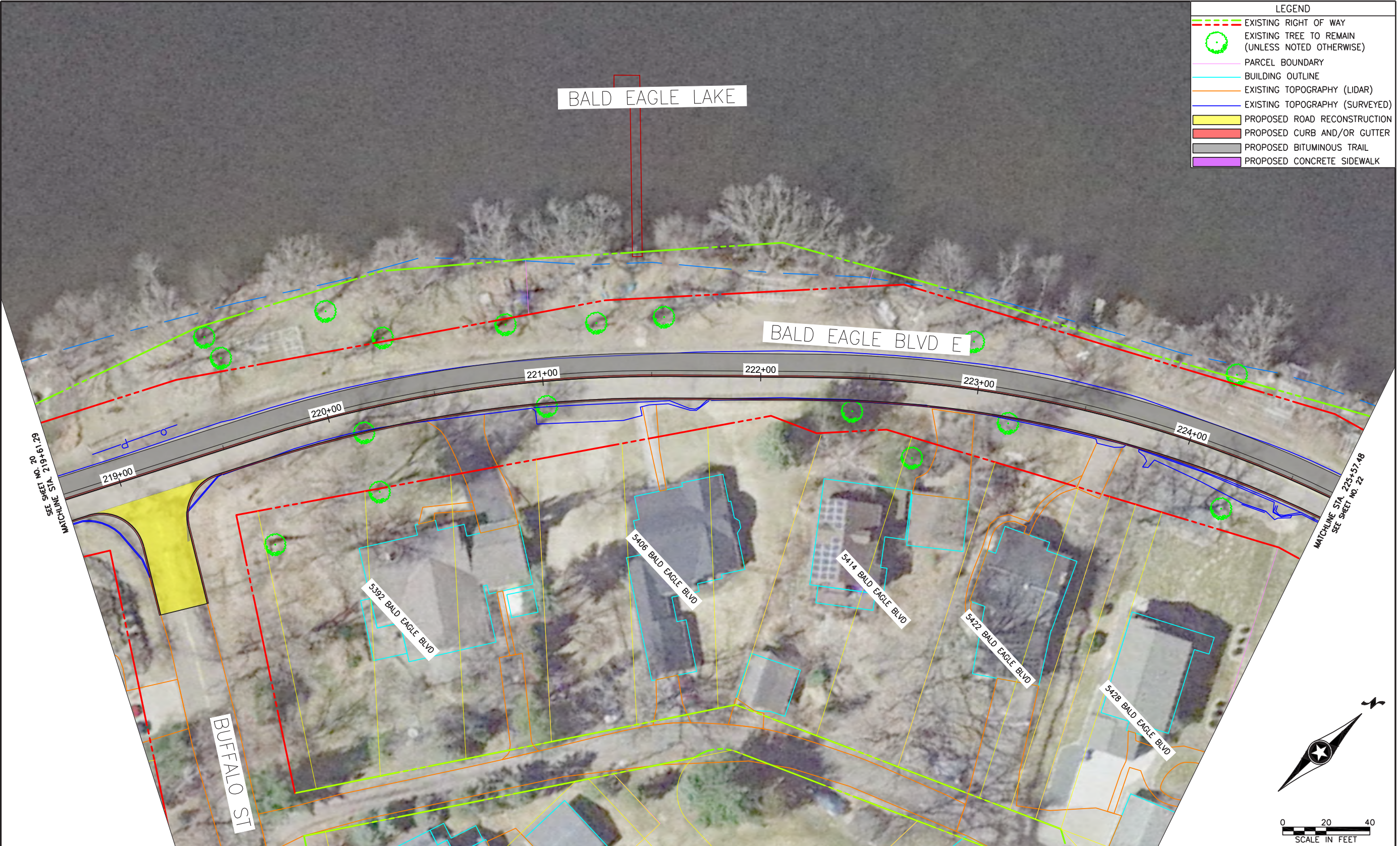
RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
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	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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 BRUCE VENTO TRAIL

SHEET  
 21  
 OF  
 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



**LEGEND**

- EXISTING RIGHT OF WAY
- EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
- PARCEL BOUNDARY
- BUILDING OUTLINE
- EXISTING TOPOGRAPHY (LIDAR)
- EXISTING TOPOGRAPHY (SURVEYED)
- PROPOSED ROAD RECONSTRUCTION
- PROPOSED CURB AND/OR GUTTER
- PROPOSED BITUMINOUS TRAIL
- PROPOSED CONCRETE SIDEWALK

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 BRUCE VENTO TRAIL

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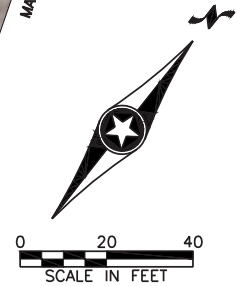
Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
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	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
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	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



SEE SHEET NO. 22  
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SEE SHEET NO. 24



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BRUCE VENTO TRAIL

SHEET  
23  
OF  
35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
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	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



MATCHLINE STA. 233+53.83  
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MATCHLINE STA. 243+50.79  
SEE SHEET NO. 25



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SHEET  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



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 BRUCE VENTO TRAIL

SHEET  
 25  
 OF  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



**LEGEND**

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- BUILDING OUTLINE
- EXISTING TOPOGRAPHY (LIDAR)
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- PROPOSED BITUMINOUS TRAIL
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 BRUCE VENTO TRAIL

SHEET  
 26  
 OF  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



SEE SHEET NO. 26  
MATCHLINE STA. 255+47.69

MATCHLINE STA. 261+48.23  
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RAMSEY COUNTY PARKS AND RECREATION  
BRUCE VENTO TRAIL

SHEET  
27  
OF  
35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
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	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



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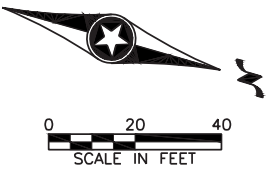
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 BRUCE VENTO TRAIL

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 29  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
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	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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	RAMSEY COUNTY PARKS AND RECREATION	SHEET 30
	BRUCE VENTO TRAIL	OF 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
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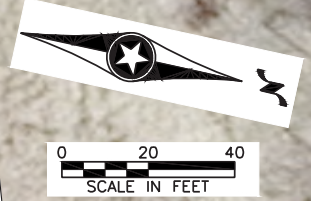
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 BRUCE VENTO TRAIL

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 OF  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
	EXISTING RIGHT OF WAY
	EXISTING TREE TO REMAIN (UNLESS NOTED OTHERWISE)
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
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	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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MATCHLINE STA. 285+83.51

MATCHLINE STA. 291+60.65  
SEE SHEET NO. 33



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BRUCE VENTO TRAIL

SHEET  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



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RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
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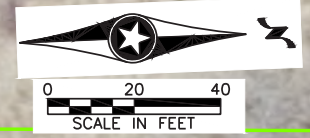
Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road

LEGEND	
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	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK



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BRUCE VENTO TRAIL

SHEET  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and One-Way Roadway on Hugo Road



SEE SHEET NO. 34  
MATCHLINE STA. 303+59.82

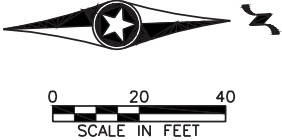
BALD EAGLE LAKE PARK

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HUGO RD

COUNTY ROAD J

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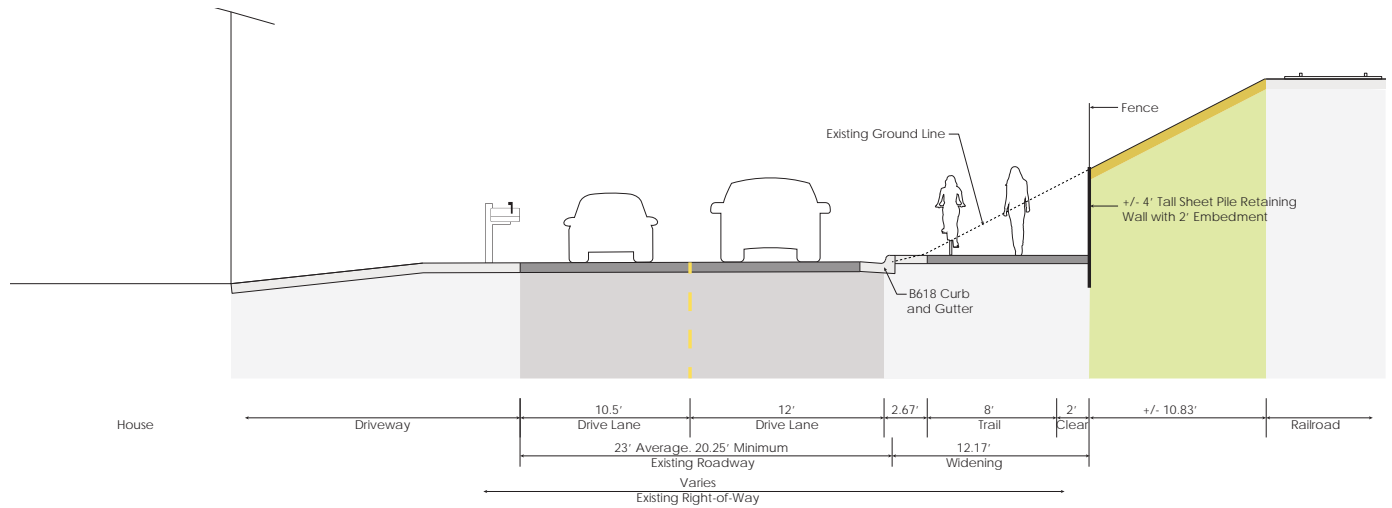
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## Preferred Alignment with Two-Way Roadway on Hugo Road at Pinch Point

The following five pages include concept plans for trail concept with a two-way roadway on Hugo Road and a two-way roadway on Bald Eagle Boulevard. Figure 16 shows a conceptual section of the proposed trail placement along Hugo Road at the pinch point location.



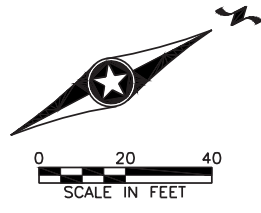
**FIGURE 16: Two-Way Roadway on Hugo Road at Pinch Point Section.**



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and Two-Way Roadway on Hugo Road



LEGEND	
	EXISTING RIGHT OF WAY
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SHEET  
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 OF  
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Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and Two-Way Roadway on Hugo Road



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NO	DATE	BY	CKD	APPR

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
 Print Name: \_\_\_\_\_  
 Date \_\_\_\_\_ License # \_\_\_\_\_

STATE AID PROJECT NO.	DRAWN BY
STATE PROJECT NO.	DESIGNED BY
COUNTY PROJECT NO.	CHECKED BY
CITY PROJECT NO.	COMM. NO. 14110



RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET 26 OF 35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and Two-Way Roadway on Hugo Road



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NO	DATE	BY	CKD	APPR

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Print Name: \_\_\_\_\_  
Date \_\_\_\_\_ License # \_\_\_\_\_

STATE AID PROJECT NO. \_\_\_\_\_  
STATE PROJECT NO. \_\_\_\_\_  
COUNTY PROJECT NO. \_\_\_\_\_  
CITY PROJECT NO. \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
DESIGNED BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_  
COMM. NO. 14110



RAMSEY COUNTY PARKS AND RECREATION

BRUCE VENTO TRAIL

SHEET  
27  
OF  
35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and Two-Way Roadway on Hugo Road



LEGEND	
	EXISTING RIGHT OF WAY
	PARCEL BOUNDARY
	BUILDING OUTLINE
	EXISTING TOPOGRAPHY (LIDAR)
	EXISTING TOPOGRAPHY (SURVEYED)
	PROPOSED ROAD RECONSTRUCTION
	PROPOSED CURB AND/OR GUTTER
	PROPOSED BITUMINOUS TRAIL
	PROPOSED CONCRETE SIDEWALK

SEE SHEET NO. 27  
MATCHLINE STA. 261+48.23

MATCHLINE STA. 267+49.37  
SEE SHEET NO. 29

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NO	DATE	BY	CKD	APPR

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
Print Name: \_\_\_\_\_  
Date \_\_\_\_\_ License # \_\_\_\_\_

STATE AID PROJECT NO.	DRAWN BY
STATE PROJECT NO.	DESIGNED BY
COUNTY PROJECT NO.	CHECKED BY
CITY PROJECT NO.	COMM. NO. 14110



RAMSEY COUNTY PARKS AND RECREATION  
BRUCE VENTO TRAIL

SHEET  
28  
OF  
35



Preferred Design Scenario with One-Way Roadway on Bald Eagle Boulevard from Park Avenue to Taylor Avenue and Two-Way Roadway on Hugo Road



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NO	DATE	BY	CHKD	APPR

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
 Print Name: \_\_\_\_\_  
 Date \_\_\_\_\_ License # \_\_\_\_\_

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STATE PROJECT NO.	DESIGNED BY
COUNTY PROJECT NO.	CHECKED BY
CITY PROJECT NO.	COMM. NO. 14110



RAMSEY COUNTY PARKS AND RECREATION  
 BRUCE VENTO TRAIL

SHEET  
 29  
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 35



## COST ESTIMATES

Cost estimates were prepared for the two alternatives of the alignment shown graphically in this report, the preferred alignment with one-way roadway on Hugo Road at the pinch point and the preferred alignment with two-way roadway on Hugo Road at pinch point.

The preferred alignment with one-way roadway on Hugo Road at the pinch point includes the following:

- Two-way roadway on Bald Eagle Boulevard to Park Avenue.
- One-way roadway on Bald Eagle Boulevard North of Park Avenue.
- One-way roadway on Taylor Avenue.
- One-way roadway on Hugo Road from Taylor Avenue to the Park.

The preferred alignment with two-way roadway on Hugo Road at pinch point is the same as above with the exception of a two-way roadway at Hugo Road south segment between Taylor Avenue and the Park.

Total costs for the two preferred alternates are as follows:

- Preferred alignment with one-way roadway on Hugo Road at the pinch point: \$7,018,850.
- Preferred alignment with two-way roadway on Hugo Road at the pinch point: \$7,927,300.

In order to provide additional support to propose removing southbound traffic on Bald Eagle Boulevard and Taylor Avenue between Hugo Road and Park Avenue, two additional estimates were prepared to examine concept costs to add a trail while maintaining two-way traffic on Bald Eagle Boulevard. Graphics were not prepared for these alternatives but rather defined by the following assumptions:

- Removal of existing roadway.
- New trail built on lake side of the road.
- Roadway fully reconstructed and shifted to the house side of the road.
- Retaining walls on the house side of the road.
- Additional allowances for drainage improvements, including extension of the box culvert south of Buffalo Street.
- Provisions for permanent and temporary easements along the house side of the road.
- Extensive tree removal.

Total costs for these two additional alternatives are as follows:

- Two-way Bald Eagle Boulevard with one-way roadway on Hugo Road at the pinch point: \$13,169,350.
- Two-way Bald Eagle Boulevard with two-way roadway on Hugo Road at the pinch point: \$14,077,800.

Cost estimates for the alignments noted above are shown in Figures 17-20. Complete cost estimates are provided in Appendix F.





**Bruce Vento Regional Trail - Phase 2**  
**One Way Alternative on Bald Eagle Boulevard, One Way Alternative on Hugo Road**  
**Concept Cost Estimate (based upon 2020 bid price information)**  
**Prepared By: SRF Consulting Group, Inc., June 14, 2022**

					TOTAL	
ITEM DESCRIPTION			UNIT	UNIT PRICE	EST. QUANTITY	EST. AMOUNT
<b>PAVING AND GRADING COSTS</b>						
GrP 1a	2106 Excavation - common & subgrade		cu. yd.	\$10.00	8,160	\$81,600
GrP 2d	2106 Granular Subgrade (CV)		cu. yd.	\$15.00	5,640	\$84,600
GrP 3g	Local & Frontage Road Pavement	(1)	sq. yd.	\$20.00	6,700	\$134,000
GrP 4a	Concrete Walk / Trail / Median	(2)	sq. yd.	\$75.00	1,250	\$93,750
GrP 4b	Bituminous Walk / Trail	(2)	sq. yd.	\$40.00	25,700	\$1,028,000
GrP 4c	ADA Pedestrian Curb Ramp		each	\$1800.00	45	\$81,000
GrP 5	Concrete Curb and Gutter		lin. ft.	\$27.00	15,300	\$413,100
GrP 8a	Removals - Pavement		sq. yd.	\$6.00	26,300	\$157,800
<b>SUBTOTAL PAVING AND GRADING COSTS:</b>						<b>\$2,073,850</b>
<b>DRAINAGE, UTILITIES AND EROSION CONTROL</b>						
Dr 3	Water Quality Ponds & Other BMPs		l.s.			\$200,000
Dr 5	Drainage - urban		l.s.			\$500,000
Dr 6	Drainage - rural	(3)	mile	\$100,000	0.7	\$70,000
Dr 7	Turf Establishment & Erosion Control		10%			\$212,000
Dr 8	Landscaping & Site Amenities		10%			\$212,000
<b>SUBTOTAL DRAINAGE, UTILITIES AND EROSION CONTROL</b>						<b>\$1,194,000</b>
<b>RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS</b>						
RW 1	Box Culvert -	(4)	lin. ft.	\$1,000		
RW 32	Steel Sheet Pile Wall & Safety Fencing		sq. ft.	\$125		
<b>SUBTOTAL RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS:</b>						
<b>SIGNAL AND LIGHTING COSTS</b>						
SGL 1	Railroad Signal System Relocation/Upgrade on Bald Eagle Ave		each	\$250,000	1	\$250,000
<b>SUBTOTAL SIGNAL AND LIGHTING COSTS:</b>						<b>\$250,000</b>
<b>SIGNING &amp; STRIPING COSTS</b>						
SGN 1	Mainline Signing (C&D)		mile	\$17,500	4.2	\$73,500
SGN 2	Mainline Striping		mile	\$2,500	4.2	\$10,500
<b>SUBTOTAL SIGNING &amp; STRIPING COSTS:</b>						<b>\$84,000</b>
<b>SUBTOTAL CONSTRUCTION COSTS:</b>						<b>\$3,601,850</b>
<b>MISCELLANEOUS COSTS</b>						
M 1	Mobilization		10%			\$366,000
M 2	Non Quantified Minor Items		10%			\$366,000
M 7	Utility Relocation (Bald Eagle Ave & Hugo Road)		10%			\$366,000
M 8	Traffic Control		3%			\$112,000
<b>SUBTOTAL MISCELLANEOUS COSTS:</b>						<b>\$1,210,000</b>
<b>ESTIMATED TOTAL CONSTRUCTION COSTS without Contingency:</b>						<b>\$4,811,850</b>
1	Contingency or "risk"		25%			\$1,207,000
<b>ESTIMATED TOTAL CONSTRUCTION COSTS PLUS CONTINGENCY:</b>						<b>\$6,018,850</b>
<b>INFLATION COST (CURRENT YR. TO YR. OF OPENING)</b>			Years	3%		\$1,000,000
<b>TOTAL PROJECT COST (OPENING YEAR DOLLARS)</b>						<b>\$7,018,850</b>

NOTE: (1) Includes aggregate base class 5 and PASB or OGAB, as appropriate.  
 (2) Includes aggregate base class 5 and earthwork.  
 (3) Does not include pavement edge drains, see separate item.  
 (4) Does not include excavation or backfill.  
 (5) Assumes Mn/DOT Standard Plan Sheet designs; does not include excavation or backfill.  
 (6) Does not include Moment Slab.  
 (7) Includes moment slab and concrete barrier.  
 (8) Tie back system required for exposed retained heights greater than 15 Feet. This cost is additional to the item for temporary steel sheet

Utility Agreements based upon:  
 R/W Acquisitions based upon:

**FIGURE 17: Phase 2 Estimate for One Way Alternative on Hugo Road**





**Bruce Vento Regional Trail - Phase 2**  
**One Way Alternative on Bald Eagle Boulevard, Two Way Alternative on Hugo Road**  
**Concept Cost Estimate (based upon 2020 bid price information)**  
**Prepared By: SRF Consulting Group, Inc., June 14, 2022**

				TOTAL		
ITEM DESCRIPTION			UNIT	UNIT PRICE	EST. QUANTITY	EST. AMOUNT
<b>PAVING AND GRADING COSTS</b>						
GrP 1a	2106 Excavation - common & subgrade		cu. yd.	\$10.00	7,080	\$70,800
GrP 2d	2106 Granular Subgrade (CV)		cu. yd.	\$15.00	4,920	\$73,800
GrP 3g	Local & Frontage Road Pavement	(1)	sq. yd.	\$20.00	5,850	\$117,000
GrP 4a	Concrete Walk / Trail / Median	(2)	sq. yd.	\$75.00	1,250	\$93,750
GrP 4b	Bituminous Walk / Trail	(2)	sq. yd.	\$40.00	25,000	\$1,000,000
GrP 4c	ADA Pedestrian Curb Ramp		each	\$1800.00	45	\$81,000
GrP 5	Concrete Curb and Gutter		lin. ft.	\$27.00	14,650	\$395,550
GrP 8a	Removals - Pavement		sq. yd.	\$6.00	22,400	\$134,400
<b>SUBTOTAL PAVING AND GRADING COSTS:</b>						<b>\$1,966,300</b>
<b>DRAINAGE, UTILITIES AND EROSION CONTROL</b>						
Dr 3	Water Quality Ponds & Other BMPs		l.s.			\$200,000
Dr 5	Drainage - urban		l.s.			\$500,000
Dr 6	Drainage - rural	(3)	mile	\$100,000	0.7	\$70,000
Dr 7	Turf Establishment & Erosion Control		10%			\$201,000
Dr 8	Landscaping & Site Amenities		10%			\$201,000
<b>SUBTOTAL DRAINAGE, UTILITIES AND EROSION CONTROL</b>						<b>\$1,172,000</b>
<b>RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS</b>						
RW 1	Box Culvert -	(4)	lin. ft.	\$1,000	5	\$5,000
RW 32	Steel Sheet Pile Wall & Safety Fencing		sq. ft.	\$125	4,800	\$600,000
<b>SUBTOTAL RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS:</b>						<b>\$605,000</b>
<b>SIGNAL AND LIGHTING COSTS</b>						
SGL 1	Railroad Signal System Relocation/Upgrade on Bald Eagle Ave		each	\$250,000	1	\$250,000
<b>SUBTOTAL SIGNAL AND LIGHTING COSTS:</b>						<b>\$250,000</b>
<b>SIGNING &amp; STRIPING COSTS</b>						
SGN 1	Mainline Signing (C&D)		mile	\$17,500	4.2	\$73,500
SGN 2	Mainline Striping		mile	\$2,500	4.2	\$10,500
<b>SUBTOTAL SIGNING &amp; STRIPING COSTS:</b>						<b>\$84,000</b>
<b>SUBTOTAL CONSTRUCTION COSTS:</b>						<b>\$4,077,300</b>
<b>MISCELLANEOUS COSTS</b>						
M 1	Mobilization		10%			\$413,000
M 2	Non Quantified Minor Items		10%			\$413,000
M 7	Utility Relocation (Bald Eagle Ave & Hugo Road)		10%			\$413,000
M 8	Traffic Control		3%			\$126,000
<b>SUBTOTAL MISCELLANEOUS COSTS:</b>						<b>\$1,365,000</b>
<b>ESTIMATED TOTAL CONSTRUCTION COSTS without Contingency:</b>						<b>\$5,442,300</b>
1	Contingency or "risk"		25%			\$1,365,000
<b>ESTIMATED TOTAL CONSTRUCTION COSTS PLUS CONTINGENCY:</b>						<b>\$6,807,300</b>
<b>INFLATION COST (CURRENT YR. TO YR. OF OPENING)</b>			Years	3%		\$1,120,000
<b>TOTAL PROJECT COST (OPENING YEAR DOLLARS)</b>						<b>\$7,927,300</b>

- NOTE:
- (1) Includes aggregate base class 5 and PASB or OGAB, as appropriate.
  - (2) Includes aggregate base class 5 and earthwork.
  - (3) Does not include pavement edge drains, see separate item.
  - (4) Does not include excavation or backfill.
  - (5) Assumes Mn/DOT Standard Plan Sheet designs; does not include excavation or backfill.
  - (6) Does not include Moment Slab.
  - (7) Includes moment slab and concrete barrier.

Utility Agreements based upon:  
R/W Acquisitions based upon:

**FIGURE 18: Phase 2 Estimate for Two Way Alternative on Hugo Road**





**Bruce Vento Regional Trail - Phase 2**  
**Two Way Alternative on Bald Eagle Boulevard, One Way Alternative on Hugo Road**  
**Concept Cost Estimate (based upon 2020 bid price information)**  
**Prepared By: SRF Consulting Group, Inc., September 8, 2022**

				TOTAL		
ITEM DESCRIPTION			UNIT	UNIT PRICE	EST. QUANTITY	EST. AMOUNT
<b>PAVING AND GRADING COSTS</b>						
GrP 1a	2106 Excavation - common & subgrade		cu. yd.	\$10.00	19,800	\$198,000
GrP 2d	2106 Granular Subgrade (CV)		cu. yd.	\$15.00	13,440	\$201,600
GrP 3g	Local & Frontage Road Pavement	(1)	sq. yd.	\$20.00	16,400	\$328,000
GrP 4a	Concrete Walk / Trail / Median	(2)	sq. yd.	\$75.00	1,250	\$93,750
GrP 4b	Bituminous Walk / Trail	(2)	sq. yd.	\$40.00	25,700	\$1,028,000
GrP 4c	ADA Pedestrian Curb Ramp		each	\$1800.00	45	\$81,000
GrP 5	Concrete Curb and Gutter		lin. ft.	\$27.00	17,500	\$472,500
GrP 8a	Removals - Pavement		sq. yd.	\$6.00	32,000	\$192,000
<b>SUBTOTAL PAVING AND GRADING COSTS:</b>						<b>\$2,594,850</b>
<b>DRAINAGE, UTILITIES AND EROSION CONTROL</b>						
Dr 3	Water Quality Ponds & Other BMPs		l.s.			\$200,000
Dr 5	Drainage - urban		l.s.			\$525,000
Dr 6	Drainage - rural	(3)	mile	\$100,000	0.7	\$70,000
Dr 7	Turf Establishment & Erosion Control		10%			\$264,000
Dr 8	Landscaping & Site Amenities		10%			\$264,000
<b>SUBTOTAL DRAINAGE, UTILITIES AND EROSION CONTROL</b>						<b>\$1,323,000</b>
<b>RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS</b>						
RW 1	Box Culvert -	(4)	lin. ft.	\$1,000	15	\$15,000
RW 32	Steel Sheet Pile Wall & Safety Fencing		sq. ft.	\$125	19,100	\$2,387,500
<b>SUBTOTAL RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS:</b>						<b>\$2,402,500</b>
<b>SIGNAL AND LIGHTING COSTS</b>						
SGL 1	Railroad Signal System Relocation/Upgrade on Bald Eagle Ave		each	\$250,000	1	\$250,000
<b>SUBTOTAL SIGNAL AND LIGHTING COSTS:</b>						<b>\$250,000</b>
<b>SIGNING &amp; STRIPING COSTS</b>						
SGN 1	Mainline Signing (C&D)		mile	\$17,500	4.2	\$73,500
SGN 2	Mainline Striping		mile	\$2,500	4.2	\$10,500
<b>SUBTOTAL SIGNING &amp; STRIPING COSTS:</b>						<b>\$84,000</b>
<b>SUBTOTAL CONSTRUCTION COSTS:</b>						<b>\$6,654,350</b>
<b>MISCELLANEOUS COSTS</b>						
M 1	Mobilization		10%			\$672,000
M 2	Non Quantified Minor Items		10%			\$672,000
M 7	Utility Relocation (Bald Eagle Ave & Hugo Road)		10%			\$672,000
M 8	Traffic Control		3%			\$203,000
<b>SUBTOTAL MISCELLANEOUS COSTS:</b>						<b>\$2,219,000</b>
<b>ESTIMATED TOTAL CONSTRUCTION COSTS without Contingency:</b>						<b>\$8,873,350</b>
1	Contingency or "risk"		25%			\$2,223,000
<b>ESTIMATED TOTAL CONSTRUCTION COSTS PLUS CONTINGENCY:</b>						<b>\$11,096,350</b>
<b>OTHER PROJECT COSTS:</b>						
<b>R/W ACQUISITIONS</b>			Lump Sum	\$223,000	1	\$223,000
<b>TOTAL PROJECT COST</b>						<b>\$11,319,350</b>
<b>INFLATION COST (CURRENT YR. TO YR. OF OPENING)</b>			Years	3%		\$1,850,000
<b>TOTAL PROJECT COST (OPENING YEAR DOLLARS)</b>						<b>\$13,169,350</b>

- NOTE:
- (1) Includes aggregate base class 5 and PASB or OGAB, as appropriate.
  - (2) Includes aggregate base class 5 and earthwork.
  - (3) Does not include pavement edge drains, see separate item.
  - (4) Does not include excavation or backfill.
  - (5) Assumes Mn/DOT Standard Plan Sheet designs; does not include excavation or backfill.
  - (6) Does not include Moment Slab.
  - (7) Includes moment slab and concrete barrier.

**FIGURE 19: Phase 2 Estimate for Two Way Alternative on Bald Eagle Boulevard, One Way Alternative on Hugo Road**





**Bruce Vento Regional Trail - Phase 2**  
**Two Way Alternative on Bald Eagle Boulevard, Two Way Alternative on Hugo Road**  
**Concept Cost Estimate (based upon 2020 bid price information)**  
**Prepared By: SRF Consulting Group, Inc., September 21, 2022**

					TOTAL		
ITEM DESCRIPTION				UNIT	UNIT PRICE	EST. QUANTITY	EST. AMOUNT
<b>PAVING AND GRADING COSTS</b>							
GrP 1a	2106 Excavation - common & subgrade		cu. yd.	\$10.00	18,720	\$187,200	
GrP 2d	2106 Granular Subgrade (CV)		cu. yd.	\$15.00	12,720	\$190,800	
GrP 3g	Local & Frontage Road Pavement	(1)	sq. yd.	\$20.00	15,550	\$311,000	
GrP 4a	Concrete Walk / Trail / Median	(2)	sq. yd.	\$75.00	1,250	\$93,750	
GrP 4b	Bituminous Walk / Trail	(2)	sq. yd.	\$40.00	25,000	\$1,000,000	
GrP 4c	ADA Pedestrian Curb Ramp		each	\$1800.00	45	\$81,000	
GrP 5	Concrete Curb and Gutter		lin. ft.	\$27.00	16,850	\$454,950	
GrP 8a	Removals - Pavement		sq. yd.	\$6.00	28,100	\$168,600	
<b>SUBTOTAL PAVING AND GRADING COSTS:</b>						<b>\$2,487,300</b>	
<b>DRAINAGE, UTILITIES AND EROSION CONTROL</b>							
Dr 3	Water Quality Ponds & Other BMPs		l.s.			\$200,000	
Dr 5	Drainage - urban		l.s.			\$525,000	
Dr 6	Drainage - rural	(3)	mile	\$100,000	0.7	\$70,000	
Dr 7	Turf Establishment & Erosion Control		10%			\$253,000	
Dr 8	Landscaping & Site Amenities		10%			\$253,000	
<b>SUBTOTAL DRAINAGE, UTILITIES AND EROSION CONTROL</b>						<b>\$1,301,000</b>	
<b>RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS</b>							
RW 1	Box Culvert -	(4)	lin. ft.	\$1,000	20	\$20,000	
RW 32	Steel Sheet Pile Wall & Safety Fencing		sq. ft.	\$125	23,900	\$2,987,500	
<b>SUBTOTAL RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS:</b>						<b>\$3,007,500</b>	
<b>SIGNAL AND LIGHTING COSTS</b>							
SGL 1	Railroad Signal System Relocation/Upgrade on Bald Eagle Ave		each	\$250,000	1	\$250,000	
<b>SUBTOTAL SIGNAL AND LIGHTING COSTS:</b>						<b>\$250,000</b>	
<b>SIGNING &amp; STRIPING COSTS</b>							
SGN 1	Mainline Signing (C&D)		mile	\$17,500	4.2	\$73,500	
SGN 2	Mainline Striping		mile	\$2,500	4.2	\$10,500	
<b>SUBTOTAL SIGNING &amp; STRIPING COSTS:</b>						<b>\$84,000</b>	
<b>SUBTOTAL CONSTRUCTION COSTS:</b>						<b>\$7,129,800</b>	
<b>MISCELLANEOUS COSTS</b>							
M 1	Mobilization		10%			\$719,000	
M 2	Non Quantified Minor Items		10%			\$719,000	
M 7	Utility Relocation (Bald Eagle Ave & Hugo Road)		10%			\$719,000	
M 8	Traffic Control		3%			\$217,000	
<b>SUBTOTAL MISCELLANEOUS COSTS:</b>						<b>\$2,374,000</b>	
<b>ESTIMATED TOTAL CONSTRUCTION COSTS without Contingency:</b>						<b>\$9,503,800</b>	
1	Contingency or "risk"		25%			\$2,381,000	
<b>ESTIMATED TOTAL CONSTRUCTION COSTS PLUS CONTINGENCY:</b>						<b>\$11,884,800</b>	
<b>OTHER PROJECT COSTS:</b>							
<b>R/W ACQUISITIONS</b>			Lump Sum	\$223,000	1	\$223,000	
<b>TOTAL PROJECT COST</b>						<b>\$12,107,800</b>	
<b>INFLATION COST (CURRENT YR. TO YR. OF OPENING)</b>				Years	3%	\$1,970,000	
<b>TOTAL PROJECT COST (OPENING YEAR DOLLARS)</b>						<b>\$14,077,800</b>	

- NOTE:
- (1) Includes aggregate base class 5 and PASB or OGAB, as appropriate.
  - (2) Includes aggregate base class 5 and earthwork.
  - (3) Does not include pavement edge drains, see separate item.
  - (4) Does not include excavation or backfill.
  - (5) Assumes Mn/DOT Standard Plan Sheet designs; does not include excavation or backfill.
  - (6) Does not include Moment Slab.
  - (7) Includes moment slab and concrete barrier.

Utility Agreements based upon:  
R/W Acquisitions based upon:  
14577 SF P/E at \$15/SF and 869 SF T/E at \$5/SF for wall and grading along house side of Bald Eagle Boulevard

**FIGURE 20: Phase 2 Estimate for Two Way Alternative on Bald Eagle Boulevard, Two Way Alternative on Hugo Road**



## ENGAGEMENT SUMMARY

The project included a public engagement plan to engage the community and gather public input as the project worked to identify a preferred alignment for Phase 2 Extension of the Bruce Vento Regional Trail. The engagement plan included a pop-up event, three rounds of public open houses, two on-line surveys, an on-line comment map, a project video and a promotional campaign and project website.

The early phases of the engagement plan were geared to soliciting input from the public to inform the project decision-making and design processes.

A later phase of the plan where designed to inform the public of the preferred alternative(s) and gather further input on the design.

### Engagement Purpose and Objectives:

The Bruce Vento Regional Trail is envisioned as a 13.3-mile trail from the east side of downtown Saint Paul to the north County boundary in White Bear Township. This project is intended to complete a major gap in Metropolitan Council Regional Bike Transportation Network and the National US Bike Route 41 (USBR 41) by furthering the design process for the final 3.5 miles of the trail corridor.

The preferred conceptual route between Whitaker Street and County Road J primarily utilizes Bald Eagle Avenue, Bald Eagle Boulevard, Taylor Avenue and Hugo Road; an alternative route would traverse the White Bear Lake High School property and use a section of Division Street and Park Avenue to connect to Hugo Road. Ramsey County will work to incorporate input from residents and stakeholders with a racial equity lens to develop this critical link of regional trail to connect pedestrian and bicycle facilities in the County between Whitaker Street and County Road J. This is a challenging project with many traffic, design, utility, and stakeholder issues to work through. Some of the key objectives of this project included:

- Prepare schematic and preliminary plans to strengthen the County's chances for future grant requests.
- To obtain meaningful input from stakeholders living near the project corridor through highly interactive and engaging public involvement activities.
- Create high quality community spaces that promote user experiences for all people, all ages, and all

abilities.

- To provide a regional trail facility and site amenities that complement the areas natural resources and incorporate sustainable and resilient design.
- Provide trail connections to downtown White Bear Lake, the Highway 96 Regional Trail, the Lake Avenue Trail, and the White Bear Lake Marina.



Photo from White Bear Lake Market Fest Pop-up event, July 1, 2021.

Summaries of the Round 1, 2, and 3 engagement processes are included in the following sections and public engagement data, including a log of comments received, can be found in Appendices G through K.











# BRUCE VENTO REGIONAL TRAIL EXTENSION – PHASE 2 ROUND 1 & 2 ENGAGEMENT SUMMARY

## PROJECT DESCRIPTION

Ramsey County Parks & Recreation is looking to re-envision the Bruce Vento Regional Trail as a 13.3 mile trail from downtown Saint Paul to the county boundary in White Bear Township.

Ramsey County is working to design an extension of the Bruce Vento Regional Trail that will stretch the remaining 3.5 miles of the corridor between Whitaker Street and County Road J. This document provides a summary of community and stakeholder engagement that has helped to inform the initial design of the trail.

 <p><b>HOW DID WE GATHER FEEDBACK?</b></p>	<p>In the summer and fall of 2021, project partners hosted a pop-up event, virtual engagement, public open house and community outreach to present initial design concepts and gather community feedback. These in-person and virtual public input opportunities were promoted broadly, including mailed meeting notifications, social media promotion, a press release, email notifications, and updates to the project website to share digital copies of meeting materials.</p>
 <p><b>WHAT ENGAGEMENT OCCURRED?</b></p>	<ul style="list-style-type: none"> <li><b>135+</b> Engaged at WBL MarketFest pop-up event (July 1, 2021)</li> <li><b>2</b> virtual public meetings (lunch / evening ) on July 14, 2021</li> <li><b>90+</b> Attendance for public meeting on Oct. 21 2021</li> <li><b>135+</b> Survey responses. Survey open for public input from Oct. 21 - Nov. 10</li> <li><b>70+</b> Interactive map comments</li> </ul>
 <p><b>WHAT SUGGESTIONS AND DESIGN CONSIDERATIONS DID WE RECIEVE FROM THE COMMUNITY?</b></p>	<ul style="list-style-type: none"> <li>  <p><b>Pedestrian safety</b> concerns with trail crossing at Hwy 96.</p> </li> <li>  <p><b>Property and environmental impacts</b> are significant community concerns. Project partners have heard a range of opinions about the potential route of the Bruce Vento Trail extension. We are working to address these community concerns.</p> </li> <li>  <p>Suggested to <b>route trail on lakeside</b> of Bald Eagle Avenue.</p> </li> <li>  <p>Project <b>planned in coordination</b> with White Bear Lake, White Bear Township and White Bear Lake School District.</p> </li> <li>  <p>Suggestion to consider <b>potential one-way roadway</b> on portions of Bald Eagle Blvd. and/or Hugo Road.</p> </li> </ul>

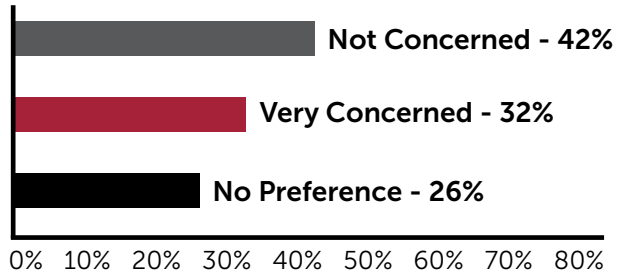




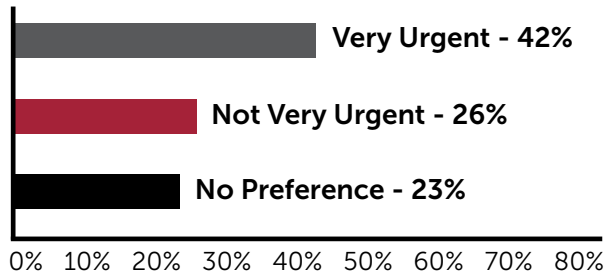
## WHAT DID WE HEAR?

In October 2021, Ramsey County conducted a survey to better understand community preferences for several design considerations for the Phase 2 Extension of the Bruce Vento Regional Trail. The survey received more than 135 responses and below is a summary of community feedback provided on the survey.

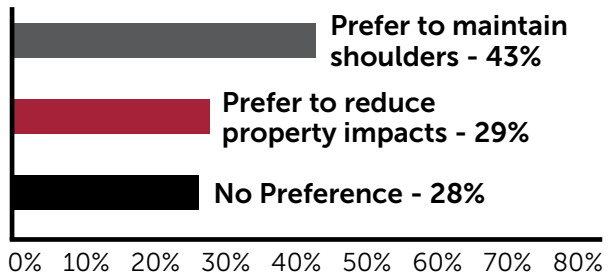
**1) Would you be concerned about the narrowing of Lincoln Ave. and the potential loss of on-street parking?**



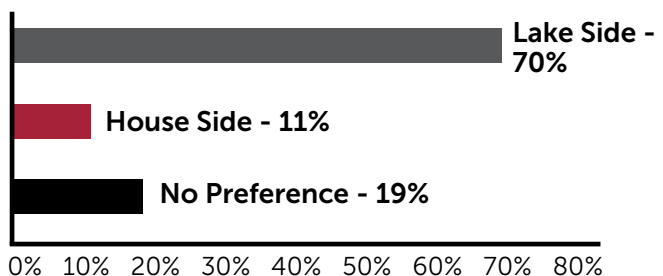
**2) In your opinion, if safe bicycle and pedestrian access can be accomplished with improvements at road-level, is a pedestrian bridge over Highway 96 still urgent?**



**3) How important is it that existing road shoulders north of the high school remain in place once the project is constructed?**



**4) Do you have a preference for which side of the road a trail would be located along Bald Eagle Blvd.?**

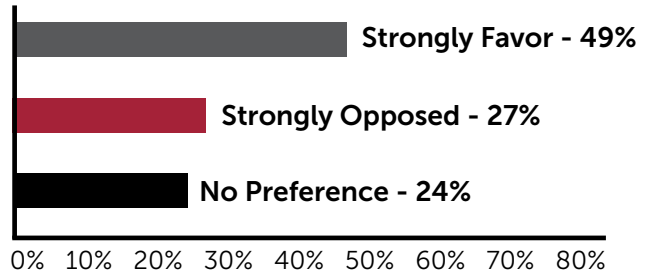




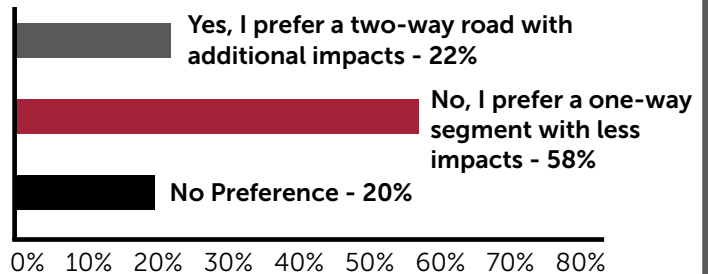


## WHAT DID WE HEAR?

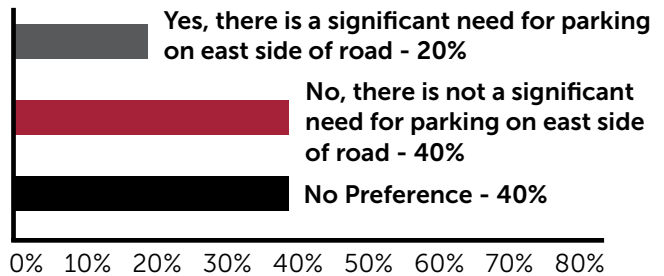
**5) How do you feel about converting a portion of Bald Eagle Blvd. to one-way? (Park Ave. to Taylor Ave.)**



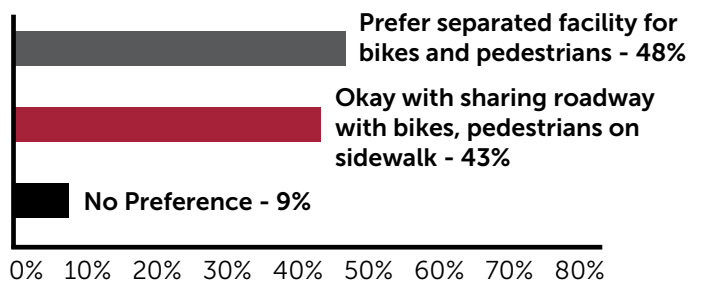
**6) Do you prefer that Bald Eagle Blvd. remain a two-way road with trail if that means impacts such as acquiring more road right of way to fit a trail will occur?**



**7) In your opinion, is there a significant need for parking on the east side of Hugo Road, between Taylor Ave. and the Bald Eagle picnic area?**



**8) How do you feel about sharing the roadway with bikes along Hugo Road, between Taylor Ave. and the Bald Eagle picnic area?**

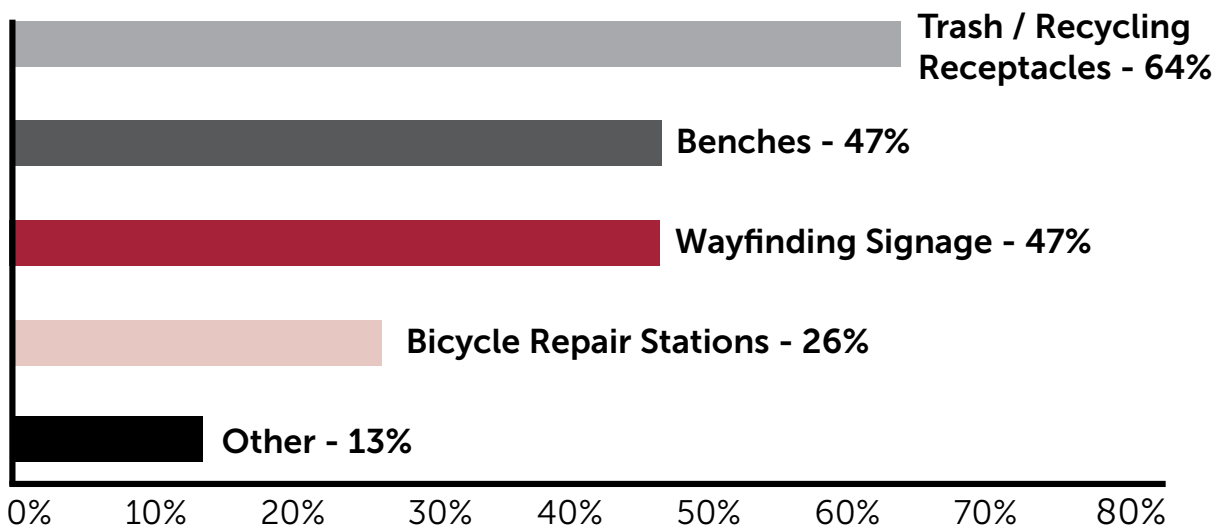






WHAT DID WE HEAR?

**9) What amenities would you like to see along the trail?  
Check all that apply.**





## Round 3 Final Survey Results by Geographic Location

In April and May of 2022, Ramsey County conducted an on-line survey to better understand community preferences for each segment of the proposed alignment for the Phase 2 Extension of the Bruce Vento Regional Trail. The survey received 190 responses and below is a question-by-question summary of community feedback provided

on the survey. The following tables provide a summary of survey responses based upon responses to the survey question "Where do you live in relation to the Bruce Vento Regional Trail?". A full summary of survey responses and comments received on the comment map are available in the appendix.

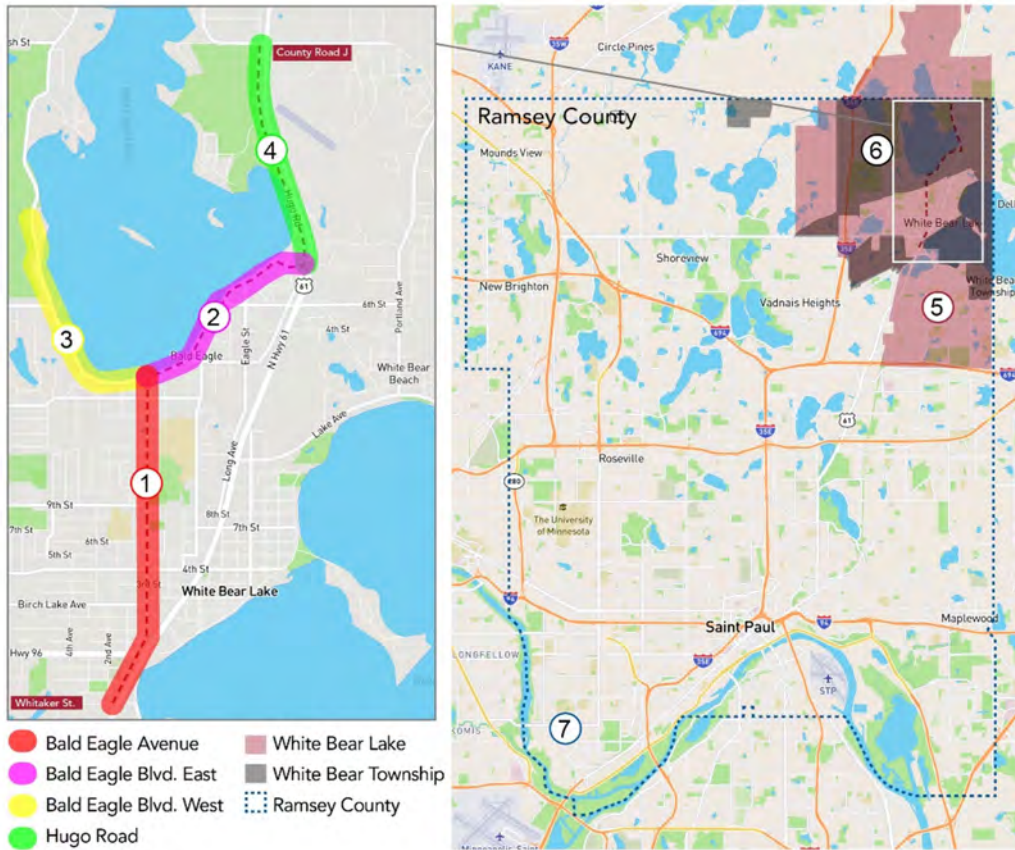


FIGURE 21: Phase 3 final survey map.

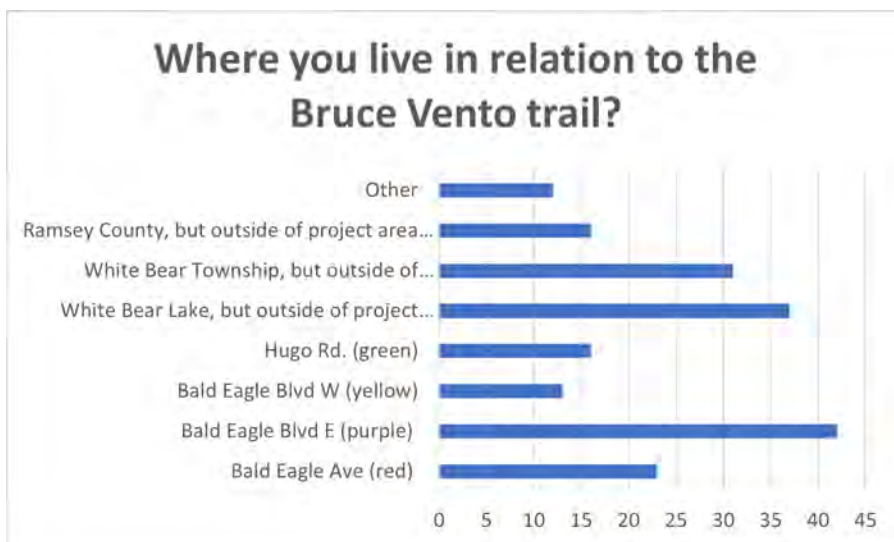


FIGURE 22: Phase 3 final survey results graph.

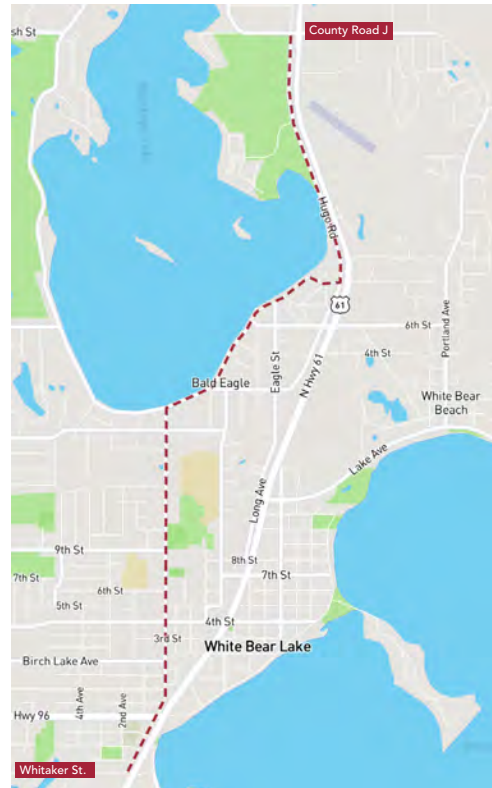


# BRUCE VENTO REGIONAL TRAIL EXTENSION – PHASE 3 ROUND 3 ENGAGEMENT SUMMARY

## PROJECT DESCRIPTION

Ramsey County Parks & Recreation is looking to re-envision the Bruce Vento Regional Trail as a 13.3 mile trail from downtown Saint Paul to the county boundary in White Bear Township.

Ramsey County has proposed a recommended alignment for the Phase 2 extension of the Bruce Vento Regional Trail, which will stretch the remaining 3.5 miles of the corridor between Whitaker Street and County Road J. This document provides a summary of community and stakeholder engagement conducted in the spring of 2022, to gather community feedback on the proposed alignment of the Phase 2 extension.



## WHAT ENGAGEMENT OCCURRED?



### Survey

April 20 – May 10  
190 community responses)



### Open House

April 21 at White Bear Lake City Hall  
90+ attendees



### Project Overview Video

[www.youtube.com/watch?v=fTcOo1YMpzU&t=5s](https://www.youtube.com/watch?v=fTcOo1YMpzU&t=5s)



### Comment Map

260 community comments

## Survey Analytics:

**190** survey responses

**71%** of responses from 55110 Zip Code

**79%** of responses identify as White/Caucasian

**17%** of responses identify as BIPOC



Visit the project website to learn more  
and share your feedback:

[ramseycounty.us/VentoPhase2](https://ramseycounty.us/VentoPhase2)



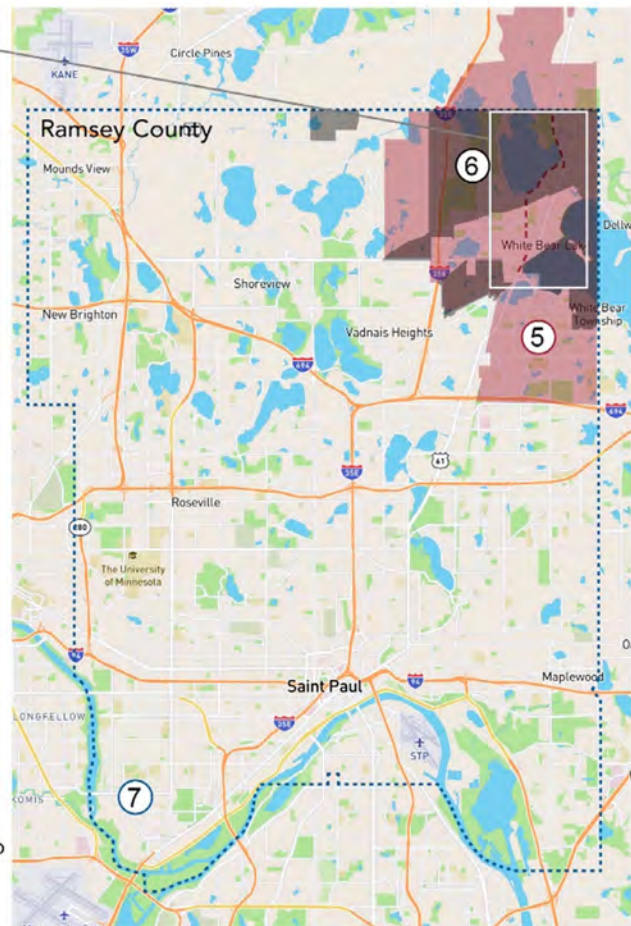
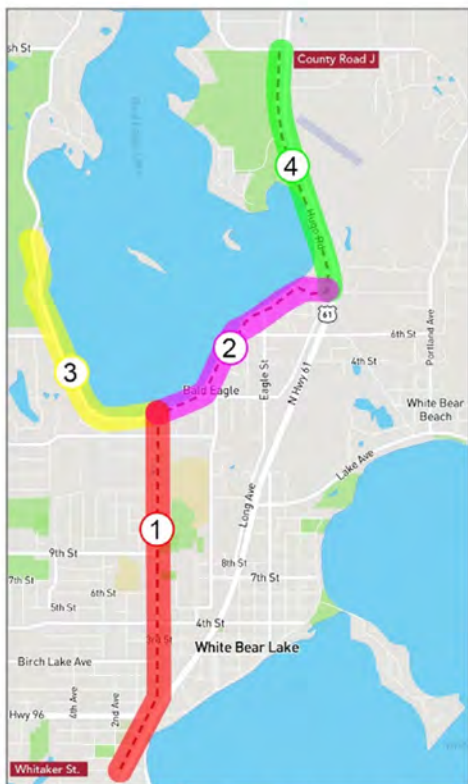
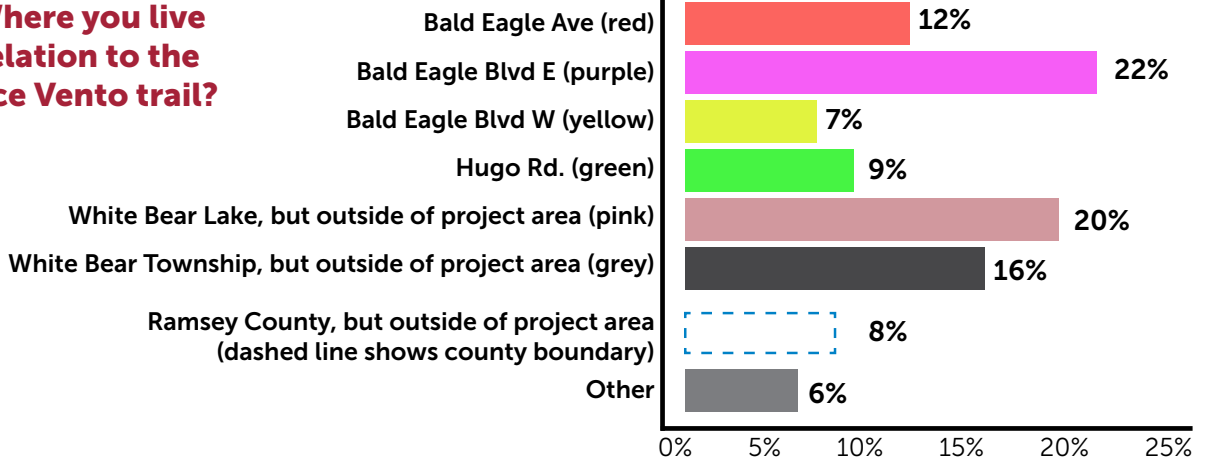




## WHAT DID WE HEAR?

In April and May of 2022, Ramsey County conducted an online survey to better understand community preferences for each segment of the proposed alignment for the Phase 2 Extension of the Bruce Vento Regional Trail. The survey received 190 responses and below is a question by question summary of community feedback provided on the survey.

### 1) Where you live in relation to the Bruce Vento trail?

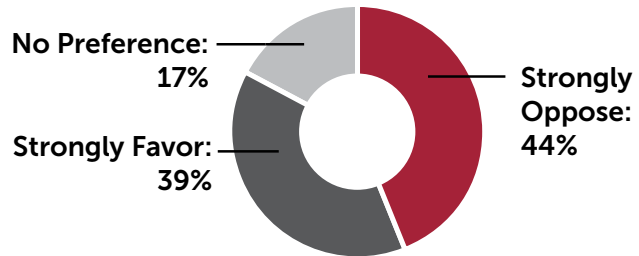




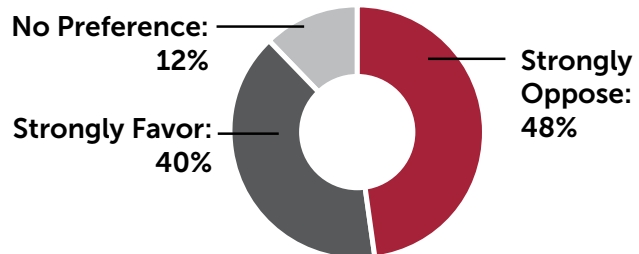


## WHAT DID WE HEAR?

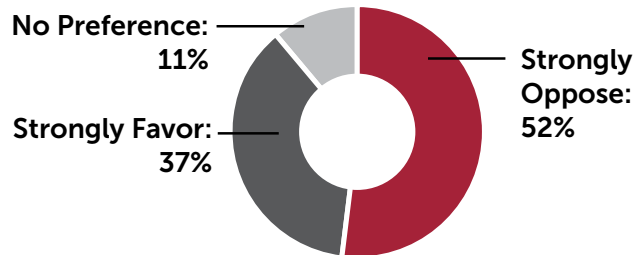
### 2) What is your level of support for the proposed alignment of Lincoln Avenue?



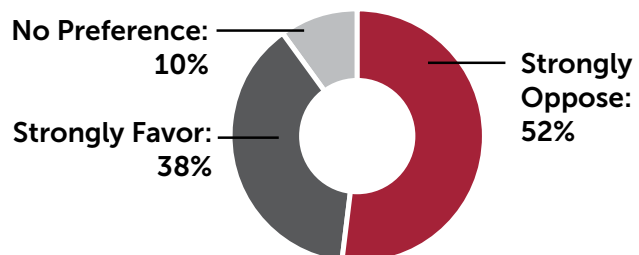
### 3) What is your level of support for the proposed alignment of Bald Eagle Avenue (Hwy 96 to White Bear Lake High School)?



### 4) What is your level of support for the proposed alignment of Bald Eagle Avenue (White Bear Lake High School to Bald Eagle Lake)?



### 5) What is your level of support for the proposed alignment of Bald Eagle Blvd E to Park Avenue?

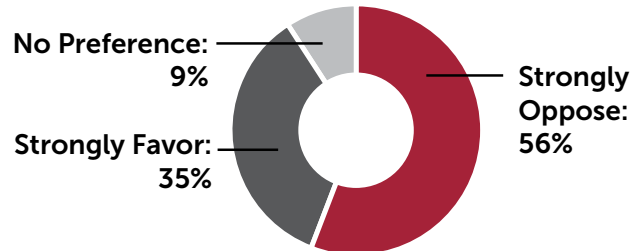




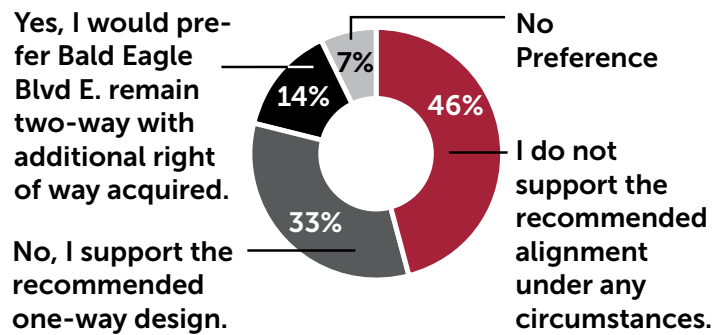


WHAT DID WE HEAR?

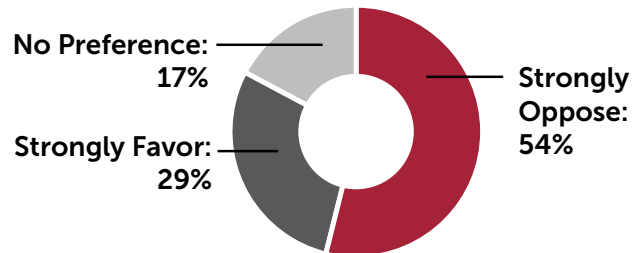
6) What is your level of support for the proposed alignment from Bald Eagle Blvd E from Park Avenue to Hugo Road?



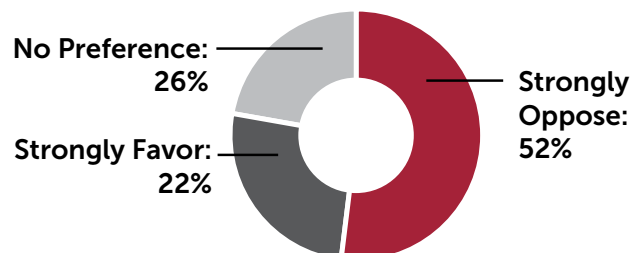
7) Would you prefer that Bald Eagle Blvd E remain a two-way road with an added trail, even if that means impacts such as acquiring more road right of way to fit a trail?



8) Hugo Road one-way - What is your level of support for the proposed alignment from the intersection of Taylor Avenue/Hugo Road to County Road J?



9) Hugo Road two-way - What is your level of support for the proposed alignment from the intersection of Taylor Avenue/Hugo Road to County Road J?





## NEXT STEPS

### Agency Engagement

As the project moves forward with a more detailed design it is important to maintain continued coordination between agency stakeholders and policymakers, including:

- White Bear Township
- City of White Bear Lake
- Rice Creek Watershed District
- Vadnais Lake Area Watershed Management Organization
- White Bear Lake High School

### Design Advancement and Recommendation

The project completed design for the preferred alignment with one-way roadway on Hugo Road at the pinch point and the preferred alignment with two-way roadway on Hugo Road at the pinch point to a 15-20% level (Alignment, Profile, Typical Sections, Preliminary Sections) as the preferred design scenarios at this time. The one-way alternate for Bald Eagle Boulevard between Park Avenue and Hugo Road was advanced as it has the least grading and disruption of vegetation along the lake. By minimizing added impervious surfaces, it also is the least impactful alternative relative to stormwater management. Future design efforts will provide additional detail and specificity to the project. Some of the likely future design efforts include the following:

- Affirm alignment alternatives for Bald Eagle Boulevard E, Taylor Avenue, and Hugo Road.
- Further discussions with White Bear Township regarding road turnback associated with the one-way vs. two-way alternatives on Bald Eagle Blvd and Hugo Road.
- Further assessment of an alternate alignment through White Bear Lake High School to address resident comments about the trail along Bald Eagle Blvd. This includes evaluation of route through the High school property, along Division Ave. and Park Ave. up to Hugo Road.
- Additional consideration of the design of the proposed Lincoln Ave. cul-de-sac

- Additional refinement of grading and construction limits
- Advance design details
- Utility coordination
- Permitting and advancement of stormwater design with watersheds, local agencies and White Bear Lake High School
- Discussion of maintenance and operations of the trail and supporting infrastructure
- Complete preliminary design and final design
- Updated cost estimates
- Project specifications

### Public Engagement

Through this project, it has been clear that the affected communities are very interested in this project and want to be engaged as the project moves forward. In particular, additional engagement is suggested to address resident questions about preferred alignment as the design progresses. Additional engagement is suggested in part because of evolving public opinion of the trail that was observed in the Round 2 and Round 3 surveys.

### Additional Railroad Coordination

The project will require additional railroad coordination as the project moves forward. Specific items to coordinate include the following:

- Additional coordination with BNSF Railroad regarding potential impacts to embankment and right of way along Hugo Road
- Additional coordination with BNSF Railroad regarding at-grade crossing at Highway 96 in conjunction with MnDOT intersection improvements
- Final Design coordination with Canadian Pacific Railroad regarding trail crossing along Bald Eagle Avenue



# APPENDIX

**Appendix A – Traffic Memorandum**

**Appendix B – Bald Eagle Blvd Cross-Section Analysis**

**Appendix C – Pedestrian Bridge Alternatives**

**Appendix D – Drainage Memorandum**

**Appendix E - Plans and Sections**

**Appendix F - Cost Estimates**

**Appendix G- Public Engagement Plan**

**Appendix H - Sample Survey #1**

**Appendix I - Sample Survey #2**

**Appendix J - Compiled Public Comment Log**

**Appendix K - GARE Worksheet**

**Appendix L – Bruce Vento Regional Trail Planning Timeline**



## **Appendix A -- Traffic Memorandum**





**To:** Scott Yonke, Director of Planning and Development  
Ramsey County Parks

**From:** Mike McGarvey, ASLA, PLA, Director  
Tom Sachi, PE, Project Manager

**Date:** June 28, 2022

**Subject:** Bruce Vento Trail Traffic Study

## Introduction

An evaluation was completed to determine the traffic impacts of constructing an extension of the Bruce Vento Trail on Bald Eagle Boulevard and Taylor Avenue in White Bear Lake, MN along the southeast shore of Bald Eagle Lake. A high-level traffic study was conducted to evaluate the impacts of the various trail designs on daily traffic volumes and services to the surrounding neighborhood.

## Design Impacts and Data Collection

Multiple roadway designs were considered for adding the bike trail extension. The designs include:

- Keeping both lanes of vehicle traffic and using right-of-way to add a trail on either the east or west side of the roadways.
- Closing the northbound/eastbound vehicle lane and adding the trail on either the east or west side of the roadways.
- Closing the southbound/westbound vehicle lane and adding a bike lane on either the east or west side of the roadways.

Year 2016 and 2019 daily traffic volumes were provided by the Minnesota Department of Transportation (MnDOT) for roadways within the study area and are shown in Figure 1. Various impacts to driver behavior, civil services, and/or private properties are associated with each design.

## Keeping Both Vehicle Lanes

In this design, daily vehicle traffic patterns are not expected to change, but there are considerable impacts to properties. On the east side of Bald Eagle Boulevard, the land slopes upward, resulting in topographical impacts. This configuration would require cutting into the slope to add a trail and likely would involve building retaining walls for most of the length along roadway. Walkway stairs leading to the homes would also need to be replaced for many of the homes. On the south side of Taylor Avenue, utility poles would need to be relocated.





**Existing Traffic Volumes**

Bruce Vento Trail Extension  
 City of White Bear Lake, Minnesota

02014110  
 June 2021

Figure 1



On both streets, mailboxes and trash pickup would require that a small boulevard be constructed between the roadway and the trail, or these items would need to be moved to the west side of the street, causing the post office and waste services to alter their routes and the homeowners to cross the street to retrieve their mail and waste bins. This relatively high level of impact and potential need to acquire more public right of way to accomplish this design, as well as massive disruption to the corridor (tree removal, extensive grading, etc.) contribute to this being a less desirable alternative.

On the west side of Bald Eagle Boulevard, the land slopes downward toward Bald Eagle Lake. In some parts it is a gradual decline and in others it is a steep drop-off less than one foot from the edge of the road. Land would have to be filled in and then held in place with a retaining wall for most of the length along the roadway. Multiple homeowners' stairways leading to their docks would need to be replaced. On the northside of Taylor Avenue, mailboxes and trash pickup would either be between the roadway and the trail on a newly constructed boulevard, or moved to the south side of the street, causing the post office and waste services to alter their routes and the homeowners to cross the street to retrieve their mail and waste bins.

### **Closing Northbound and Eastbound Lanes**

In this design, vehicle traffic is changed with the closure of the northbound lane on Bald Eagle Boulevard and the eastbound lane on Taylor Avenue. The forecasted impacts on the daily vehicle traffic volumes are shown in Figure 2. As shown, the bulk of the traffic is expected to divert to Park Avenue and Hugo Road. It is not expected that there will be any significant traffic operational impacts from the lane closure as daily volumes are expected to be well below the daily thresholds for two-lane roadways.

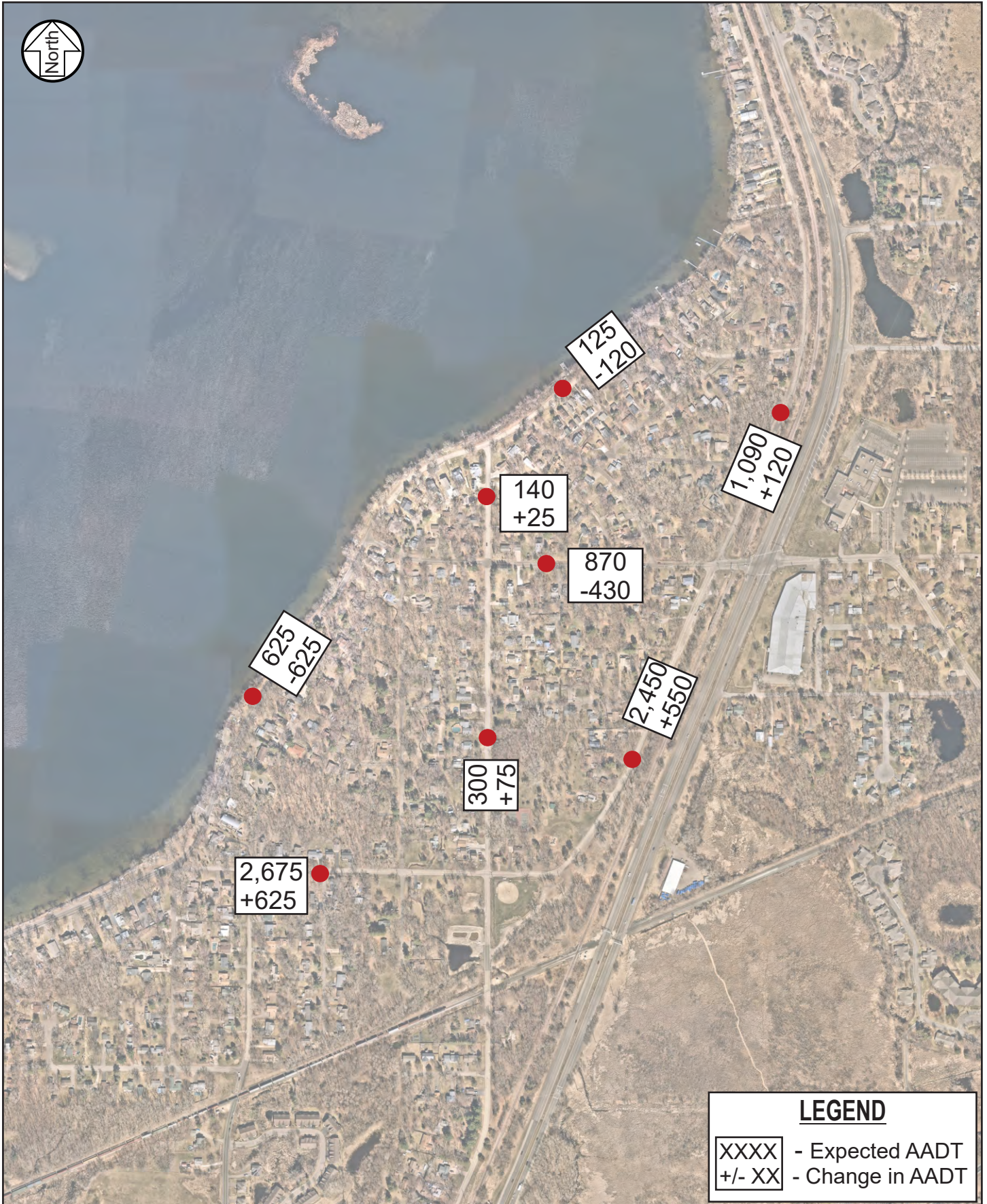
With the lane closures, mailboxes and trash pickup would need to be moved to the west side of Bald Eagle Boulevard and the north side of Taylor Avenue in order to be accommodated by postal and waste service vehicles. Limiting the roadways to one-way traffic can also negatively impact the response times of emergency services going to some of the homes in the design area, as they will now have slightly longer routes to these homes, depending on location.

### **Closing Southbound and Westbound Lanes**

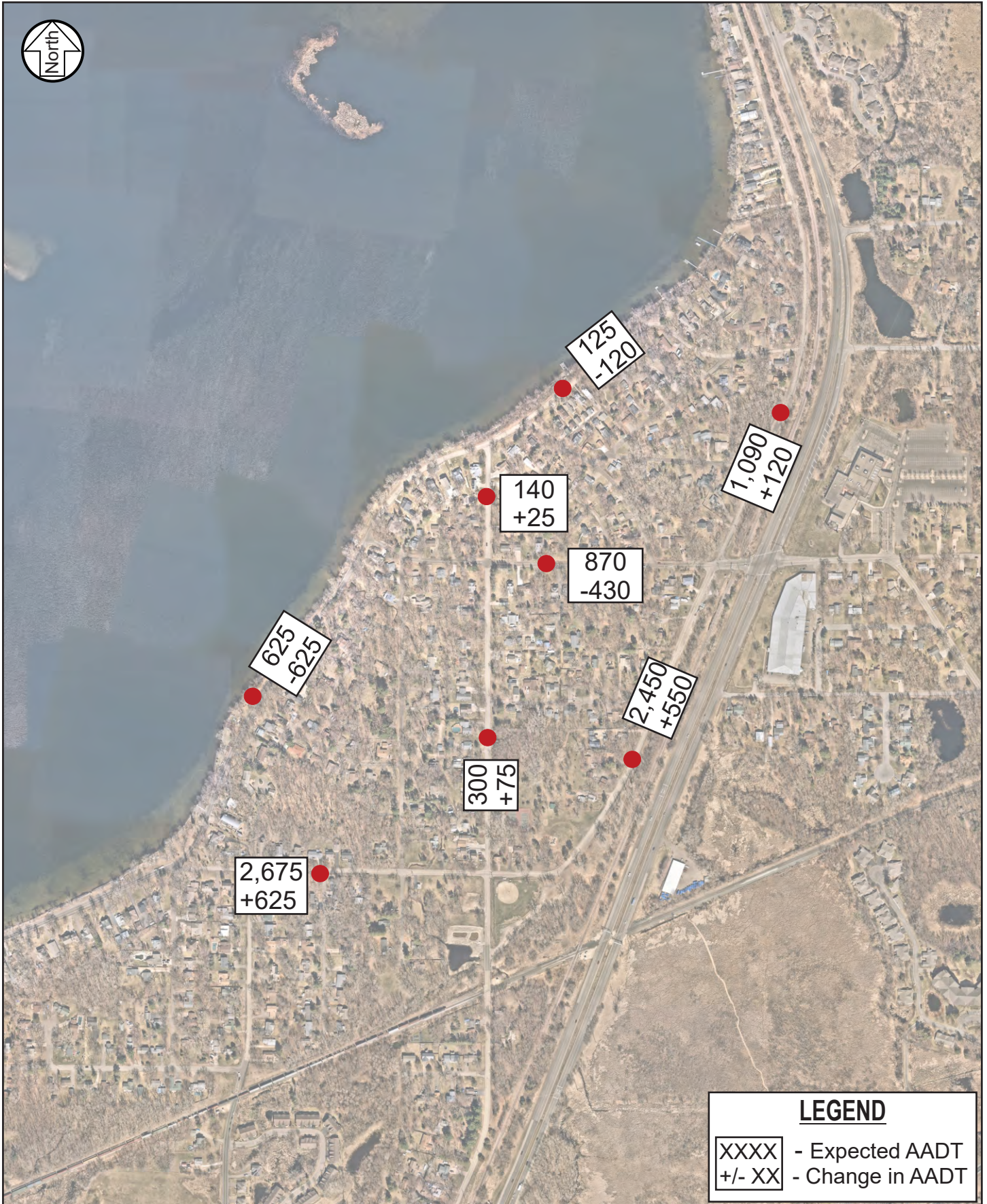
In this design, vehicle traffic is changed with the closure of the southbound lane on Bald Eagle Boulevard and the westbound lane on Taylor Avenue. The forecasted impacts on the vehicle traffic are shown in Figure 3. As shown, the bulk of the traffic is expected to divert to Park Avenue and Hugo Road. It is not expected that there will be any significant traffic operational impacts from the lane closure as daily volumes are expected to be well below the daily thresholds for two-lane roadways.

With the lane closures, mailboxes and trash pickup would need to be moved to the south side of Taylor Avenue in order to comply with the postal and waste service vehicles. Limiting the roadways to one-way traffic may also negatively impact the response times of emergency services going to some of the homes in the design area, as they will now have slightly longer routes to these homes. This alternative would appear to least impactful to adjacent properties.











**Appendix B – Bald Eagle Blvd Cross-Section Analysis**







**Alternatives Reviewed**

- TS1 2 way ideal, Lake side
- TS2 2 way ideal, House side
- TS3 2 way minimum, Lake side
- TS4 2 way minimum, House side
- TS5 1 way ideal, House side
- TS6 1 way minimum, House side
- TS7 1 way minimum, Lake side

**Blocks Reviewed**

B1	Bald Eagle Avenue to Park Avenue	189+00	200+00
B2	Park Avenue to Short Street	201+00	213+00
B3	Short Street to Buffalo Street	214+00	219+00
B4	Buffalo Street to Eagle Street	220+00	226+00
B5	Eagle Street to Williams Avenue	227+00	229+00
B6	Williams Avenue to Shadyside Lane	230+00	238+00
B7	Shadyside Lane to Hoxie Avenue	239+00	241+00
B8	Hoxie Avenue to Hugo Road	242+00	244+00

**Questions**

Feas?	Constructable as drawn? Does grading work?	Y = reasonable cross section, N = excessive grading (into lake or house)
R/W?	Is trail (with CZ) contained within existing ROW?	Y = Trail in ROW, N = Extends beyond Ex ROW
Wall?	Could a wall make trail feasible?	Y = Makes trail feasible (May still have lake access or other impacts), N = wall not necessary
PE?	Would PE be required to build XS?	Y = PE required for trail + CZ, N = PE not required
TE?	Would TE be required to build XS?	Y = TE required for grading, N = TE not required

Station	Drive	Walk	Lake Access	TS1 - TWO WAY IDEAL, LAKE SIDE							TS2 - TWO WAY IDEAL, HOUSE SIDE							TS3 - TWO WAY MIN, LAKE SIDE							TS4 - TWO WAY MIN, HOUSE SIDE							TS5 - ONE WAY IDEAL, HOUSE SIDE							TS6 - ONE WAY MIN, HOUSE SIDE							TS7 - ONE WAY MIN, LAKE SIDE						
				Feas?	R/W?	Wall?	PE?	TE?	Other	Feas?	R/W?	Wall?	PE?	TE?	Other	Feas?	R/W?	Wall?	PE?	TE?	Other	Feas?	R/W?	Wall?	PE?	TE?	Other	Feas?	R/W?	Wall?	PE?	TE?	Other	Feas?	R/W?	Wall?	PE?	TE?	Other	Feas?	R/W?	Wall?	PE?	TE?	Other							
220+00	RT		LT	N	N	Y	Y	Y	IN LAKE	Y	Y	N	N	N		Y	Y	N	N	Y		Y	Y	N	N	N		Y	Y	N	N	N		Y	Y	N	N	N		Y	Y	N	N	N								
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228+00			LT	N	N	Y	Y	Y	IN LAKE	N	Y	Y	Y	Y	BUILDING	N	Y	Y	N	N	IN LAKE	N	Y	Y	N	N	BUILDING	N	Y	Y	N	N		Y	Y	N	Y	Y		N	Y	Y	N	N	IN LAKE							
229+00			LT	N	Y	Y	Y	Y	IN LAKE	Y	Y	N	N	N		N	Y	Y	N	N	IN LAKE	Y	Y	N	N	N		Y	Y	N	N	N		Y	Y	N	N	N		N	Y	Y	N	N	IN LAKE							
230+00		RT	LT	N	Y	Y	Y	Y	IN LAKE	Y	N	N	Y	Y		N	Y	Y	N	N	IN LAKE	Y	Y	N	N	Y		Y	Y	N	N	N		Y	Y	N	N	N		N	Y	Y	N	N	IN LAKE							
231+00	RT		LT	N	Y	Y	Y	Y	IN LAKE	Y	N	N	Y	Y		N	Y	Y	N	N	IN LAKE	Y	Y	N	N	Y		Y	Y	N	N	N		Y	Y	N	N	N		N	Y	Y	N	N	IN LAKE							
232+00		RT	LT	N	Y	Y	Y	Y	IN LAKE	N	N	Y	Y	Y	N	Y	Y	N	N	IN LAKE	N	Y	Y	Y	Y	N	Y	Y	N	N		Y	Y	Y	N	N		N	Y	Y	N	N	IN LAKE									
233+00			LT	N	Y	Y	Y	Y	IN LAKE	N	N	Y	Y	Y	N	Y	Y	N	N	IN LAKE	N	Y	Y	N	Y	Y	Y	Y	N	Y	Y		Y	Y	N	Y	Y		N	Y	Y	N	N	IN LAKE								
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235+00	RT		LT	N	Y	Y	Y	Y	IN LAKE	Y	N	N	Y	Y		N	Y	Y	N	N	IN LAKE	Y	Y	N	N	N		Y	Y	N	N	N		Y	Y	N	N	N		N	Y	Y	N	N	IN LAKE							
236+00			LT	N	Y	Y	Y	Y	IN LAKE	Y	N	N	Y	Y		N	Y	Y	N	N	IN LAKE	Y	Y	N	N	N		Y	Y	N	N	N		Y	Y	N	N	N		N	Y	Y	N	N	IN LAKE							
237+00				N	Y	Y	Y	Y	IN LAKE	Y	Y	N	N	N		N	Y	Y	N	N	IN LAKE	Y	Y	N	N	N		X	X	X	X	X		X	X	X	X	X		X	X	X	X	X								
238+00				Y	N	N	Y	Y		Y	N	N	Y	Y		Y	Y	N	N	N		Y	Y	N	N	Y		X	X	X	X	X		X	X	X	X	X		X	X	X	X	X								
239+00		RT		Y	Y	N	N	N		Y	N	N	Y	Y		Y	Y	N	N	N		Y	N	N	Y	Y		X	X	X	X	X		X	X	X	X	X		X	X	X	X	X								
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242+00				Y	N	N	Y	Y		Y	N	N	Y	Y		Y	Y	N	N	N		Y	N	N	Y	Y		X	X	X	X	X		X	X	X	X	X		X	X	X	X	X								
243+00		LT		Y	N	N	Y	Y		Y	N	N	Y	Y		Y	Y	N	N	N		Y	N	N	Y	Y		X	X	X	X	X		X	X	X	X	X		X	X	X	X	X								
244+00		LT, RT		Y	Y	N	N	Y		Y	N	N	Y	Y		Y	Y	N	N	Y		Y	N	N	Y	Y		X	X	X	X	X		X	X	X	X	X		X	X	X	X	X								

**Summary**

- TS1 Walls required majority of segment, difficult to meld this with existing lake access, however no impacts on the house side
- TS2 Some walls but not pervasive, maintains existing lake access improvements, appears to fit OK
- TS3 Only a slight improvement from TS1
- TS4 A slight improvement from TS2 but still requires walls, reduced wall benefit doesn't outweigh loss of grassy boulevard
- TS5 Eliminates most walls from TS2, easements still required, implements one way and jurisdictional transfer
- TS6 Improves TS5 but still walls and easements
- TS7 Some improvement from TS1 and TS3 but same issues largely remain, could be worth presenting for NIMBY reaction

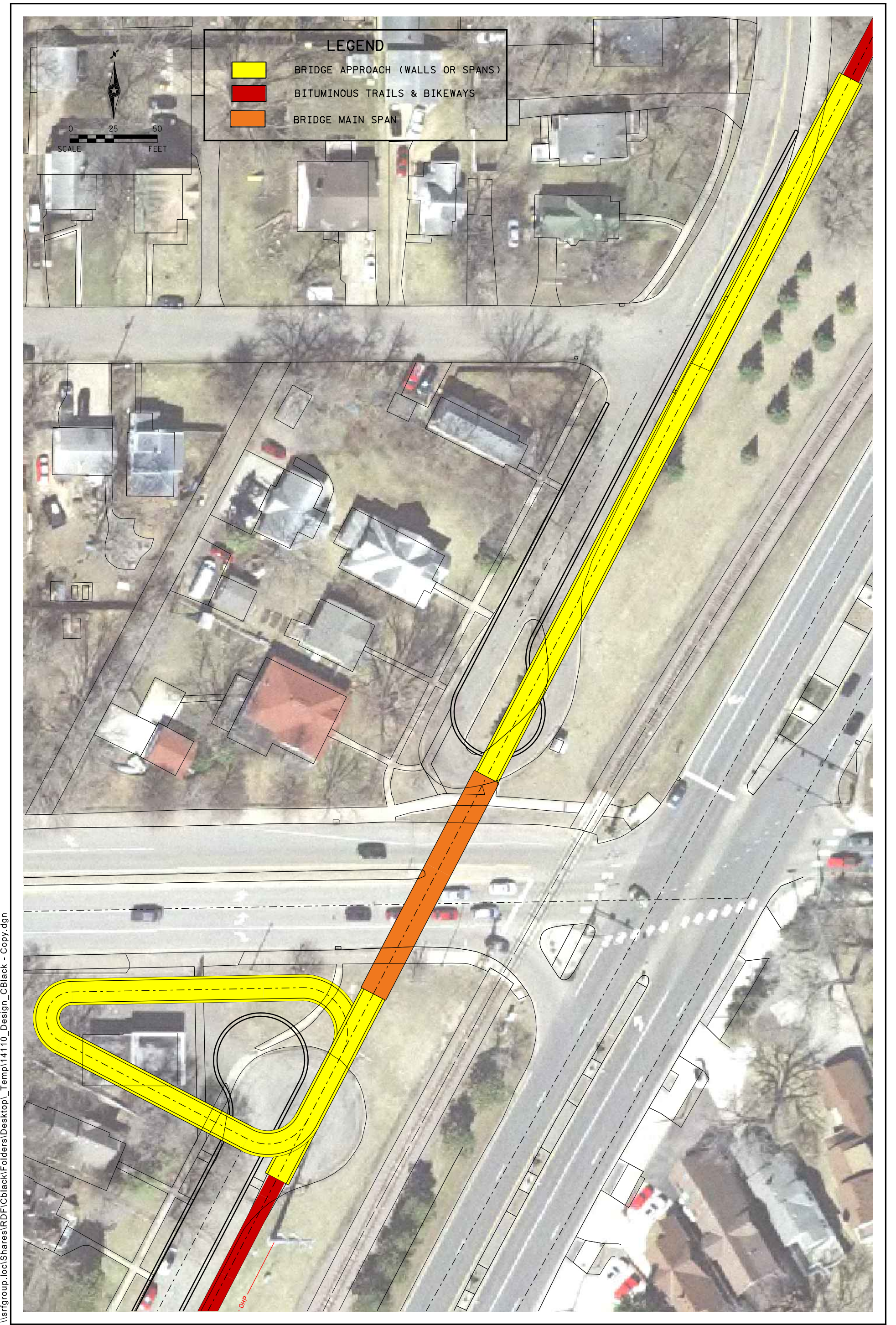
**Recommendations**

Present TS2, TS5 and TS7 as alternatives for public comment  
 Don't see reason (at this point) that high school alignment needs to be shared



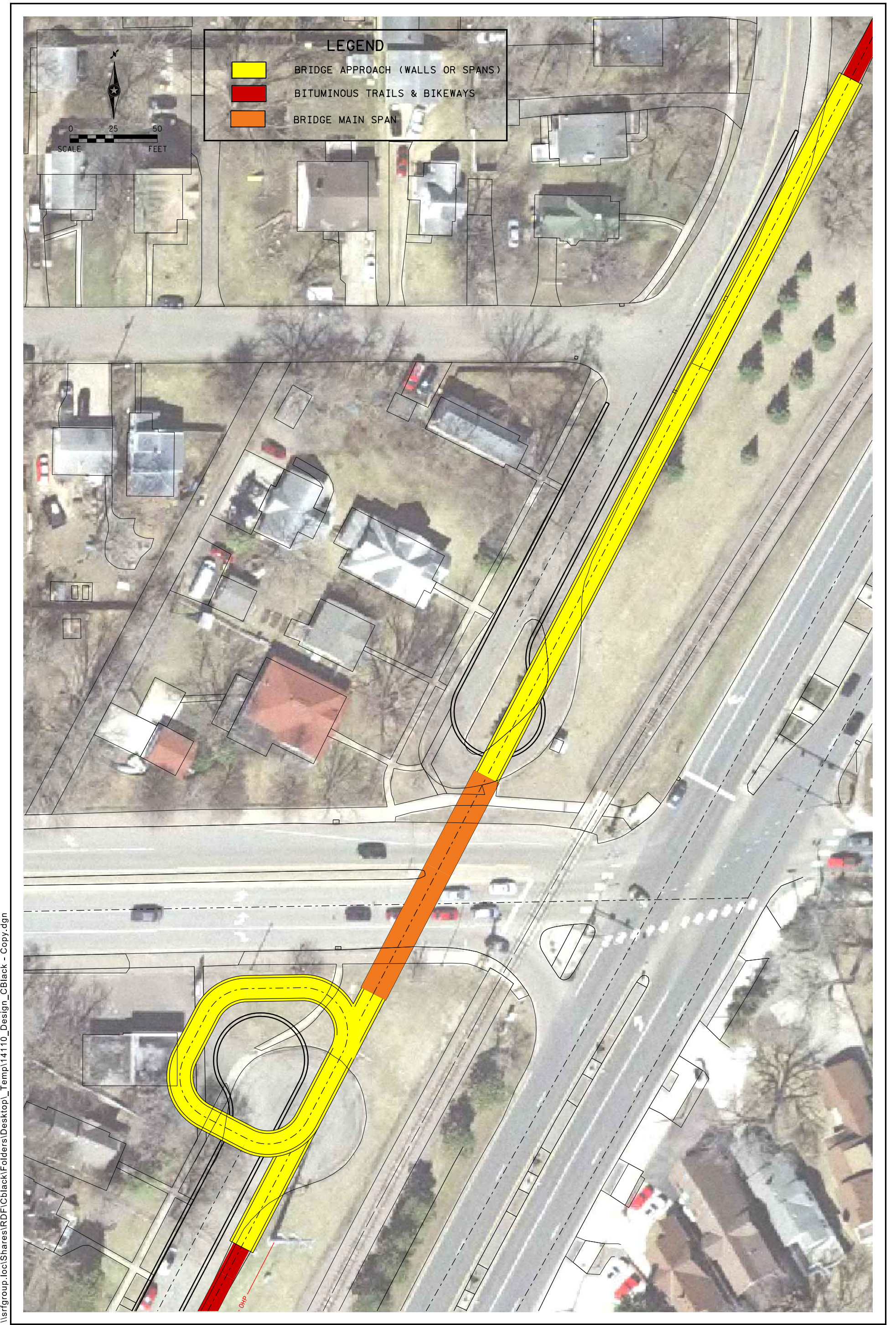
**Appendix C – Pedestrian Bridge Alternatives**





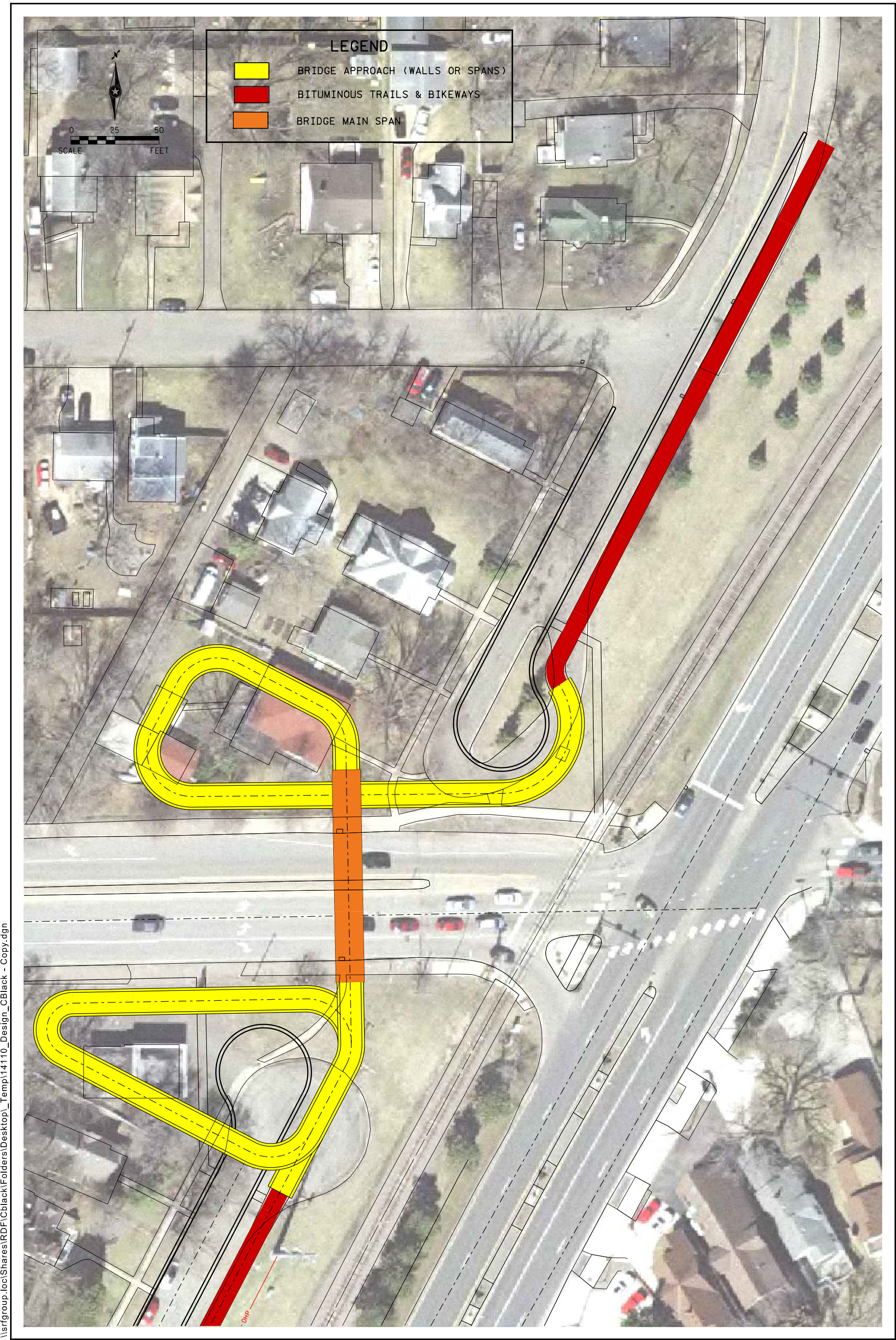
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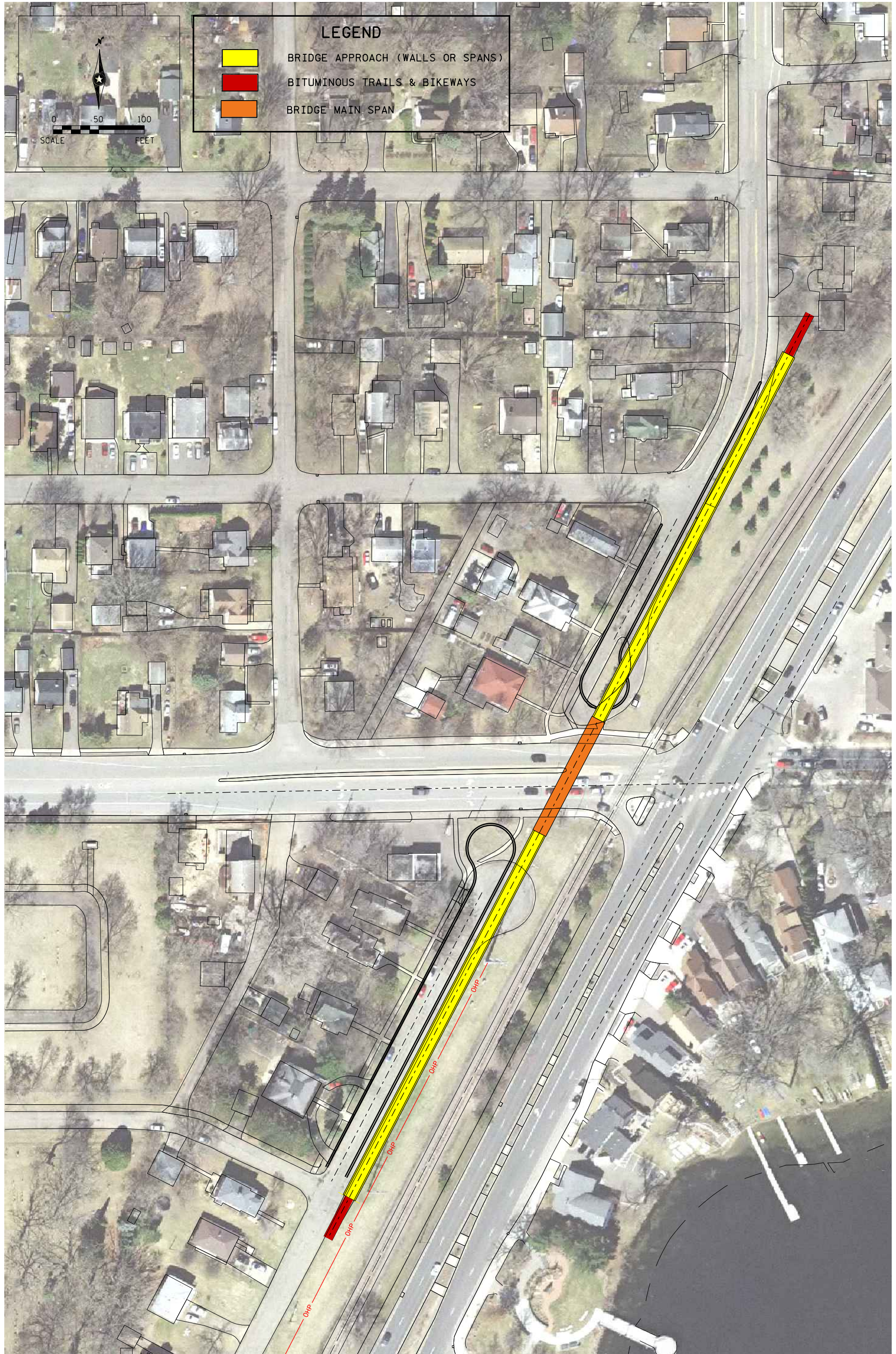




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**Appendix D – Drainage Memorandum**



SRF No. 14110

**To:** Scott Yonke  
Ramsey County

**From:** Jacques DuVal, PE  
David Filipiak, PE

**Date:** June 13, 2022

**Subject:** Bruce Vento Trail (Phase 2) Preliminary Drainage Design - Technical Memorandum

## 1.0 Purpose & Background

SRF Consulting Group, Inc. was retained by Ramsey County to provide technical assistance in preparing the preliminary drainage design for the Bruce Vento Trail (Phase 2) project, which will provide trail facilities in the City of White Bear Lake. This project will also include other modifications to the corridor including converting sections of Bald Eagle Boulevard and Hugo Road to one-way streets.

This memorandum documents the preliminary drainage design of the stormwater treatment system that will support the proposed roadway improvements and meet the current requirements of the regulatory agencies. The Drainage Overview Map (DOM) in Appendix A includes the preliminary drainage design elements of this project.

## 2.0 Data Collection

The following data was reviewed during the preliminary design:

- Topography and planimetrics – Provided by Ramsey County
- Bald Eagle Boulevard record plans – Provided by Ramsey County
- Hugo Road record plans – Provided by White Bear Township
- Storm sewer map and GIS data – Provided by City of White Bear Lake
- Contour data – From SRF survey and LiDAR data
- Soil data – From the United States Department of Agriculture (USDA) Web Soil Survey
- Drinking water supply management area (DWSMA) – From the Minnesota Department of Health (MDH).
- Flood Insurance Rate Maps (FIRMs) – From the Federal Emergency Management Agency (FEMA) and confirmed by Rice Creek Watershed District.



### 3.0 Regulatory Environment

The project corridor is located within the City of White Bear Lake and White Bear Township. It is also within two watershed organizations, Rice Creek Watershed District (RCWD) and Vadnais Lake Area Watershed Management Organization (VLAWMO). The city is the local government unit (LGU) for water resources-related permitting within the VLAWMO, but defaults to VLAWMO's standards. The project is designed to meet requirements as they currently are defined in the RCWD Rules and VLAWMO Water Management Policy. The next phase of design is not anticipated to begin for several years, during which time the regulatory environment may change and should be reviewed prior to progressing design. The regulatory information included in this technical memorandum addresses the stormwater management and floodplain permitting but does not include wetland permitting information.

Runoff from the corridor flows to Lambert Creek and Bald Eagle Lake. Coordination has taken place with the RCWD and VLAWMO and will continue through final design. Additional coordination will be required with the city on stormwater management during the final design phase.

Water resource related permits required at this time include:

- City of White Bear Lake – Stormwater Management and Erosion and Sediment Control Permits
- RCWD – Stormwater Management and Erosion and Sediment Control Permits
- VLAWMO – The City is the LGU for water resources related permits, but the VLAWMO will be included on all submittals.
- Minnesota Pollution Control Agency (MPCA) – National Pollutant Discharge Elimination System (NPDES) Construction Stormwater General Permit, and the associated Stormwater Pollution Prevention Plan (SWPPP).

The City of White Bear Lake, RCWD, VLAWMO, and the MPCA NPDES permit guide the stormwater management approach for this project. Appendix B summarizes the stormwater requirements.

#### 3.1 Rate Control

Runoff from the project corridor is routed via storm sewer and drainage ditches ultimately discharging to the receiving waters noted above. RCWD and VLAWMO require discharge rates for the 2-, 10-, and 100-year storm events to be less than or equal to existing peak discharge rates using the Atlas 14 rainfall magnitudes. Existing and proposed HydroCAD models were developed to determine the scale of rate increases within the limits of disturbance. Only the impervious areas within the limits were considered at this stage. Major outflow points are identified on the Drainage Overview Map located in Appendix A, HydroCAD modeling summaries are in Appendix C and a summary of discharge rates are included in Appendix D. In general:

- Where BMPs are proposed, it is expected that they will provide attenuation for the increased rates, however this will need to be confirmed with more detailed modeling during final design.



- At Highway 96 the downstream storm sewer system will need to be reviewed to confirm if it has capacity for the slight increase in rates or if surcharging within the storm sewer structures is acceptable. If it does not, an underground rate control BMP may be required.
- BMPs are not proposed along Bald Eagle Lake. Additional coordination with the watershed will be required to confirm if the increase in rates to the lake is acceptable, or if rate control measures will be required.

### **3.2 Volume Control**

The proposed project will also be required to meet the City, RCWD, VLAWMO, and the NPDES volume control requirements as shown in Appendix E. RCWD and NPDES requires treating the volume of runoff equal to 1.0-inch over the new impervious area or 0.5-inch over the new and reconstructed impervious area, whichever is greater, while the City and VLAWMO requires treating the volume of runoff equal to 1.1-inch over the new impervious area or 0.55-inch over the new and reconstructed impervious area, whichever is greater. Where feasible, infiltration is encouraged in both RCWD and VLAWMO. NPDES only counts infiltrated volume towards their requirements. VLAWMO calculates volume control credits based on guidance provided by the MN Stormwater Manual. A soils investigation will be conducted to determine the soil types and estimated groundwater levels within the proposed treatment locations. Information from USDA's Web Soil Survey is included in Appendix F.

### **3.3 Water Quality**

RCWD and the NPDES Construction permit have water quality requirements for projects with one or more acres of new or reconstructed impervious surface. RCWD requires that Total Suspended Solids (TSS) shall be removed to the maximum extent practical. Additionally, they require adequate pretreatment measures to removed sediment upstream of biofiltration/infiltration practices. The NPDES permit requirement is to treat the water quality volume equivalent to 1" of runoff from the new impervious surface in a permanent BMP if infiltration is infeasible. Filtration systems must be designed to remove at least 80% of TSS.

### **3.4 Groundwater**

Groundwater elevations will be confirmed with soil borings along the corridor. Due to proximity to Bald Eagle Lake and several wetlands, groundwater elevations are expected to be shallow in some locations.

Portions of the Bruce Vento Trail (Phase 2) project are located within a Drinking Water Supply Management Area (DWSMA), which require further review for the use of infiltration practices. Areas adjacent to the railroad crossing at Bald Eagle Boulevard, near Eagle St by Bald Eagle Lake, and the southernmost portion of the project are in high-risk zones.



### **3.5 Floodplains**

Floodplain boundaries were identified within the project corridor by reviewing Federal Emergency Management (FEMA) Flood Insurance Rate Maps (FIRMs) and confirmed by RCWD. The only mapped boundary is at Bald Eagle Lake. While there is the potential for some modifications at existing outlets to the lake, no impacts to the floodplain are anticipated with this project. See Appendix G for FIRMs.

## **4.0 Preliminary Drainage Design**

The project can essentially be split into two by watershed, VLAWO to the south of the high school and RCWD to the north. This section provides a detailed discussion of the two areas.

### **4.1 Bruce Vento Trail (Phase 2) - VLAWMO**

#### **4.1.1 Existing Conditions**

Lincoln Avenue, from Whitaker Street to Highway 96, is a mix of rural and urban roadway. Project runoff drains to adjacent grassy areas, with the low point in the road draining into a low-lying residential property. The low-lying area appears to drain away via a culvert and will eventually reach Lambert Creek via an existing buried 96" RCP that runs from 5<sup>th</sup> Street in White Bear Lake and Ditch 13. Bald Eagle Avenue is primarily urban with several storm sewer networks picking up runoff water and connecting to the city storm sewer system, ultimately discharging to the Ditch 13 system.

#### **4.1.2 Proposed Conditions**

This project will add pedestrian facilities adjacent to Lincoln Avenue starting at the intersection with Whitaker Street and continuing along Bald Eagle Avenue until the watershed terminus near the White Bear Lake High School. Lincoln Avenue will be fully reconstructed in conjunction with the trail work, and Bald Eagle Avenue will have some new or reconstructed curb and gutter along the east side of the road.

Several potential BMP locations have been identified, as shown in Appendix A. In several locations, the proposed BMPs will take on existing impervious areas, in addition to the proposed impervious, to meet the water quality volume requirements. Basin W1, south of Lincoln Avenue, is sized to provide water quality volume for Lincoln Avenue and is expected to provide for rate control. Basin B1, just north of Highway 96, is expected to provide water quality volume and rate control for the Birch Lake Avenue Trunk and Highway 96 area. Basin B2 will require coordination with the White Bear Lake Area High School. Pond B3, adjacent to the railroad, is intended to be a dry or wet pond a wet pond for rate control and water quality.

Additionally, from the intersection of Bald Eagle Avenue and 4<sup>th</sup> Street to Bald Eagle Boulevard and Park Ave, the project will follow State Aid requirements. In this region new storm sewer will be



needed where curb and gutter is added, and a spread analysis will be required to confirm if new inlets are required in the areas with existing curb and gutter.

## **4.2 Bruce Vento Trail (Phase 2) - RCWD**

### **4.2.1 Existing Conditions**

Within RCWD, Bald Eagle Avenue is primarily rural, with some segments of existing curb and gutter. From 12<sup>th</sup> St to Ridgewood Avenue, the runoff drains towards a series of wetlands and low-lying areas around the SOO Line railroad crossing. These wetlands drain to Bald Eagle Lake through a judicial ditch crossing under Bald Eagle Boulevard between Short Street and Buffalo Street.

North of Ridgewood Avenue. There are some short segments of storm sewer pipe that connect runoff from Bald Eagle Avenue, Bald Eagle Boulevard, and Taylor Avenue discharging directly to Bald Eagle Lake.

Hugo Road is a rural roadway with ditches on both sides of the road. These ditches drain to Bald Eagle Lake through a judicial ditch crossing approximately 700 feet north of Taylor Avenue, a handful of drop inlet connections, or directly to the lake near the Bald Eagle Park and Boat Launch. In the park, trails drain to a series of wetlands, ultimately discharging to the lake.

### **4.2.2 Proposed Conditions**

New curb and gutter will be added to Bald Eagle Avenue, from 12<sup>th</sup> Street to Bald Eagle Boulevard, and Bald Eagle Boulevard, Bald Eagle Avenue to Park Avenue. Along Bald Eagle Boulevard, from Park Avenue to Taylor Avenue, and Taylor Avenue, from Bald Eagle Boulevard to Hugo Road, the northern lane will be converted to trail, with new curb and gutter to the remaining one-way route. Bald Eagle Boulevard has recently had a mill and overlay project, so the project intends to minimize disturbance to only what is needed for the new trail.

Hugo Road has two alternatives: the preferred alternative, where eastern lane will be converted to trail with some bump outs for parking. This alternative is the basis for the computations in this Report. The other alternative would maintain two-way traffic along Hugo Road but would require a wall for several hundred feet to provide space for the trail at pinch point with the railroad to the east. In the Preferred Alternative, the one way will stop at a proposed cul-de-sac near the entrance to the southern parking lot for the Bald Eagle Lake Park and Boat Launch, which will also allow access to the new one-way portion of Hugo Road. South of the proposed cul-de-sac the trail will cross and into the park and add or reconstruct existing trail north until it terminates at N County Line Road E.

Pond B3 will receive runoff from Bald Eagle Avenue and will require coordination with the White Bear Lake Area High School. The pond is located adjacent to the railroad and is intended to be a dry or wet pond for rate control and water quality.



No BMPs are proposed along Bald Eagle Boulevard, Taylor Avenue, and Hugo Road due to the limited space within the public right of way. Furthermore, much of the proposed work along the lake is considered exempt from water quality requirements. Either the proposed work does not impact the roadway's underlying aggregate base or the trail is 10 feet wide or less with a vegetated down gradient at least 5 feet wide, both of which are also exempt. There is potential for structural pretreatment devices to be added at existing discharge points along the lake to provide some water quality benefits, as shown in Appendix A. Exact locations will need to be worked out in final design.

Along Hugo Road new storm sewer will be required to reach existing ditch systems. Ditches on the east side will require regrading to maintaining positive drainage adjacent to the railroad. Within the Bald Eagle Lake Park and Boat Launch, two possible BMPs are shown. Basin H2 could provide for all the water quality volume needs within RCWD but Basin H1 was left as a potential BMP.

## 5.0 Next Steps

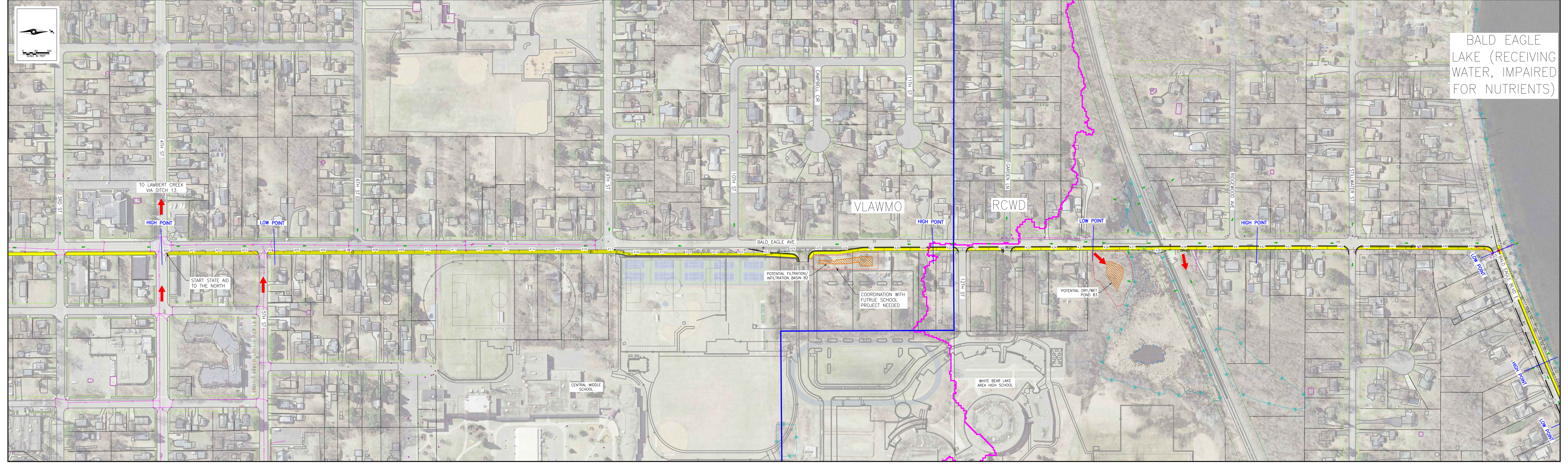
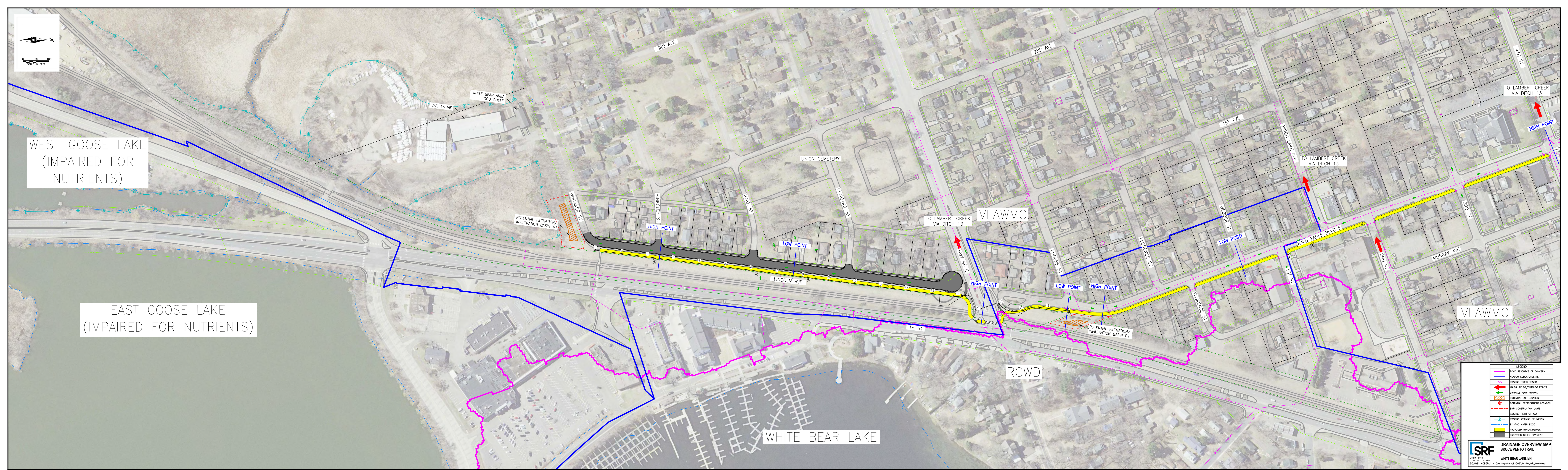
Through collaboration with the project stakeholders throughout the preliminary design process, further important issues and next steps in the design process have been identified. These include the following:

- The project will need to confirm soil conditions by conducting soil borings at the BMP locations.
- Confirm wetland impacts once wetland survey and report are available.
- Obtain additional survey and storm sewer information and refine as needed.
- Confirm proposed BMPs can reduce rates by refining the HydroCAD modeling.
- Review capacity of downstream systems where BMPs are not proposed. If increased rates are not acceptable confirm that oversized storm sewer can be used to reduce rates.
- Coordinate with the White Bear Lake Area High School on potential BMPs.
- Review regulatory environment at time of final design for changes to the requirements
- Confirm impervious calculations are still valid, particularly the definition of reconstructed impervious

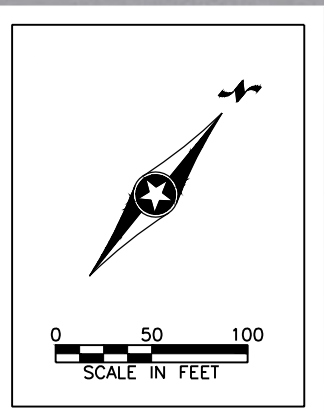


## **Appendix A – Drainage Overview Map**

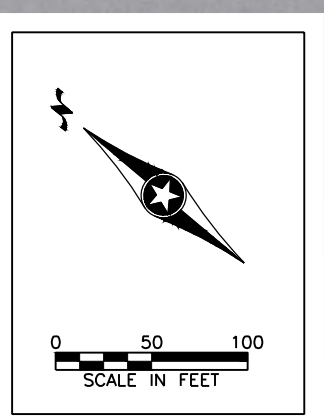
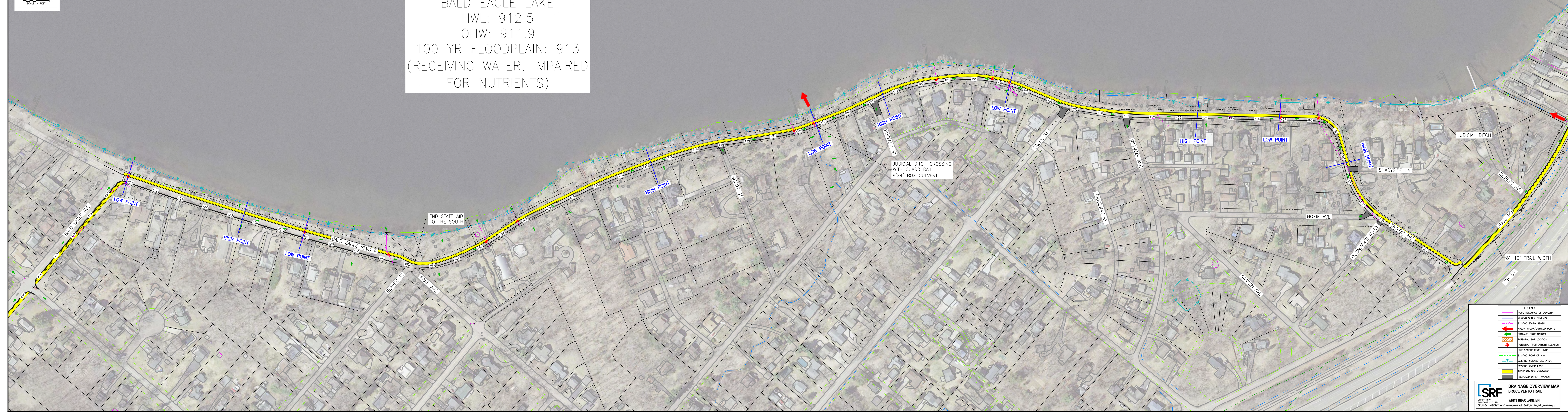








BALD EAGLE LAKE  
 HWL: 912.5  
 OHW: 911.9  
 100 YR FLOODPLAIN: 913  
 (RECEIVING WATER, IMPAIRED  
 FOR NUTRIENTS)



BALD EAGLE LAKE  
 HWL: 912.5  
 OHW: 911.9  
 100 YR FLOODPLAIN: 913  
 (RECEIVING WATER, IMPAIRED  
 TO NUTRIENTS)





## **Appendix B – Regulatory Requirements**



ENTITY	SURFACE WATER RATES	SURFACE WATER QUANTITY	SURFACE WATER QUALITY
<p><b>Minnesota Pollution Control Agency via the NPDES Permit Program</b></p> <ul style="list-style-type: none"> <li>▪ Required for construction activity that disturbs greater than 1 ac.</li> </ul>	<ul style="list-style-type: none"> <li>▪ N/A</li> </ul>	<ul style="list-style-type: none"> <li>▪ Abstraction of the first 1 inch of rainfall from the site’s new impervious surface or 0.5 inch of rainfall from the site’s new and fully reconstructed impervious surface, whichever is greater.</li> <li>▪ Pretreatment must be used before filtration/infiltration.</li> </ul>	<ul style="list-style-type: none"> <li>▪ Water quality volume is equal to 1 inch of runoff from the new impervious surfaces.</li> <li>▪ Permanent pond volume of 1800 CF/acre of storage below the outlet pipe.</li> <li>▪ Water quality volume maximum discharge shall be no more than 5.66 cfs per acre of pond surface area at the water quality volume.</li> <li>▪ Filtration systems must be designed to remove at least 80% of TSS.</li> </ul>
<p><b>Rice Creek Watershed District</b></p> <ul style="list-style-type: none"> <li>▪ Required for public linear projects that create or reconstruct 1 acre or more impervious.</li> <li>▪ Sidewalks and trails 10’ or narrower with 5’ plus width downstream vegetated strip are exempt from stormwater permit requirements.</li> </ul>	<ul style="list-style-type: none"> <li>▪ No increase for 2-, 10- and 100-year storm peak discharge rate.</li> </ul>	<ul style="list-style-type: none"> <li>▪ Greater of 1.0 inch of runoff from new impervious surfaces OR 0.5 in of runoff from new and reconstructed impervious surfaces.</li> <li>▪ For a public linear project, treatment volume for reconstructed impervious must be provided only to the extent feasible.</li> <li>▪ If infiltration is feasible, on-site or regional BMPs must provide volume control. If not feasible, any BMP may be used.</li> <li>▪ Compensatory storage required if filling more than 100 cu yards into a floodplain.</li> </ul>	<ul style="list-style-type: none"> <li>▪ Runoff from all new and reconstructed impervious surface must be captured and treated for total phosphorus if feasible. Undisturbed impervious may be treated in lieu of disturbed pavement if draining to the same Resource of Concern</li> <li>▪ For all untreated surfaces, TSS must be removed to the maximum extent practical.</li> <li>▪ Biofiltration/infiltration measures require adequate pretreatment measures to remove sediment upstream of the BMP.</li> </ul>
<p><b>City of White Bear Lake</b></p> <ul style="list-style-type: none"> <li>▪ The City is the regulator on stormwater management, however, it follows the same requirements as the Vadnais Lake Area Watershed Management Organization</li> <li>▪ Permit required for 10,000 square feet or more of new or reconstructed impervious.</li> </ul>	<ul style="list-style-type: none"> <li>▪ No increase for 2-, 10- and 100-year storm peak discharge rate.</li> </ul>	<ul style="list-style-type: none"> <li>▪ Greater of 1.1 inches of runoff from net increase in impervious area OR 0.55 inch of runoff from new and fully reconstructed impervious surfaces.</li> <li>▪ Infiltration is encouraged</li> </ul>	<ul style="list-style-type: none"> <li>▪ Volume control credits calculated per the Minnesota Stormwater Manual</li> </ul>



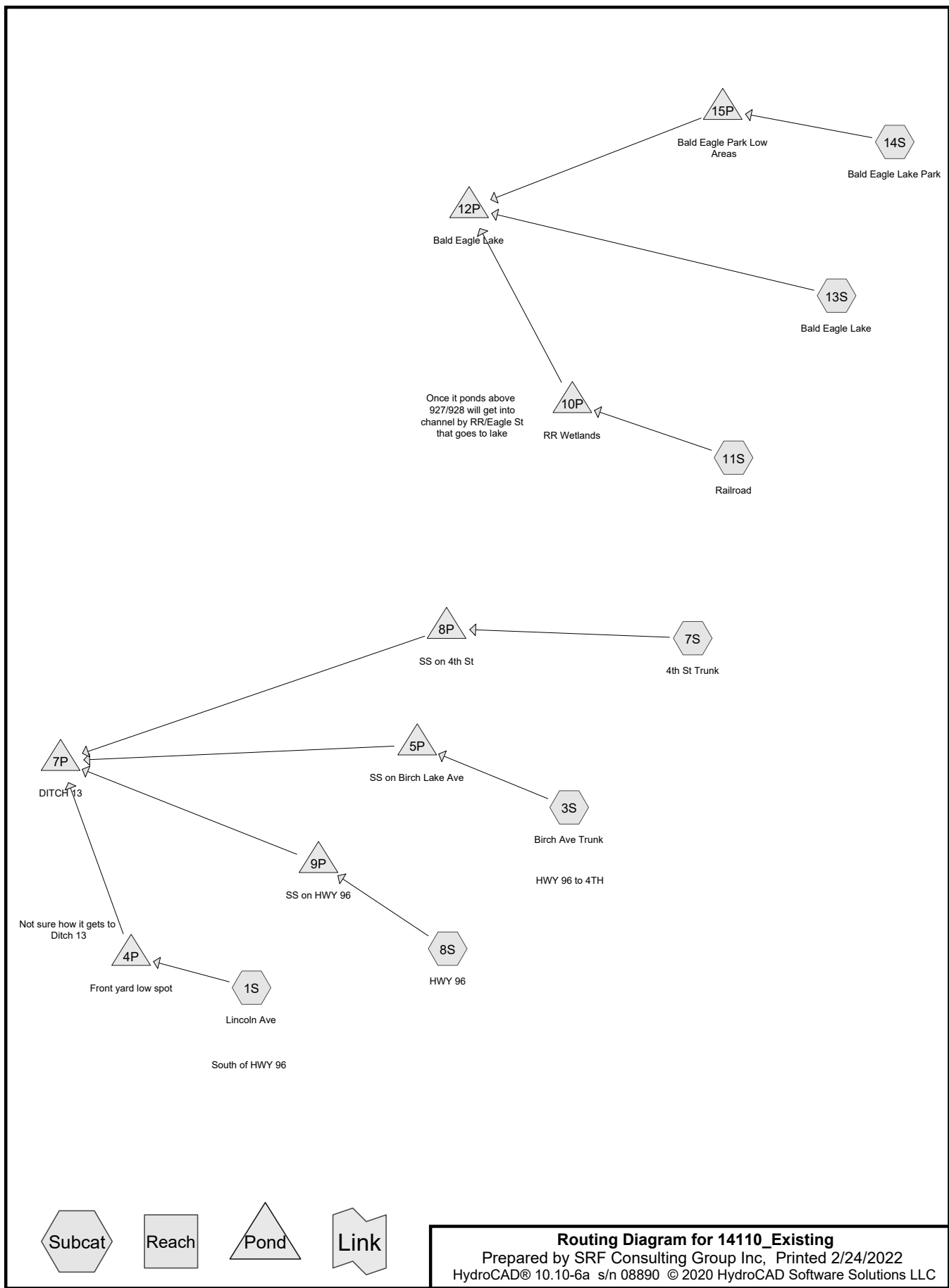
ENTITY	SURFACE WATER RATES	SURFACE WATER QUANTITY	SURFACE WATER QUALITY
<b>Vadnais Lake Area Watershed Management Organization</b> <ul style="list-style-type: none"> <li>▪ Permit required for 10,000 square feet or more of new or reconstructed impervious.</li> </ul>	<ul style="list-style-type: none"> <li>▪ No increase for 2-, 10- and 100-year storm peak discharge rate.</li> </ul>	<ul style="list-style-type: none"> <li>▪ Greater of 1.1 inches of runoff from net increase in impervious area OR 0.55 inch of runoff from new and fully reconstructed impervious surfaces.</li> <li>▪ Infiltration is encouraged</li> </ul>	<ul style="list-style-type: none"> <li>▪ Volume control credits calculated per the Minnesota Stormwater Manual</li> </ul>

**Reconstructed Impervious: The removal of an impervious surface such that the underlying structural aggregate base is effectively removed, and the underlying native soil exposed.**



## **Appendix C – HydroCAD Model Reports**







**14110\_Existing**

Prepared by SRF Consulting Group Inc

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*MSE 24-hr 3 2-Year Rainfall=2.81"*

Printed 2/24/2022

Page 2

Time span=0.00-48.00 hrs, dt=0.01 hrs, 4801 points  
 Runoff by SCS TR-20 method, UH=SCS, Weighted-CN  
 Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

<b>Subcatchment1S: Lincoln Ave</b>	Runoff Area=1.290 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=5.95 cfs 0.277 af
<b>Subcatchment3S: Birch Ave Trunk</b>	Runoff Area=0.440 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=2.03 cfs 0.095 af
<b>Subcatchment7S: 4th St Trunk</b>	Runoff Area=0.450 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=2.07 cfs 0.097 af
<b>Subcatchment8S: HWY 96</b>	Runoff Area=0.030 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=0.14 cfs 0.006 af
<b>Subcatchment11S: Railroad</b>	Runoff Area=0.210 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=0.97 cfs 0.045 af
<b>Subcatchment13S: Bald Eagle Lake</b>	Runoff Area=0.430 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=1.98 cfs 0.092 af
<b>Subcatchment14S: Bald Eagle Lake Park</b>	Runoff Area=0.180 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=0.83 cfs 0.039 af
<b>Pond 4P: Front yard low spot</b>	Inflow=5.95 cfs 0.277 af Primary=5.95 cfs 0.277 af
<b>Pond 5P: SS on Birch Lake Ave</b>	Inflow=2.03 cfs 0.095 af Primary=2.03 cfs 0.095 af
<b>Pond 7P: DITCH 13</b>	Inflow=10.19 cfs 0.475 af Primary=10.19 cfs 0.475 af
<b>Pond 8P: SS on 4th St</b>	Inflow=2.07 cfs 0.097 af Primary=2.07 cfs 0.097 af
<b>Pond 9P: SS on HWY 96</b>	Inflow=0.14 cfs 0.006 af Primary=0.14 cfs 0.006 af
<b>Pond 10P: RR Wetlands</b>	Inflow=0.97 cfs 0.045 af Primary=0.97 cfs 0.045 af
<b>Pond 12P: Bald Eagle Lake</b>	Inflow=3.78 cfs 0.176 af Primary=3.78 cfs 0.176 af
<b>Pond 15P: Bald Eagle Park Low Areas</b>	Inflow=0.83 cfs 0.039 af Primary=0.83 cfs 0.039 af

**Total Runoff Area = 3.030 ac Runoff Volume = 0.651 af Average Runoff Depth = 2.58"**  
**0.00% Pervious = 0.000 ac 100.00% Impervious = 3.030 ac**



**14110\_Existing**

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MSE 24-hr 3 10-Year Rainfall=4.19"

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Page 3

Time span=0.00-48.00 hrs, dt=0.01 hrs, 4801 points  
 Runoff by SCS TR-20 method, UH=SCS, Weighted-CN  
 Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

<b>Subcatchment1S: Lincoln Ave</b>	Runoff Area=1.290 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=8.92 cfs 0.425 af
<b>Subcatchment3S: Birch Ave Trunk</b>	Runoff Area=0.440 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=3.04 cfs 0.145 af
<b>Subcatchment7S: 4th St Trunk</b>	Runoff Area=0.450 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=3.11 cfs 0.148 af
<b>Subcatchment8S: HWY 96</b>	Runoff Area=0.030 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=0.21 cfs 0.010 af
<b>Subcatchment11S: Railroad</b>	Runoff Area=0.210 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=1.45 cfs 0.069 af
<b>Subcatchment13S: Bald Eagle Lake</b>	Runoff Area=0.430 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=2.97 cfs 0.142 af
<b>Subcatchment14S: Bald Eagle Lake Park</b>	Runoff Area=0.180 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=1.25 cfs 0.059 af
<b>Pond 4P: Front yard low spot</b>	Inflow=8.92 cfs 0.425 af Primary=8.92 cfs 0.425 af
<b>Pond 5P: SS on Birch Lake Ave</b>	Inflow=3.04 cfs 0.145 af Primary=3.04 cfs 0.145 af
<b>Pond 7P: DITCH 13</b>	Inflow=15.29 cfs 0.728 af Primary=15.29 cfs 0.728 af
<b>Pond 8P: SS on 4th St</b>	Inflow=3.11 cfs 0.148 af Primary=3.11 cfs 0.148 af
<b>Pond 9P: SS on HWY 96</b>	Inflow=0.21 cfs 0.010 af Primary=0.21 cfs 0.010 af
<b>Pond 10P: RR Wetlands</b>	Inflow=1.45 cfs 0.069 af Primary=1.45 cfs 0.069 af
<b>Pond 12P: Bald Eagle Lake</b>	Inflow=5.67 cfs 0.270 af Primary=5.67 cfs 0.270 af
<b>Pond 15P: Bald Eagle Park Low Areas</b>	Inflow=1.25 cfs 0.059 af Primary=1.25 cfs 0.059 af

**Total Runoff Area = 3.030 ac Runoff Volume = 0.999 af Average Runoff Depth = 3.95"**  
**0.00% Pervious = 0.000 ac 100.00% Impervious = 3.030 ac**



**14110\_Existing**

MSE 24-hr 3 100-Year Rainfall=7.36"

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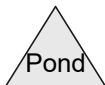
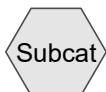
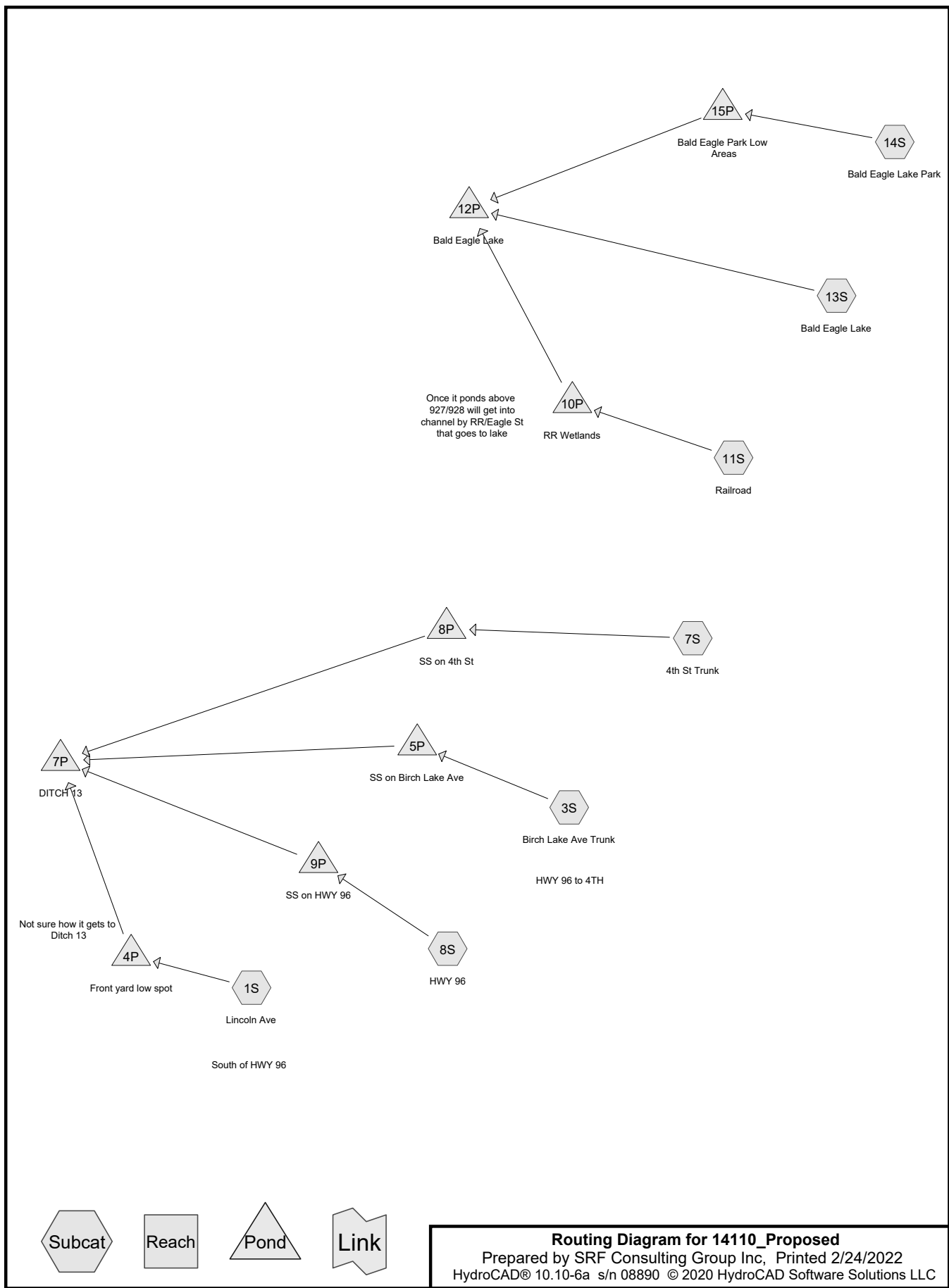
Page 4

Time span=0.00-48.00 hrs, dt=0.01 hrs, 4801 points  
 Runoff by SCS TR-20 method, UH=SCS, Weighted-CN  
 Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

<b>Subcatchment1S: Lincoln Ave</b>	Runoff Area=1.290 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=15.73 cfs 0.765 af
<b>Subcatchment3S: Birch Ave Trunk</b>	Runoff Area=0.440 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=5.37 cfs 0.261 af
<b>Subcatchment7S: 4th St Trunk</b>	Runoff Area=0.450 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=5.49 cfs 0.267 af
<b>Subcatchment8S: HWY 96</b>	Runoff Area=0.030 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=0.37 cfs 0.018 af
<b>Subcatchment11S: Railroad</b>	Runoff Area=0.210 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=2.56 cfs 0.125 af
<b>Subcatchment13S: Bald Eagle Lake</b>	Runoff Area=0.430 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=5.24 cfs 0.255 af
<b>Subcatchment14S: Bald Eagle Lake Park</b>	Runoff Area=0.180 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=2.20 cfs 0.107 af
<b>Pond 4P: Front yard low spot</b>	Inflow=15.73 cfs 0.765 af Primary=15.73 cfs 0.765 af
<b>Pond 5P: SS on Birch Lake Ave</b>	Inflow=5.37 cfs 0.261 af Primary=5.37 cfs 0.261 af
<b>Pond 7P: DITCH 13</b>	Inflow=26.95 cfs 1.311 af Primary=26.95 cfs 1.311 af
<b>Pond 8P: SS on 4th St</b>	Inflow=5.49 cfs 0.267 af Primary=5.49 cfs 0.267 af
<b>Pond 9P: SS on HWY 96</b>	Inflow=0.37 cfs 0.018 af Primary=0.37 cfs 0.018 af
<b>Pond 10P: RR Wetlands</b>	Inflow=2.56 cfs 0.125 af Primary=2.56 cfs 0.125 af
<b>Pond 12P: Bald Eagle Lake</b>	Inflow=10.00 cfs 0.487 af Primary=10.00 cfs 0.487 af
<b>Pond 15P: Bald Eagle Park Low Areas</b>	Inflow=2.20 cfs 0.107 af Primary=2.20 cfs 0.107 af

**Total Runoff Area = 3.030 ac Runoff Volume = 1.798 af Average Runoff Depth = 7.12"**  
**0.00% Pervious = 0.000 ac 100.00% Impervious = 3.030 ac**





**Routing Diagram for 14110\_Proposed**  
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**14110\_Proposed**

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*MSE 24-hr 3 2-Year Rainfall=2.81"*

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Page 2

Time span=0.00-48.00 hrs, dt=0.01 hrs, 4801 points  
 Runoff by SCS TR-20 method, UH=SCS, Weighted-CN  
 Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

<b>Subcatchment1S: Lincoln Ave</b>	Runoff Area=1.720 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=7.93 cfs 0.370 af
<b>Subcatchment3S: Birch Lake Ave Trunk</b>	Runoff Area=0.550 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=2.54 cfs 0.118 af
<b>Subcatchment7S: 4th St Trunk</b>	Runoff Area=0.880 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=4.06 cfs 0.189 af
<b>Subcatchment8S: HWY 96</b>	Runoff Area=0.070 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=0.32 cfs 0.015 af
<b>Subcatchment11S: Railroad</b>	Runoff Area=0.380 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=1.75 cfs 0.082 af
<b>Subcatchment13S: Bald Eagle Lake</b>	Runoff Area=0.780 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=3.60 cfs 0.168 af
<b>Subcatchment14S: Bald Eagle Lake Park</b>	Runoff Area=0.790 ac 100.00% Impervious Runoff Depth=2.58" Tc=0.0 min CN=98 Runoff=3.64 cfs 0.170 af
<b>Pond 4P: Front yard low spot</b>	Inflow=7.93 cfs 0.370 af Primary=7.93 cfs 0.370 af
<b>Pond 5P: SS on Birch Lake Ave</b>	Inflow=2.54 cfs 0.118 af Primary=2.54 cfs 0.118 af
<b>Pond 7P: DITCH 13</b>	Inflow=14.85 cfs 0.692 af Primary=14.85 cfs 0.692 af
<b>Pond 8P: SS on 4th St</b>	Inflow=4.06 cfs 0.189 af Primary=4.06 cfs 0.189 af
<b>Pond 9P: SS on HWY 96</b>	Inflow=0.32 cfs 0.015 af Primary=0.32 cfs 0.015 af
<b>Pond 10P: RR Wetlands</b>	Inflow=1.75 cfs 0.082 af Primary=1.75 cfs 0.082 af
<b>Pond 12P: Bald Eagle Lake</b>	Inflow=8.99 cfs 0.419 af Primary=8.99 cfs 0.419 af
<b>Pond 15P: Bald Eagle Park Low Areas</b>	Inflow=3.64 cfs 0.170 af Primary=3.64 cfs 0.170 af

**Total Runoff Area = 5.170 ac Runoff Volume = 1.111 af Average Runoff Depth = 2.58"**  
**0.00% Pervious = 0.000 ac 100.00% Impervious = 5.170 ac**



**14110\_Proposed**

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MSE 24-hr 3 10-Year Rainfall=4.19"

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Page 3

Time span=0.00-48.00 hrs, dt=0.01 hrs, 4801 points  
 Runoff by SCS TR-20 method, UH=SCS, Weighted-CN  
 Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

<b>Subcatchment1S: Lincoln Ave</b>	Runoff Area=1.720 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=11.90 cfs 0.567 af
<b>Subcatchment3S: Birch Lake Ave Trunk</b>	Runoff Area=0.550 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=3.80 cfs 0.181 af
<b>Subcatchment7S: 4th St Trunk</b>	Runoff Area=0.880 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=6.09 cfs 0.290 af
<b>Subcatchment8S: HWY 96</b>	Runoff Area=0.070 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=0.48 cfs 0.023 af
<b>Subcatchment11S: Railroad</b>	Runoff Area=0.380 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=2.63 cfs 0.125 af
<b>Subcatchment13S: Bald Eagle Lake</b>	Runoff Area=0.780 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=5.40 cfs 0.257 af
<b>Subcatchment14S: Bald Eagle Lake Park</b>	Runoff Area=0.790 ac 100.00% Impervious Runoff Depth=3.95" Tc=0.0 min CN=98 Runoff=5.46 cfs 0.260 af
<b>Pond 4P: Front yard low spot</b>	Inflow=11.90 cfs 0.567 af Primary=11.90 cfs 0.567 af
<b>Pond 5P: SS on Birch Lake Ave</b>	Inflow=3.80 cfs 0.181 af Primary=3.80 cfs 0.181 af
<b>Pond 7P: DITCH 13</b>	Inflow=22.27 cfs 1.061 af Primary=22.27 cfs 1.061 af
<b>Pond 8P: SS on 4th St</b>	Inflow=6.09 cfs 0.290 af Primary=6.09 cfs 0.290 af
<b>Pond 9P: SS on HWY 96</b>	Inflow=0.48 cfs 0.023 af Primary=0.48 cfs 0.023 af
<b>Pond 10P: RR Wetlands</b>	Inflow=2.63 cfs 0.125 af Primary=2.63 cfs 0.125 af
<b>Pond 12P: Bald Eagle Lake</b>	Inflow=13.49 cfs 0.643 af Primary=13.49 cfs 0.643 af
<b>Pond 15P: Bald Eagle Park Low Areas</b>	Inflow=5.46 cfs 0.260 af Primary=5.46 cfs 0.260 af

**Total Runoff Area = 5.170 ac Runoff Volume = 1.704 af Average Runoff Depth = 3.95"**  
**0.00% Pervious = 0.000 ac 100.00% Impervious = 5.170 ac**



**14110\_Proposed**

MSE 24-hr 3 100-Year Rainfall=7.36"

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Page 4

Time span=0.00-48.00 hrs, dt=0.01 hrs, 4801 points  
 Runoff by SCS TR-20 method, UH=SCS, Weighted-CN  
 Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

<b>Subcatchment1S: Lincoln Ave</b>	Runoff Area=1.720 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=20.98 cfs 1.021 af
<b>Subcatchment3S: Birch Lake Ave Trunk</b>	Runoff Area=0.550 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=6.71 cfs 0.326 af
<b>Subcatchment7S: 4th St Trunk</b>	Runoff Area=0.880 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=10.73 cfs 0.522 af
<b>Subcatchment8S: HWY 96</b>	Runoff Area=0.070 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=0.85 cfs 0.042 af
<b>Subcatchment11S: Railroad</b>	Runoff Area=0.380 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=4.63 cfs 0.225 af
<b>Subcatchment13S: Bald Eagle Lake</b>	Runoff Area=0.780 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=9.51 cfs 0.463 af
<b>Subcatchment14S: Bald Eagle Lake Park</b>	Runoff Area=0.790 ac 100.00% Impervious Runoff Depth=7.12" Tc=0.0 min CN=98 Runoff=9.64 cfs 0.469 af
<b>Pond 4P: Front yard low spot</b>	Inflow=20.98 cfs 1.021 af Primary=20.98 cfs 1.021 af
<b>Pond 5P: SS on Birch Lake Ave</b>	Inflow=6.71 cfs 0.326 af Primary=6.71 cfs 0.326 af
<b>Pond 7P: DITCH 13</b>	Inflow=39.27 cfs 1.911 af Primary=39.27 cfs 1.911 af
<b>Pond 8P: SS on 4th St</b>	Inflow=10.73 cfs 0.522 af Primary=10.73 cfs 0.522 af
<b>Pond 9P: SS on HWY 96</b>	Inflow=0.85 cfs 0.042 af Primary=0.85 cfs 0.042 af
<b>Pond 10P: RR Wetlands</b>	Inflow=4.63 cfs 0.225 af Primary=4.63 cfs 0.225 af
<b>Pond 12P: Bald Eagle Lake</b>	Inflow=23.78 cfs 1.157 af Primary=23.78 cfs 1.157 af
<b>Pond 15P: Bald Eagle Park Low Areas</b>	Inflow=9.64 cfs 0.469 af Primary=9.64 cfs 0.469 af

**Total Runoff Area = 5.170 ac Runoff Volume = 3.068 af Average Runoff Depth = 7.12"**  
**0.00% Pervious = 0.000 ac 100.00% Impervious = 5.170 ac**



## **Appendix D – Discharge Rates Summary**



<b>Discharge Rates Analysis</b>									
Resource of Concern	Modeling Area	Peak Discharge Rates (cfs)						Proposed BMP (1)	Notes
		2-Year		10-Year		100-Year			
		Existing	Proposed	Existing	Proposed	Existing	Proposed		
Lambert Creek (via Ditch 13)	Lincoln Ave	6.0	7.9	8.9	11.9	15.7	21.0	W1	
	Highway 96	0.1	0.3	0.2	0.5	0.4	0.9		- Assumes minor rate increases to the downstream storm sewer in acceptable. Project will need to verify in final design.
	Birch Lake Ave Trunk	2.0	2.5	3.0	3.8	5.4	6.7	B1	
	4th St Trunk	2.1	4.1	3.1	6.1	5.5	10.7	B2	
<b>Rates to Lambert Creek (via Ditch 13)</b>		<b>10.2</b>	<b>14.9</b>	<b>15.3</b>	<b>22.3</b>	<b>27.0</b>	<b>39.3</b>		
Bald Eagle Lake	Railroad	1.0	1.8	1.5	2.6	2.6	4.6	B3	
	Bald Eagle Lake	2.0	3.6	3.0	5.4	5.2	9.5		- Assumes increased rates to Bald Eagle Lake is acceptable. Project will need to verify in final design.
	Bald Eagle Lake Park	0.8	3.6	1.3	5.5	2.2	9.6	H1 & H2	
<b>Rates to the Bald Eagle Lake</b>		<b>3.8</b>	<b>9.0</b>	<b>5.7</b>	<b>13.5</b>	<b>10.0</b>	<b>23.8</b>		

(1) Increased rates assumed to be mitigated by the proposed BMP. Project will need to verify in final design.



## **Appendix E – Water Quality Volume Computations**



**Water Quality Volume Computations**

Watershed	Resource of Concern	Major Outflow Point	Impervious Areas (ac) (1)					Water Quality Volume (ac-ft)					Notes	
			Existing Condition	Proposed Condition				Requirements			WQV Required	WQV Provided		
				New and Reconstructed (Total)	Exempt (RCWD)	New and Reconstructed - Exempt (RCWD)	Net New Impervious Area (Total)	VLAWMO / CITY	RCWD	NPDES		BMP		WQV
					(2)									
VLAWMO	Labert Creek (via Ditch 13)	Lincoln Ave	1.29	1.72	-	-	0.43	0.08	-	0.07	0.08	W1	0.11	
		HWY 96	0.03	0.07	-	-	0.04	0.00	-	0.00	0.00	-	-	
		Birch Lake Ave Trunk	0.44	0.55	-	-	0.12	0.03	-	0.02	0.03	B1	0.04	
		4th St Trunk	0.45	0.88	-	-	0.43	0.04	-	0.04	0.04	B2	0.15	- Coordination with High School needed
<b>VLAWMO Total</b>			<b>2.21</b>	<b>3.22</b>	<b>-</b>	<b>-</b>	<b>1.02</b>	<b>0.15</b>	<b>-</b>	<b>0.13</b>	<b>0.15</b>	<b>-</b>	<b>0.30</b>	
RCWD	Bald Eagle Lake (Impaired for Nutrients)	Railroad	0.21	0.38	0.00	0.38	0.17	-	0.02	0.02	0.02	B3	0.00	- Dry/Wet Pond for rate control
		Bald Eagle Lake	0.43	0.78	0.04	0.74	0.35	-	0.03	0.03	0.03	-	-	- Project does not anticipate increasing impervious limits - Project does not anticipate disturbing the in-situ soils below the underlying structural aggregate base along Bald Eagle Blvd, Taylor Ave, and most of Hugo Rd.
		Bald Eagle Lake Park	0.18	0.79	0.00	0.79	0.61	-	0.05	0.05	0.05	H1	0.11	
											H2	0.02		
<b>RCWD Total</b>			<b>0.82</b>	<b>1.95</b>	<b>0.04</b>	<b>1.91</b>	<b>1.13</b>	<b>-</b>	<b>0.09</b>	<b>0.09</b>	<b>0.09</b>	<b>-</b>	<b>0.13</b>	
<b>Project Total</b>			<b>3.03</b>	<b>5.17</b>	<b>0.04</b>	<b>1.91</b>	<b>2.15</b>	<b>0.15</b>	<b>0.09</b>	<b>0.23</b>	<b>0.24</b>	<b>-</b>	<b>0.43</b>	

(1) Reconstructed impervious is defined as the removal of an impervious surface such that the underlying structural aggregate base is effectively removed and the underlying native soil is exposed.

(2) In RCWD, trails up to 10' wide draining to vegetated open space that is at least 5' wide are exempt from the WQV requirements

(3) Water quality volume = larger of 1.1 inches over the net new impervious area OR 0.55 inches over the new and reconstructed impervious area.

(4) Water quality volume = larger of 1 inches over the new impervious area OR 0.5 inches over the new and reconstructed impervious area



## **Appendix F – Soils Information**







## MAP LEGEND

### Area of Interest (AOI)









 Area of Interest (AOI)

### Soils

#### Soil Rating Polygons

 A  
 A/D  
 B  
 B/D  
 C  
 C/D  
 D  
 Not rated or not available

#### Soil Rating Lines


 A  
 A/D  
 B  
 B/D  
 C  
 C/D  
 D  
 Not rated or not available

#### Soil Rating Points

 A  
 A/D  
 B  
 B/D

 C  
 C/D  
 D  
 Not rated or not available

### Water Features

 Streams and Canals

### Transportation

 Rails  
 Interstate Highways  
 US Routes  
 Major Roads  
 Local Roads

### Background

 Aerial Photography

## MAP INFORMATION

The soil surveys that comprise your AOI were mapped at 1:15,800.

Please rely on the bar scale on each map sheet for map measurements.

Source of Map: Natural Resources Conservation Service  
 Web Soil Survey URL:  
 Coordinate System: Web Mercator (EPSG:3857)

Maps from the Web Soil Survey are based on the Web Mercator projection, which preserves direction and shape but distorts distance and area. A projection that preserves area, such as the Albers equal-area conic projection, should be used if more accurate calculations of distance or area are required.

This product is generated from the USDA-NRCS certified data as of the version date(s) listed below.

Soil Survey Area: Ramsey County, Minnesota  
 Survey Area Data: Version 16, Sep 10, 2021

Soil Survey Area: Washington County, Minnesota  
 Survey Area Data: Version 17, Sep 10, 2021

Your area of interest (AOI) includes more than one soil survey area. These survey areas may have been mapped at different scales, with a different land use in mind, at different times, or at different levels of detail. This may result in map unit symbols, soil properties, and interpretations that do not completely agree across soil survey area boundaries.

Soil map units are labeled (as space allows) for map scales 1:50,000 or larger.

Date(s) aerial images were photographed: Jun 3, 2020—Jun 12, 2020

The orthophoto or other base map on which the soil lines were compiled and digitized probably differs from the background imagery displayed on these maps. As a result, some minor shifting of map unit boundaries may be evident.



## Hydrologic Soil Group

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
75	Bluffton loam	C/D	1.9	0.0%
123	Dundas fine sandy loam	B/D	19.1	0.5%
132B	Hayden fine sandy loam, 2 to 6 percent slopes	B	6.8	0.2%
155C	Chetek sandy loam, 6 to 12 percent slopes	A	13.1	0.3%
155D	Chetek sandy loam, 12 to 25 percent slopes	A	2.7	0.1%
158B	Zimmerman fine sand, 1 to 6 percent slopes	A	55.4	1.4%
158C	Zimmerman fine sand, 6 to 12 percent slopes	A	8.4	0.2%
159	Anoka loamy fine sand, 0 to 3 percent slopes	A	255.1	6.4%
159B	Anoka loamy fine sand, 3 to 9 percent slopes	A	127.8	3.2%
161	Isanti loamy fine sand, depressional	A/D	59.8	1.5%
162	Lino loamy fine sand	A/D	126.2	3.2%
169B	Braham loamy fine sand, 1 to 6 percent slopes	B	15.4	0.4%
169C	Braham loamy fine sand, 6 to 15 percent slopes	B	11.6	0.3%
170	Blomford loamy fine sand	B/D	2.1	0.1%
225	Nessel fine sandy loam, 1 to 4 percent slopes	C	13.2	0.3%
265	Soderville loamy fine sand	A/D	107.9	2.7%
342B	Kingsley sandy loam, 2 to 6 percent slopes	C	21.3	0.5%
452	Comstock silt loam, 0 to 3 percent slopes	B/D	20.8	0.5%
481	Kratka fine sandy loam	B/D	8.8	0.2%
540	Seelyeville muck	A/D	72.4	1.8%
541	Rifle muck	A/D	238.5	6.0%
543	Markey muck	A/D	65.7	1.7%
544	Cathro muck	B/D	22.0	0.6%



Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
859B	Urban land-Zimmerman complex, 1 to 8 percent slopes		800.6	20.2%
860C	Urban land-Hayden-Kingsley complex, 3 to 15 percent slopes		74.4	1.9%
860D	Urban land-Hayden-Kingsley complex, 15 to 25 percent slopes		35.8	0.9%
863	Urban land-Lino complex, 0 to 3 percent slopes		320.7	8.1%
1027	Udorthents, wet substratum		21.7	0.5%
1033	Udifluvents		63.4	1.6%
1039	Urban land		86.5	2.2%
1055	Aquolls and histosols, ponded	B/D	16.2	0.4%
1813B	Lino variant loamy fine sand, 2 to 6 percent slopes	A	8.0	0.2%
W	Water		870.8	22.0%
<b>Subtotals for Soil Survey Area</b>			<b>3,574.3</b>	<b>90.2%</b>
<b>Totals for Area of Interest</b>			<b>3,962.4</b>	<b>100.0%</b>

Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
75	Bluffton loam	C/D	0.8	0.0%
123	Dundas fine sandy loam	B/D	9.8	0.2%
132B	Hayden fine sandy loam, 2 to 6 percent slopes	B	1.7	0.0%
132C	Hayden fine sandy loam, 6 to 12 percent slopes	B	3.2	0.1%
132D	Hayden fine sandy loam, 12 to 25 percent slopes	B	0.0	0.0%
158B	Zimmerman fine sand, 1 to 6 percent slopes	A	36.3	0.9%
159B	Anoka loamy fine sand, 3 to 9 percent slopes	A	19.0	0.5%
162	Lino loamy fine sand	A/D	22.3	0.6%
169B	Braham loamy fine sand, 1 to 6 percent slopes	B	45.7	1.2%



Map unit symbol	Map unit name	Rating	Acres in AOI	Percent of AOI
169C	Braham loamy fine sand, 6 to 15 percent slopes	B	9.5	0.2%
170	Blomford loamy fine sand	B/D	7.2	0.2%
225	Nessel fine sandy loam, 1 to 4 percent slopes	C	16.2	0.4%
265	Soderville loamy fine sand	A/D	46.5	1.2%
481	Kratka fine sandy loam	B/D	107.7	2.7%
541	Rifle muck	A/D	4.9	0.1%
1033	Udifluvents		0.3	0.0%
1055	Aquolls and Histosols, ponded	B/D	10.9	0.3%
1813B	Lino variant loamy fine sand, 2 to 6 percent slopes	A	8.5	0.2%
W	Water		37.7	1.0%
<b>Subtotals for Soil Survey Area</b>			<b>388.0</b>	<b>9.8%</b>
<b>Totals for Area of Interest</b>			<b>3,962.4</b>	<b>100.0%</b>



## Description

Hydrologic soil groups are based on estimates of runoff potential. Soils are assigned to one of four groups according to the rate of water infiltration when the soils are not protected by vegetation, are thoroughly wet, and receive precipitation from long-duration storms.

The soils in the United States are assigned to four groups (A, B, C, and D) and three dual classes (A/D, B/D, and C/D). The groups are defined as follows:

Group A. Soils having a high infiltration rate (low runoff potential) when thoroughly wet. These consist mainly of deep, well drained to excessively drained sands or gravelly sands. These soils have a high rate of water transmission.

Group B. Soils having a moderate infiltration rate when thoroughly wet. These consist chiefly of moderately deep or deep, moderately well drained or well drained soils that have moderately fine texture to moderately coarse texture. These soils have a moderate rate of water transmission.

Group C. Soils having a slow infiltration rate when thoroughly wet. These consist chiefly of soils having a layer that impedes the downward movement of water or soils of moderately fine texture or fine texture. These soils have a slow rate of water transmission.

Group D. Soils having a very slow infiltration rate (high runoff potential) when thoroughly wet. These consist chiefly of clays that have a high shrink-swell potential, soils that have a high water table, soils that have a claypan or clay layer at or near the surface, and soils that are shallow over nearly impervious material. These soils have a very slow rate of water transmission.

If a soil is assigned to a dual hydrologic group (A/D, B/D, or C/D), the first letter is for drained areas and the second is for undrained areas. Only the soils that in their natural condition are in group D are assigned to dual classes.

## Rating Options

*Aggregation Method:* Dominant Condition

*Component Percent Cutoff:* None Specified

*Tie-break Rule:* Higher



## **Appendix G – FEMA Floodplain Mapping**







**NOTES TO USERS**

This map is for use in administering the National Flood Insurance Program. It does not necessarily identify all areas subject to flooding, particularly from local drainage sources of small size. The community map repository should be consulted for possible updated or additional flood hazard information.

To obtain more detailed information in areas where **Base Flood Elevations (BFEs)** and/or **floodways** have been determined, users are encouraged to consult the Flood Profiles and Floodway Data and/or Summary of Stillwater Elevations tables contained within the Flood Insurance Study (FIS) Report that accompanies this FIRM. Users should be aware that BFEs shown on the FIRM represent rounded whole-foot elevations. These BFEs are intended for flood insurance rating purposes only and should not be used as the sole source of flood elevation information. Accordingly, flood elevation data presented in the FIS Report should be utilized in conjunction with the FIRM for purposes of construction and/or floodplain management.

**Coastal Base Flood Elevations** shown on this map apply only landward of 0' North American Vertical Datum of 1988 (NAVD 88). Users of this FIRM should be aware that coastal flood elevations are also provided in the Summary of Stillwater Elevations table in the Flood Insurance Study Report for this jurisdiction. Elevations shown in the Summary of Stillwater Elevations table should be used for construction and/or floodplain management purposes when they are higher than the elevations shown on this FIRM.

Boundaries of the **floodways** were computed at cross sections and interpolated between cross sections. The floodways were based on hydraulic considerations with regard to requirements of the National Flood Insurance Program. Floodway widths and other pertinent floodway data are provided in the Flood Insurance Study Report for this jurisdiction.

Certain areas not in Special Flood Hazard Areas may be protected by **flood control structures**. Refer to Section 2.4 "Flood Protection Measures" of the Flood Insurance Study Report for information on flood control structures for this jurisdiction.

The **projection** used in the preparation of this map was Universal Transverse Mercator (UTM) zone 15. The **horizontal datum** was NAD 83, GRS 1980 spheroid. Differences in datum, spheroid, projection or UTM zones used in the production of FIRMs for adjacent jurisdictions may result in slight positional differences in map features across jurisdiction boundaries. These differences do not affect the accuracy of this FIRM.

Flood elevations on this map are referenced to the North American Vertical Datum of 1988. These flood elevations must be compared to structure and ground elevations referenced to the same vertical datum. For information regarding conversion between the National Geodetic Vertical Datum of 1929 and the North American Vertical Datum of 1988, visit the National Geodetic Survey website at <http://www.ngs.noaa.gov> or contact the National Geodetic Survey at the following address:

NGS Information Services  
NOAA, NNGS12  
National Geodetic Survey  
SSMC-3, #9202  
1315 East-West Highway  
Silver Spring, Maryland 20910-3282  
(301) 713-3242

To obtain current elevation, description, and/or location information for **bench marks** shown on this map, please contact the Information Services Branch of the National Geodetic Survey at (301) 713-3242, or visit its website at <http://www.ngs.noaa.gov>.

**Base Map** information shown on this FIRM was provided for Ramsey County by Farm Services Administration, dated 2004 and captured at a resolution of one meter.

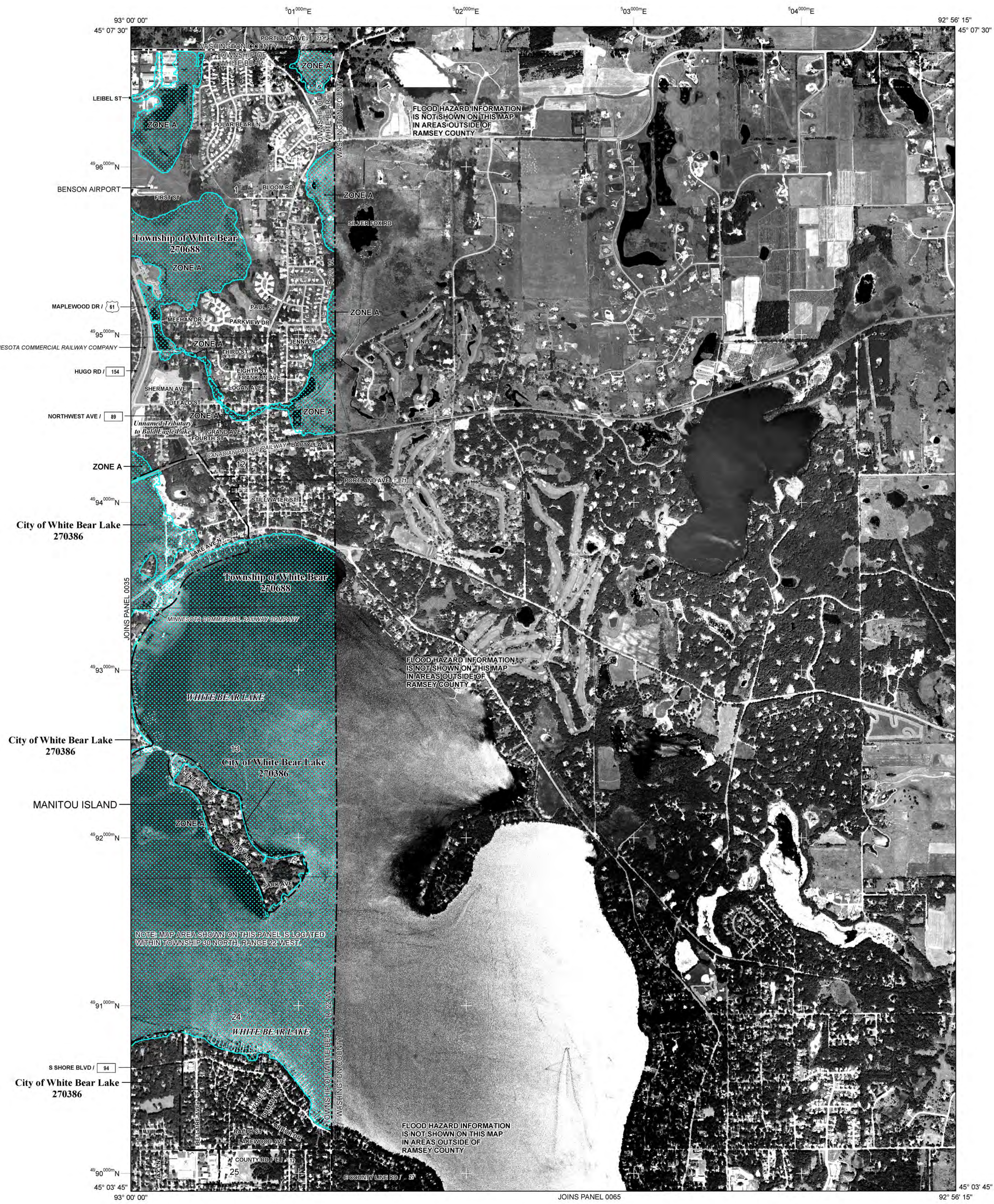
The **profile baselines** depicted on this map represent the hydraulic modeling baselines that match the flood profiles in the FIS report. As a result of improved topographic data, the **profile baseline**, in some cases, may deviate significantly from the channel centerline or appear outside the SFHA.

**Corporate limits** shown on this map are based on the best data available at the time of publication. Because changes due to annexations or de-annexations may have occurred after this map was published, map users should contact appropriate community officials to verify current corporate limit locations.

Please refer to the separately printed **Map Index** for an overview map of the county showing the layout of map panels, community map repository addresses, and a Listing of Communities table containing National Flood Insurance Program dates for each community as well as a listing of the panels on which each community is located.

Contact the **FEMA Map Service Center** at 1-800-358-9616 for information on available products associated with this FIRM. Available products may include previously issued Letters of Map Change, a Flood Insurance Study Report, and/or digital versions of this map. The FEMA Map Service Center may also be reached by Fax at 1-800-358-9620 and its website at <http://msc.fema.gov>.

If you have **questions about this map** or questions concerning the National Flood Insurance Program in general, please call 1-877-FEMA-MAP (1-877-336-2627) or visit the FEMA website at <http://www.fema.gov/business/infp/>.



**LEGEND**

**SPECIAL FLOOD HAZARD AREAS (SFHAs) SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD**  
The 1% annual chance flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equaled or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, AR, A99, V, and VE. The Base Flood Elevation is the water-surface elevation of the 1% annual chance flood.

- ZONE A** No Base Flood Elevations determined.
- ZONE AE** Base Flood Elevations determined.
- ZONE AH** Flood depths of 1 to 3 feet (usually areas of ponding); Base Flood Elevations determined.
- ZONE AO** Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain); average depths determined. For areas of alluvial fan flooding, velocities also determined.
- ZONE AR** Special Flood Hazard Areas formerly protected from the 1% annual chance flood by a flood control system that was subsequently destroyed. Zone AR indicates that the former flood control system is being restored to provide protection from the 1% annual chance or greater flood.
- ZONE A99** Area to be protected from 1% annual chance flood by a Federal flood protection system under construction; no Base Flood Elevations determined.
- ZONE V** Coastal flood zone with velocity hazard (wave action); no Base Flood Elevations determined.
- ZONE VE** Coastal flood zone with velocity hazard (wave action); Base Flood Elevations determined.

**FLOODWAY AREAS IN ZONE AE**  
The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the 1% annual chance flood can be carried without substantial increases in flood heights.

**OTHER FLOOD AREAS**

- ZONE X** Areas of 0.2% annual chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood.
- OTHER AREAS**
- ZONE X** Areas determined to be outside the 0.2% annual chance floodplain.
- ZONE D** Areas in which flood hazards are undetermined, but possible.

**COASTAL BARRIER RESOURCES SYSTEM (CBRS) AREAS**  
CBRS areas and OPAs are normally located within or adjacent to Special Flood Hazard Areas.

**OTHERWISE PROTECTED AREAS (OPAs)**  
OPAs are normally located within or adjacent to Special Flood Hazard Areas.

- 1% Annual Chance Floodplain Boundary
- 0.2% Annual Chance Floodplain Boundary
- Floodway boundary
- Zone D boundary
- CBRS and OPA boundary
- Boundary dividing Special Flood Hazard Area Zones and boundary dividing Special Flood Hazard Areas of different Base Flood Elevations, flood depths, or flood velocities.
- Base Flood Elevation line and value; elevation in feet\*
- Base Flood Elevation value where uniform within zone; elevation in feet\*

\*Referenced to the North American Vertical Datum of 1988

- Cross section line
- Transect line
- Geographic coordinates referenced to the North American Datum of 1983 (NAD 83) Western Hemisphere
- 1000-meter Universal Transverse Mercator grid values, zone 15
- Bench mark (see explanation in Notes to Users section of this FIRM panel)
- M1.5 River Mile

**MAP REPOSITORIES**  
Refer to Map Repositories list on Map Index

**EFFECTIVE DATE OF COUNTYWIDE FLOOD INSURANCE RATE MAP**  
June 4, 2010

**EFFECTIVE DATE(S) OF REVISION(S) TO THIS PANEL**

For community map revision history prior to countywide mapping, refer to the Community Map History table located in the Flood Insurance Study report for this jurisdiction.

To determine if flood insurance is available in this community, contact your insurance agent or call the National Flood Insurance Program at 1-800-638-6620.

**NATIONAL FLOOD INSURANCE PROGRAM**

**PANEL 0055G**

**FIRM**  
**FLOOD INSURANCE RATE MAP**  
**RAMSEY COUNTY,**  
**MINNESOTA**  
**(ALL JURISDICTIONS)**

**PANEL 55 OF 140**  
(SEE MAP INDEX FOR FIRM PANEL LAYOUT)

**CONTAINS:**

COMMUNITY	NUMBER	PANEL	SUFFIX
WHITE BEAR LAKE, CITY OF	270386	0055	G
WHITE BEAR TOWNSHIP OF	270688	0055	G

Notice to User: The **Map Number** shown below should be used when placing map orders; the **Community Number** shown above should be used on insurance applications for the subject community.

**MAP NUMBER**  
27123C0055G  
**EFFECTIVE DATE**  
JUNE 4, 2010  
**Federal Emergency Management Agency**

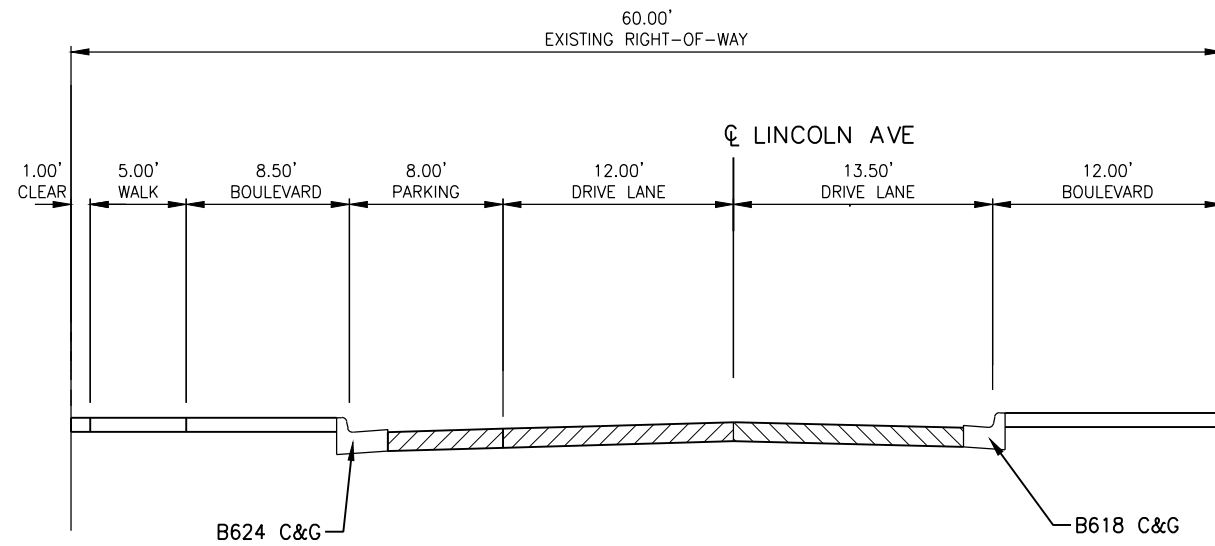


**Appendix E - Plans and Sections**









TYPICAL SECTION  
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I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
 Print Name: \_\_\_\_\_  
 Date 1/6/2022 License # \_\_\_\_\_

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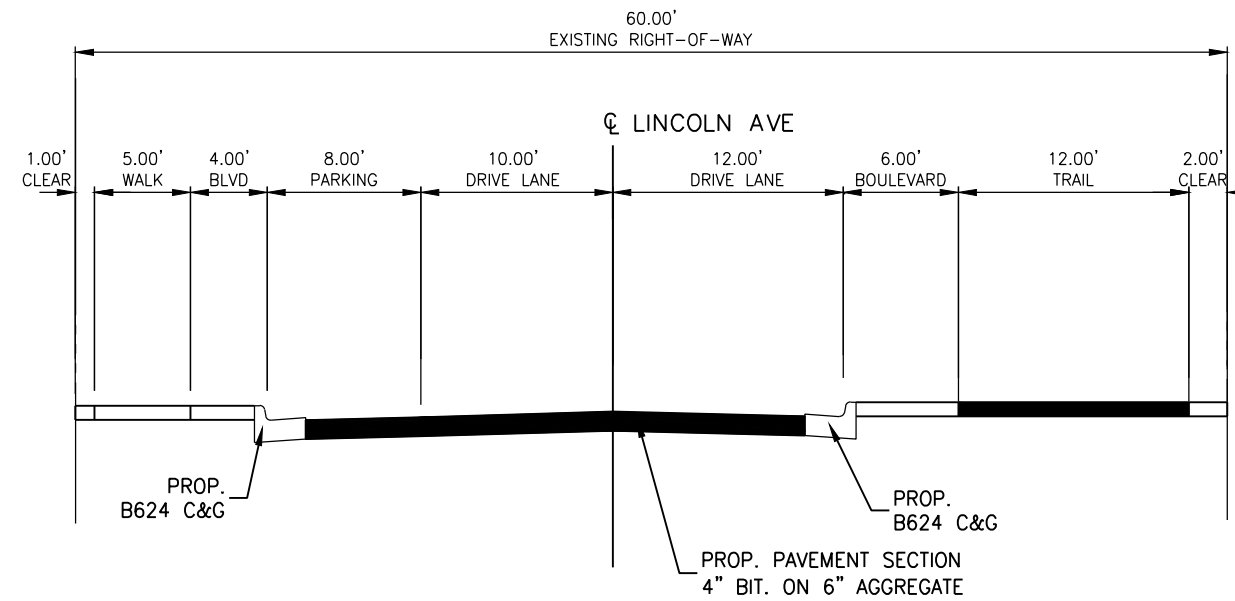


BRUCE VENTO REGIONAL TRAIL - PHASE 2

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TYPICAL SECTION  
LINCOLN AVE: PROPOSED

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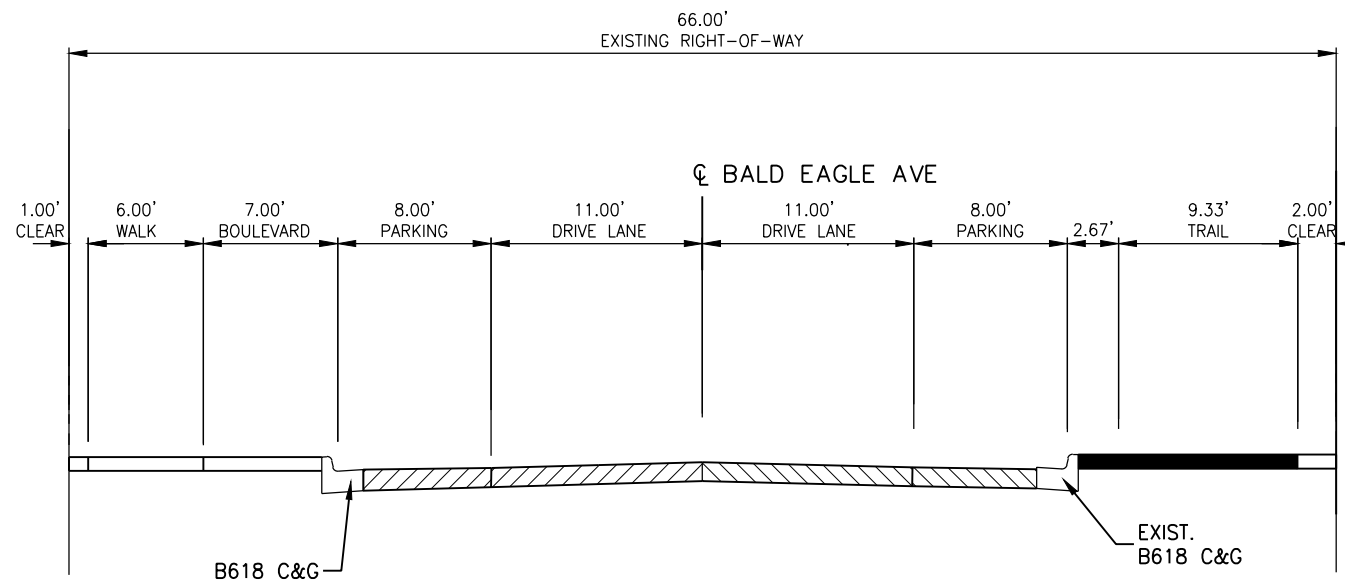
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BALD EAGLE AVE -  
HWY 96 TO WHITE BEAR LAKE HIGH SCHOOL: PROPOSED

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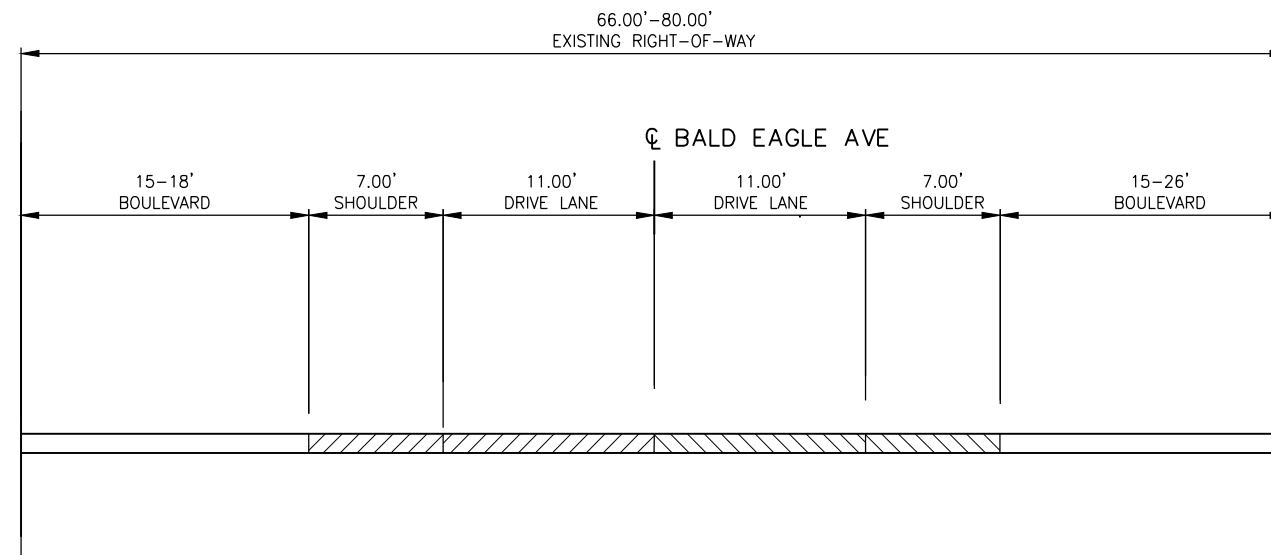
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TYPICAL SECTION  
BALD EAGLE AVE -  
WHITE BEAR LAKE HIGH SCHOOL TO BALD EAGLE LAKE: EXISTING

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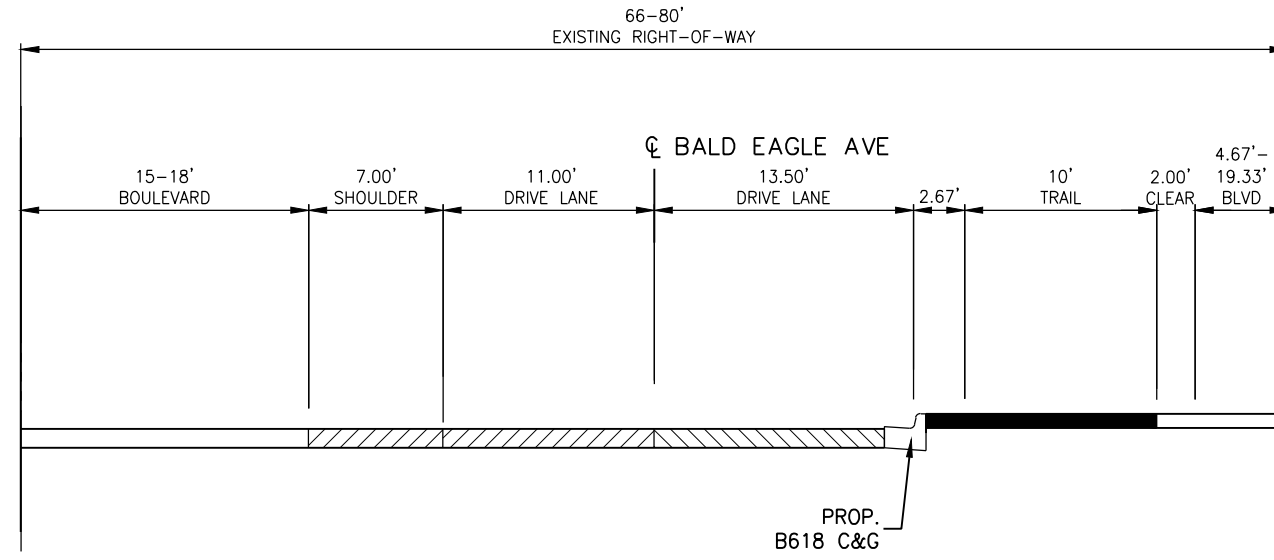
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BALD EAGLE AVE -  
WHITE BEAR LAKE HIGH SCHOOL TO BALD EAGLE LAKE: PROPOSED

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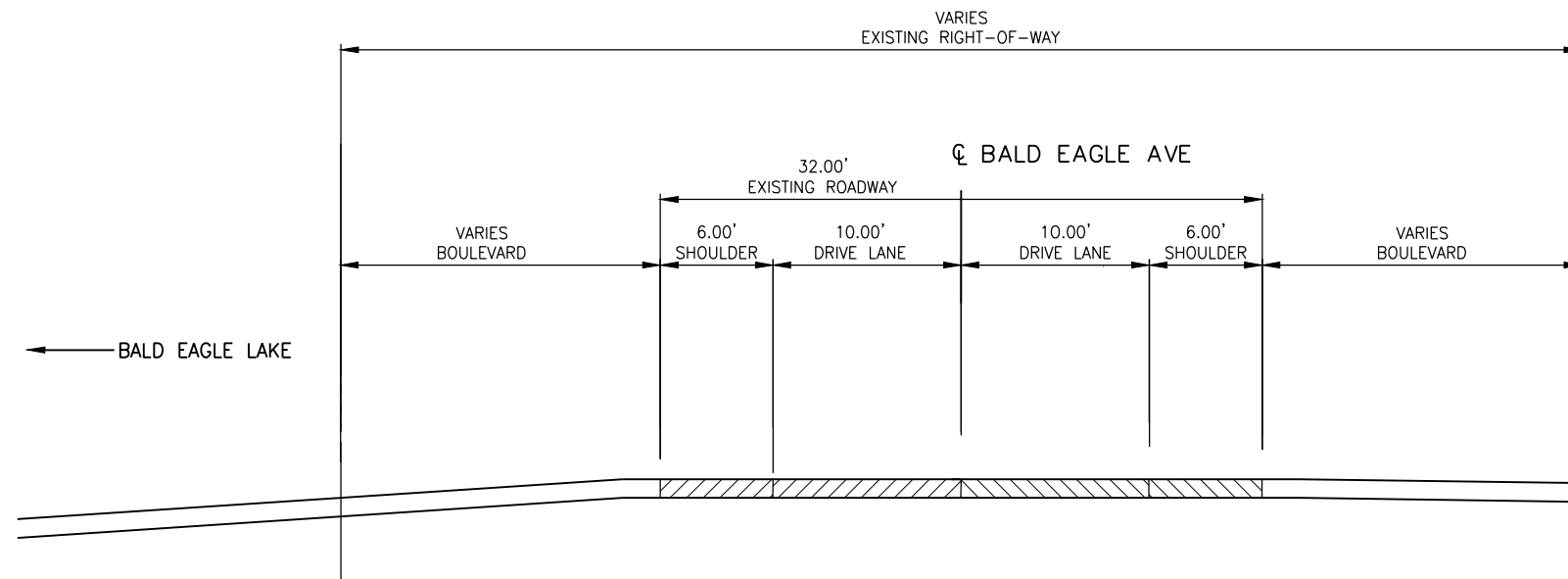
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BALD EAGLE BOULEVARD –  
BALD EAGLE AVE TO PARK AVE: EXISTING

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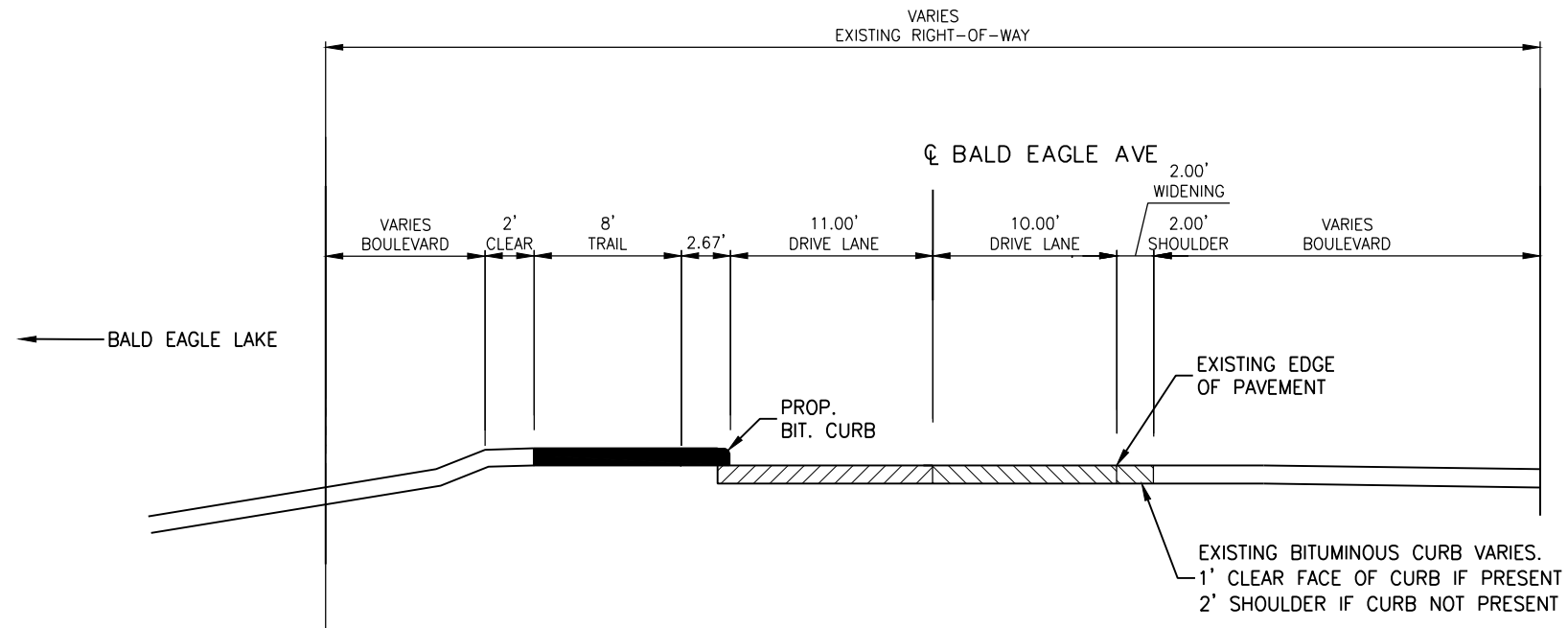
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BALD EAGLE BOULEVARD -  
BALD EAGLE AVE TO PARK AVE: PROPOSED

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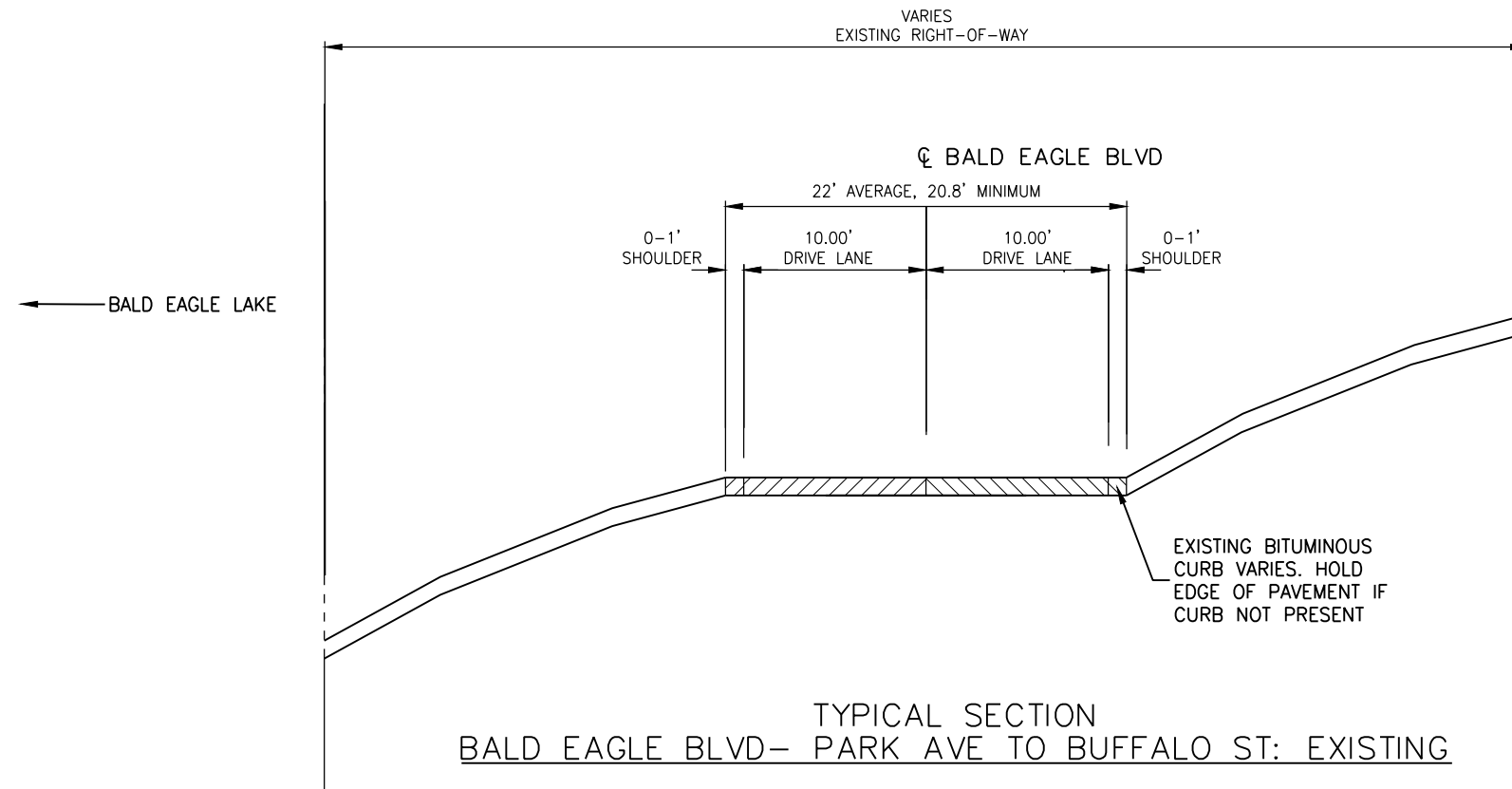


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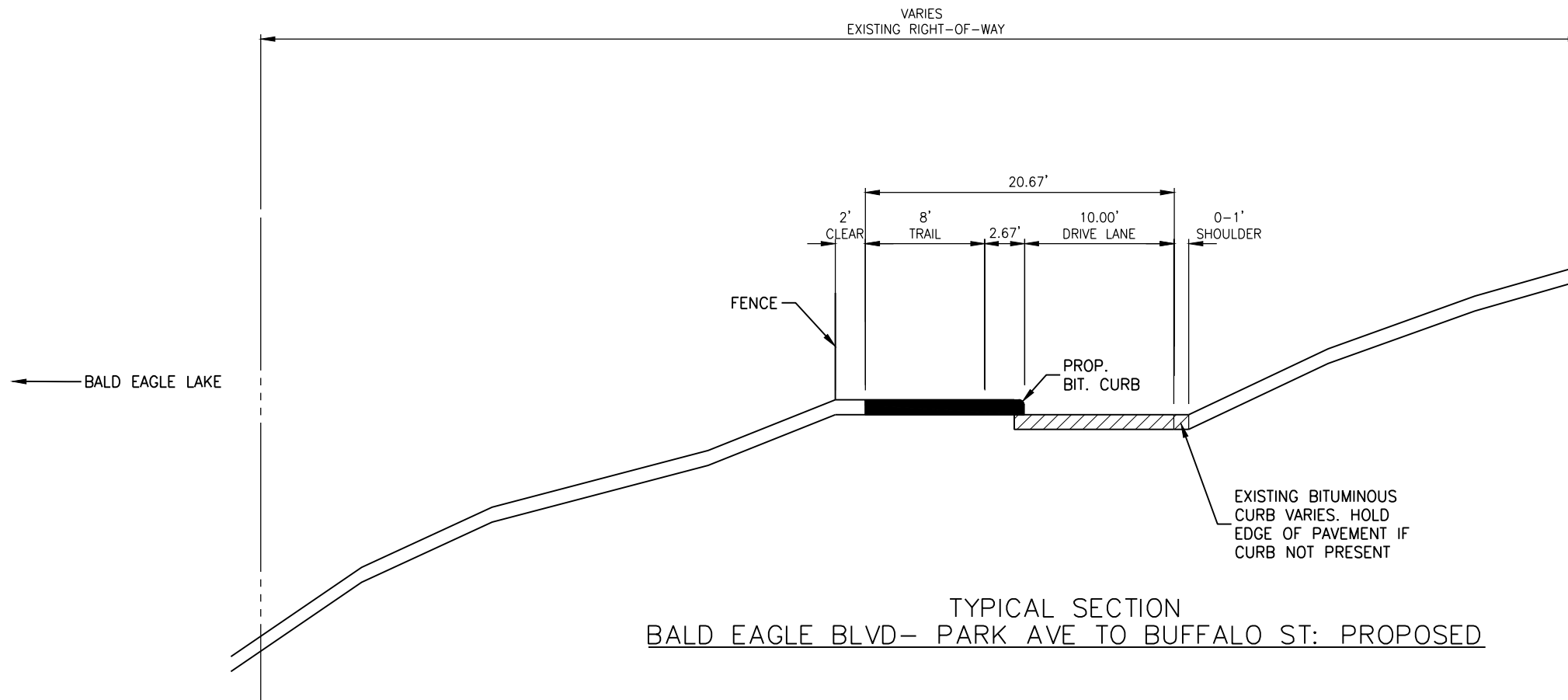


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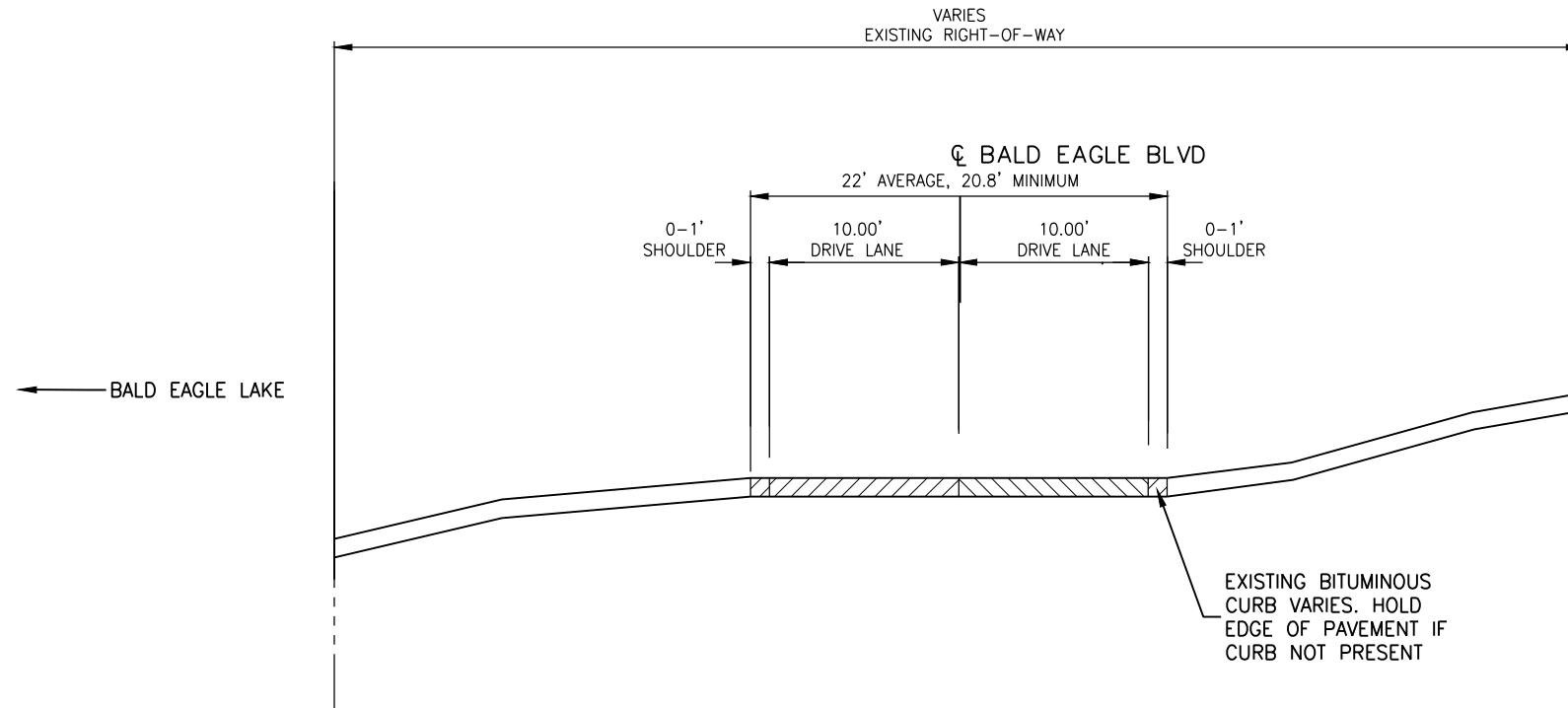
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TYPICAL SECTION  
BALD EAGLE BLVD- BUFFALO ST. TO TAYLOR AVE: EXISTING

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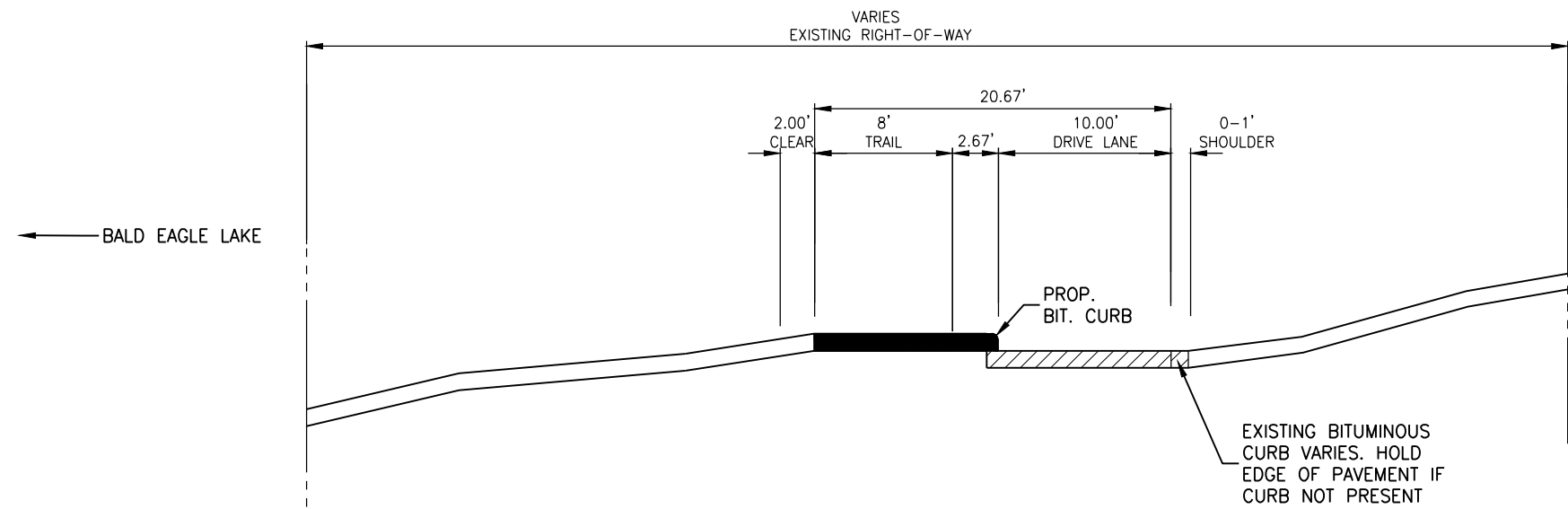


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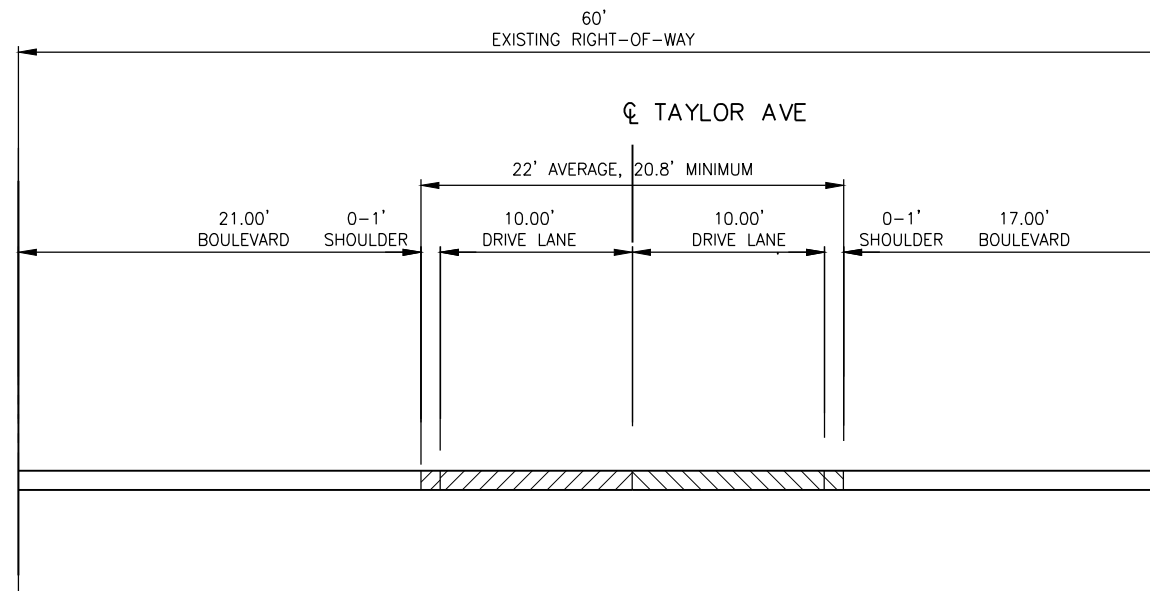


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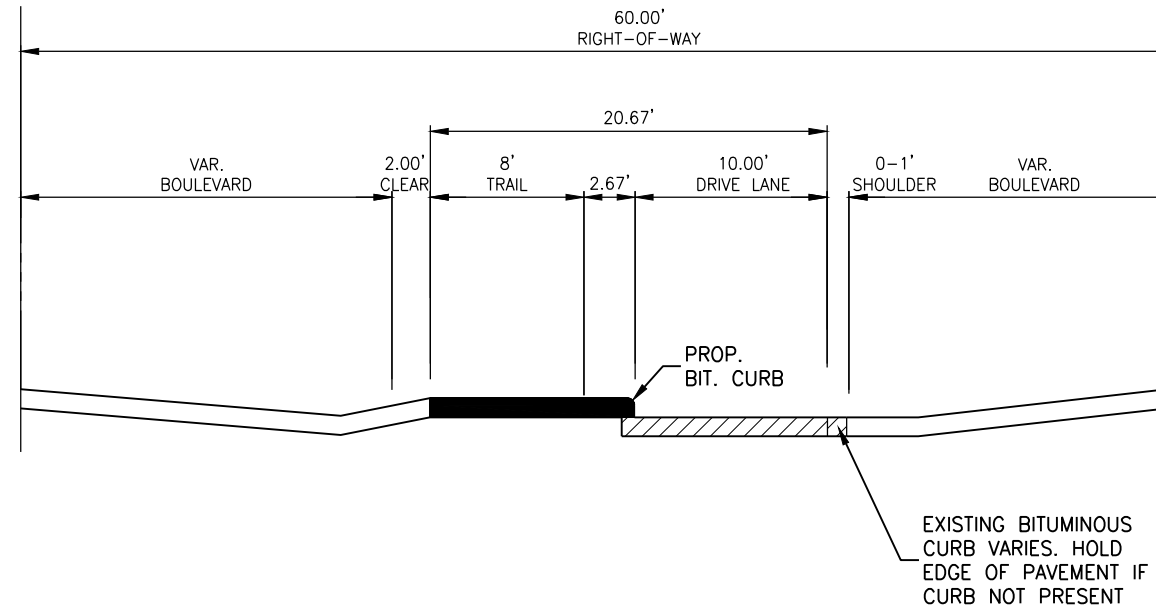
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BRUCE VENTO REGIONAL TRAIL – PHASE 2  
 TYPICAL SECTIONS  
 FINAL PRELIMINARY DESIGN

SHEET  
 13  
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TYPICAL SECTION  
TAYLOR AVE – BALD EAGLE BLVD TO HUGO RD: PROPOSED

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NO	DATE	BY	CKD	APPR

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
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Date 1/6/2022 License # \_\_\_\_\_

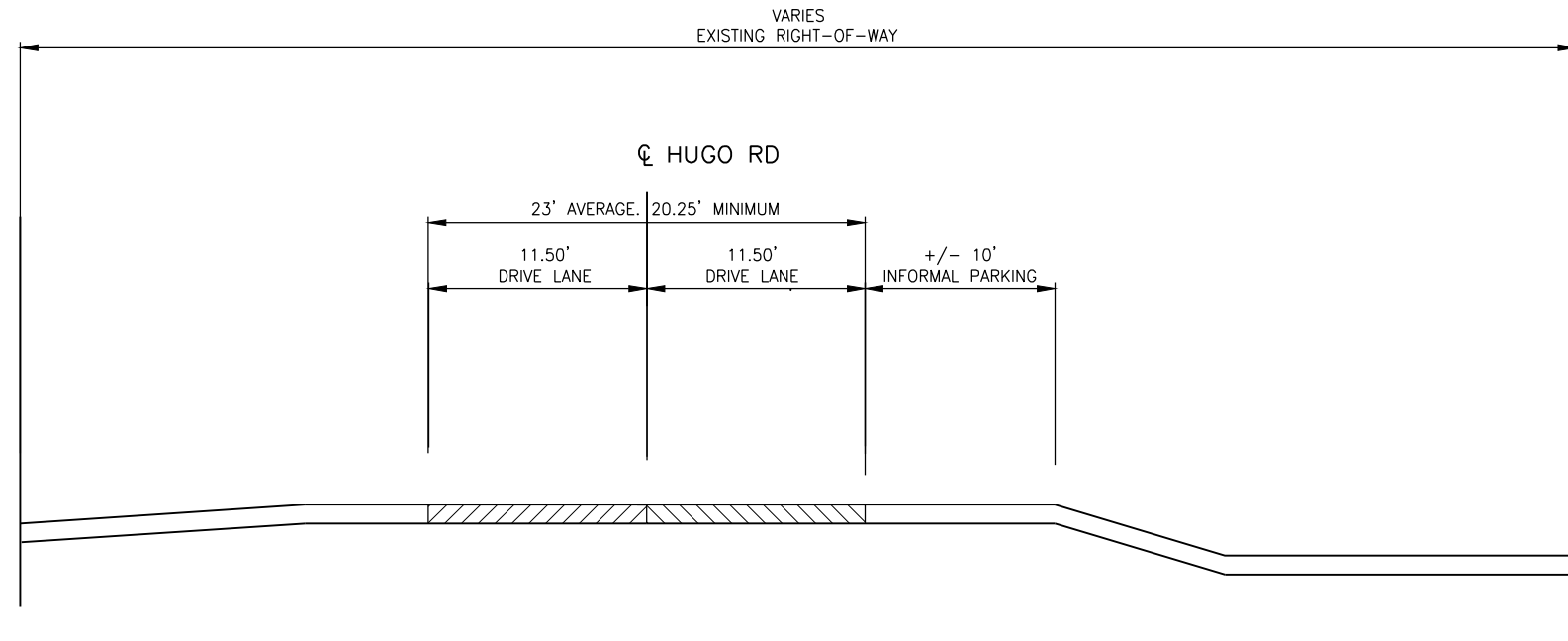
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COMM. NO. 14110



BRUCE VENTO REGIONAL TRAIL – PHASE 2  
TYPICAL SECTIONS  
FINAL PRELIMINARY DESIGN

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OF  
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TYPICAL SECTION  
HUGO RD – TAYLOR AVE TO BALD EAGLE RECREATION AREA: EXISTING

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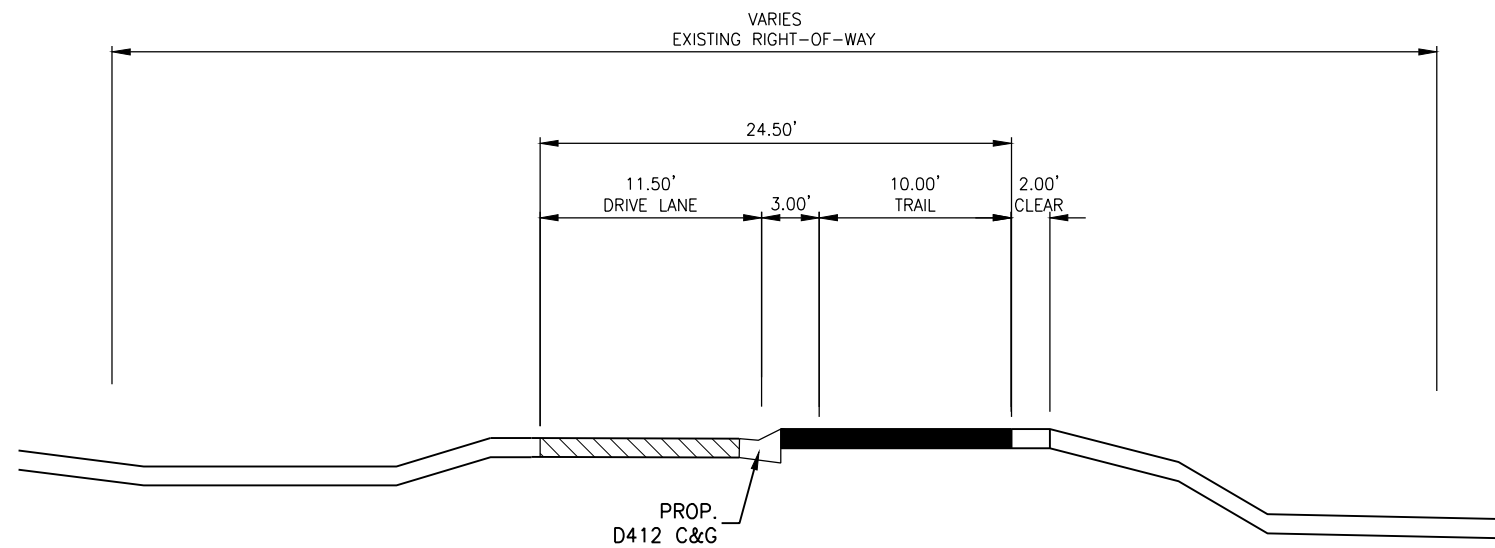


BRUCE VENTO REGIONAL TRAIL – PHASE 2

TYPICAL SECTIONS  
FINAL PRELIMINARY DESIGN

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 25





TYPICAL SECTION  
HUGO RD – TAYLOR AVE TO BALD EAGLE RECREATION AREA:  
PROPOSED ONE WAY ALTERNATIVE

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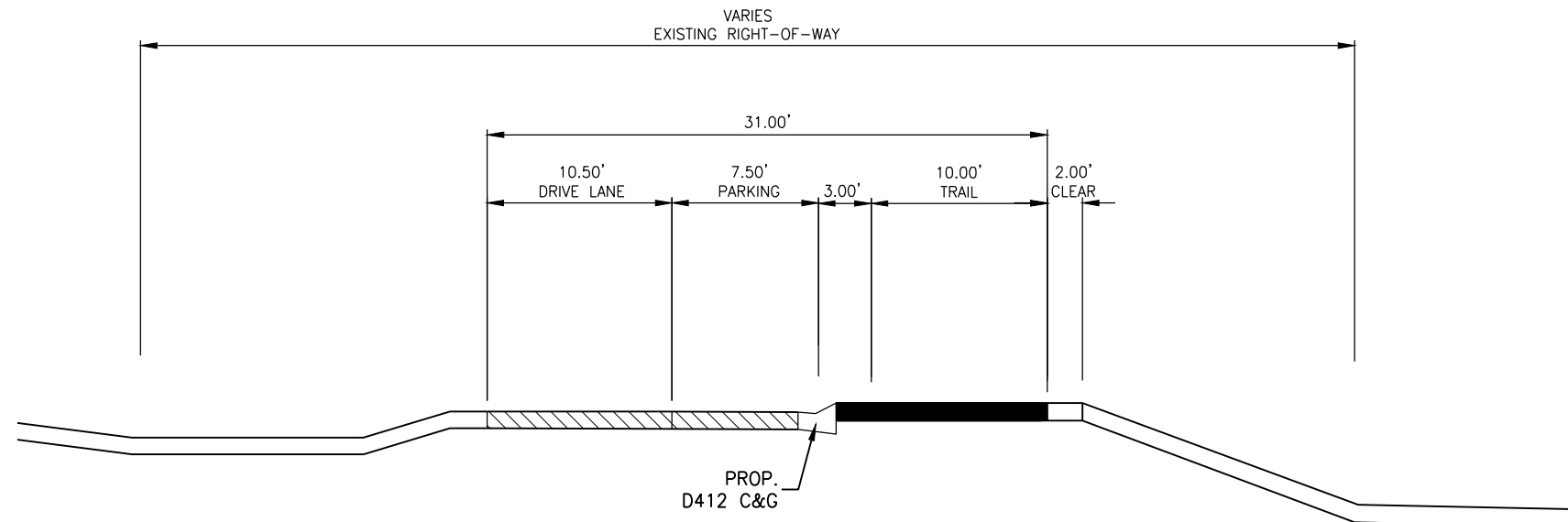
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BRUCE VENTO REGIONAL TRAIL – PHASE 2  
 TYPICAL SECTIONS  
 FINAL PRELIMINARY DESIGN

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TYPICAL SECTION  
HUGO RD – TAYLOR AVE TO BALD EAGLE RECREATION AREA:  
PROPOSED ONE WAY ALTERNATIVE AT PARKING BAY

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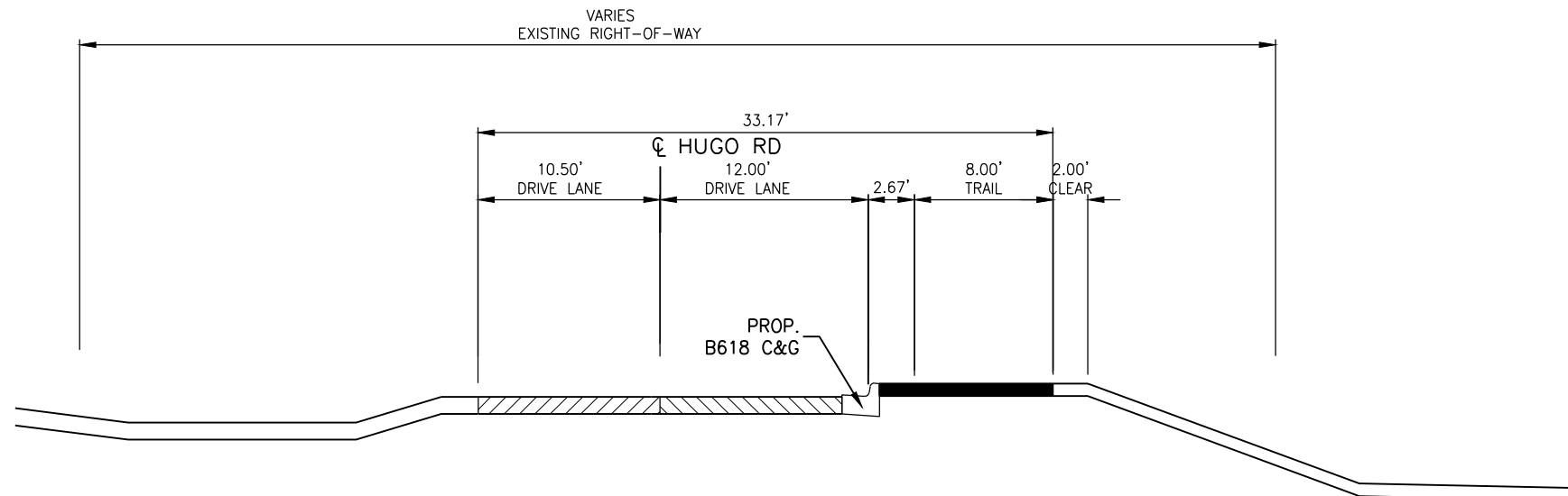
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BRUCE VENTO REGIONAL TRAIL – PHASE 2  
 TYPICAL SECTIONS  
 FINAL PRELIMINARY DESIGN

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TYPICAL SECTION  
HUGO RD - TAYLOR AVE TO BALD EAGLE RECREATION AREA:  
PROPOSED TWO WAY ALTERNATIVE

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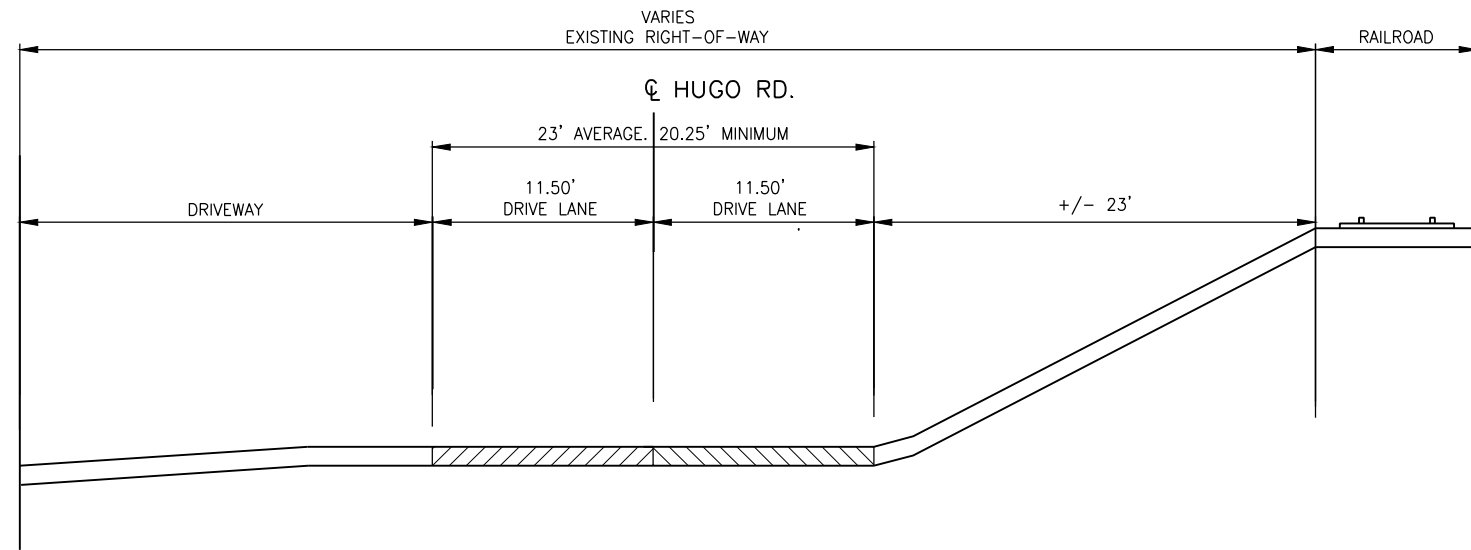
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BRUCE VENTO REGIONAL TRAIL - PHASE 2  
 TYPICAL SECTIONS  
 FINAL PRELIMINARY DESIGN

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TYPICAL SECTION  
 HUGO RD – TAYLOR AVE TO BALD EAGLE RECREATION AREA: EXISTING

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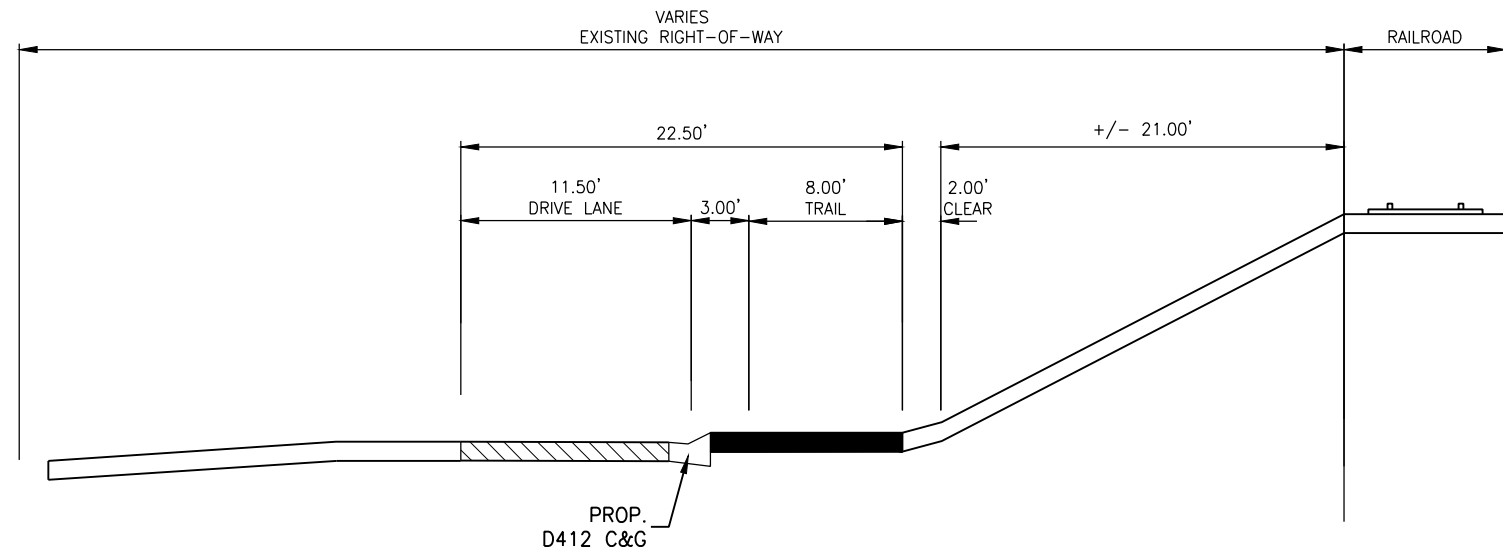


BRUCE VENTO REGIONAL TRAIL – PHASE 2

TYPICAL SECTIONS  
 FINAL PRELIMINARY DESIGN

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TYPICAL SECTION  
HUGO RD – TAYLOR AVE TO BALD EAGLE RECREATION AREA:  
PROPOSED ONE WAY ALTERNATIVE AT PINCH POINT

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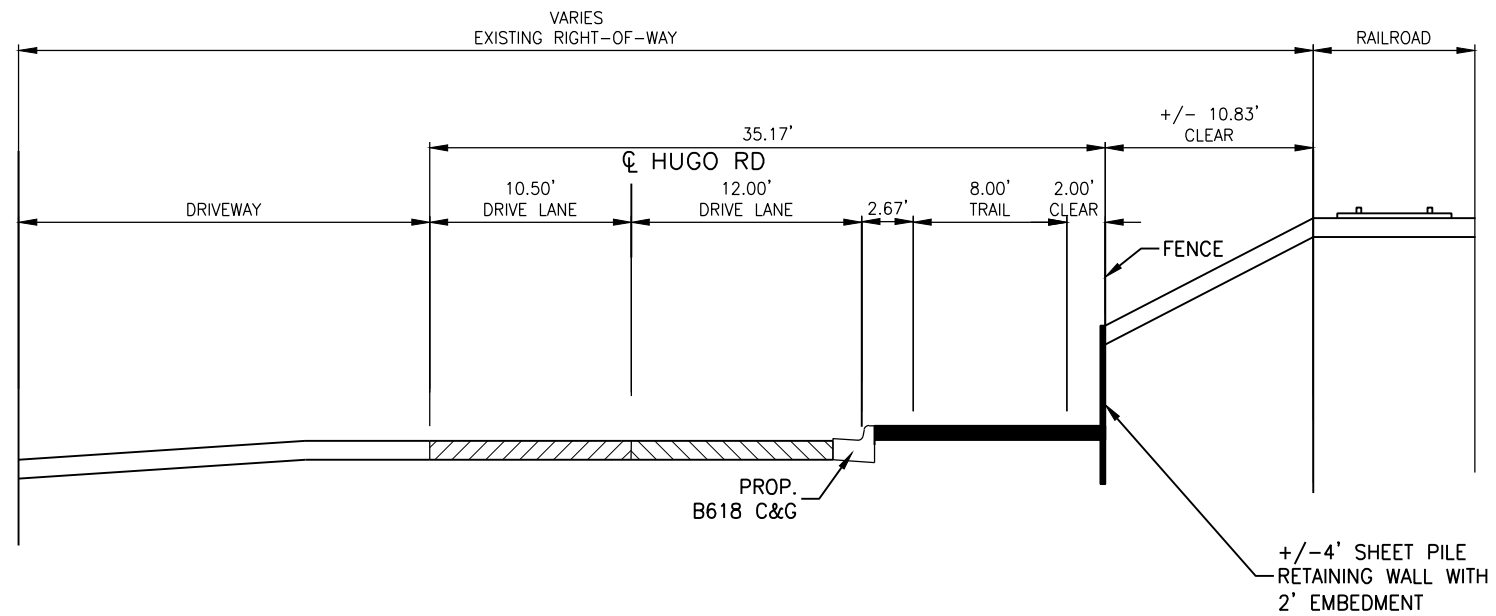
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BRUCE VENTO REGIONAL TRAIL – PHASE 2  
 TYPICAL SECTIONS  
 FINAL PRELIMINARY DESIGN

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TYPICAL SECTION  
 HUGO RD – TAYLOR AVE TO BALD EAGLE RECREATION AREA:  
 PROPOSED TWO WAY ALTERNATIVE AT PINCH POINT

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NO	DATE	BY	CKD	APPR

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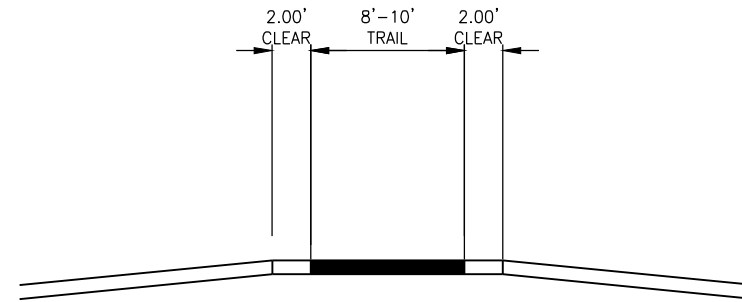


BRUCE VENTO REGIONAL TRAIL – PHASE 2

TYPICAL SECTIONS  
 FINAL PRELIMINARY DESIGN

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TYPICAL SECTION  
 TRAIL WITHIN BALD EAGLE RECREATION AREA: EXISTING

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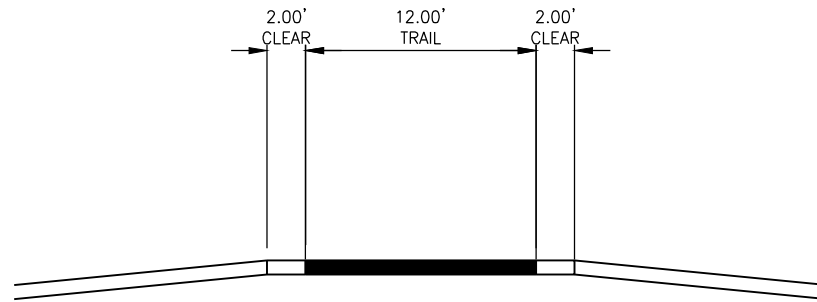
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BRUCE VENTO REGIONAL TRAIL - PHASE 2  
 TYPICAL SECTIONS  
 FINAL PRELIMINARY DESIGN

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TYPICAL SECTION  
 TRAIL WITHIN BALD EAGLE RECREATION AREA: PROPOSED

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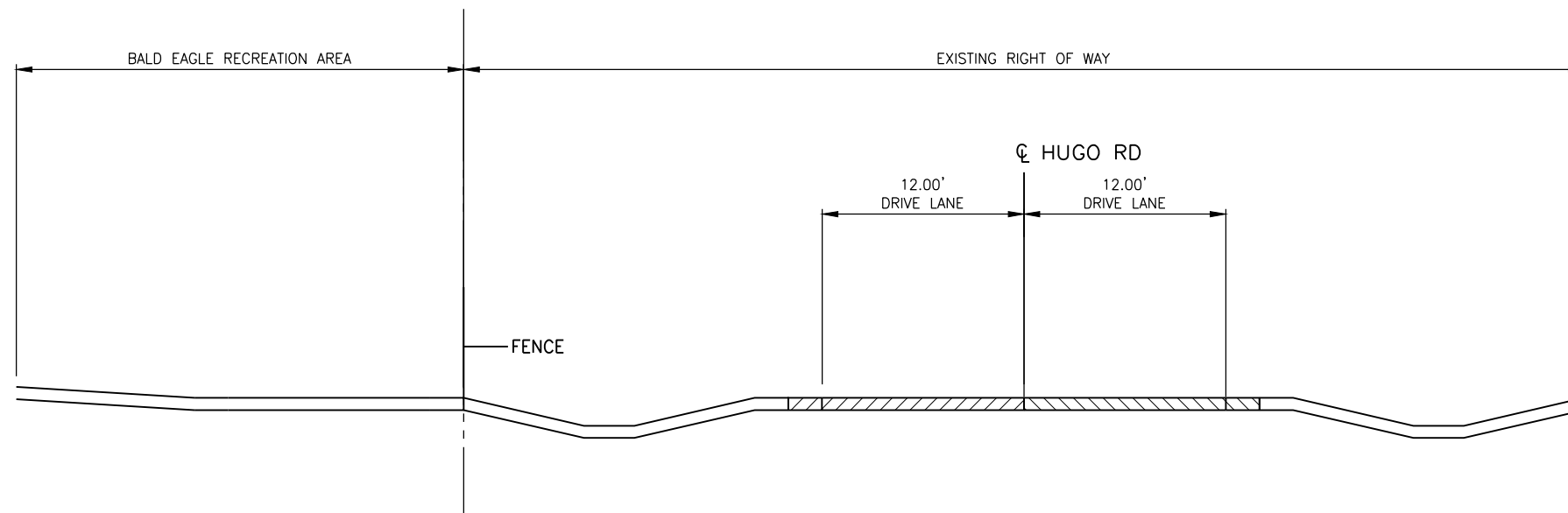
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BRUCE VENTO REGIONAL TRAIL – PHASE 2  
 TYPICAL SECTIONS  
 FINAL PRELIMINARY DESIGN

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TYPICAL SECTION  
HUGO RD – BALD EAGLE RECREATION AREA TO COUNTY ROAD J: EXISTING

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NO	DATE	BY	CKD	APPR

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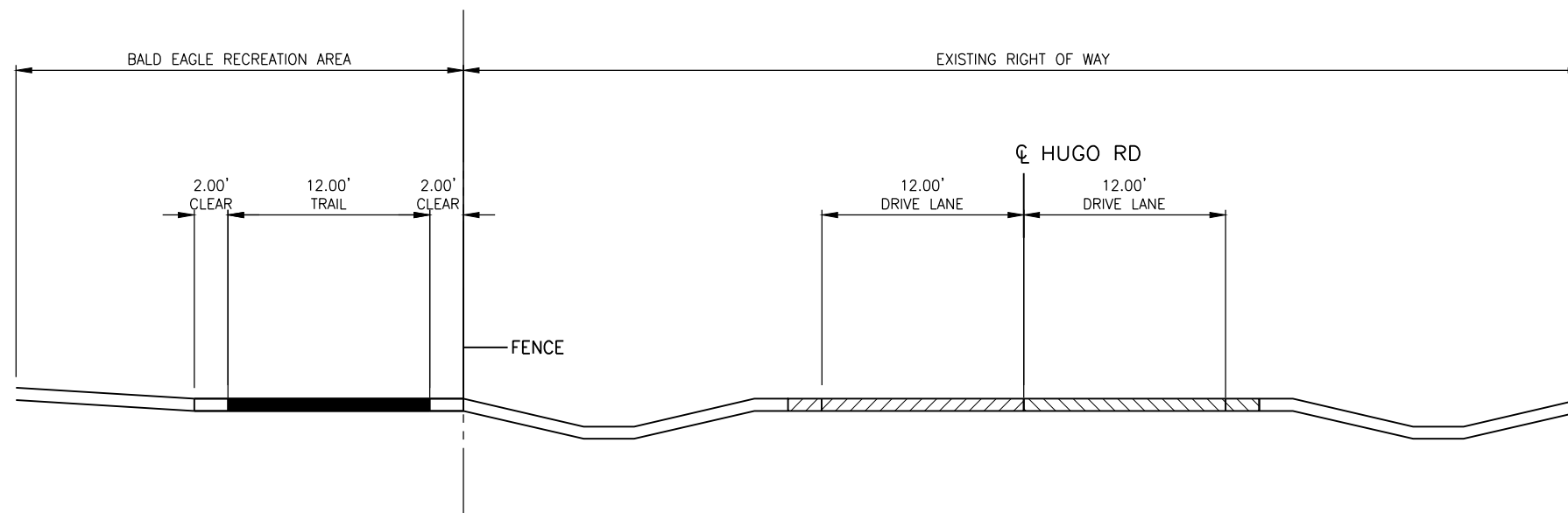
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BRUCE VENTO REGIONAL TRAIL – PHASE 2  
 TYPICAL SECTIONS  
 FINAL PRELIMINARY DESIGN

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TYPICAL SECTION  
 HUGO RD – BALD EAGLE RECREATION AREA TO COUNTY ROAD J: PROPOSED

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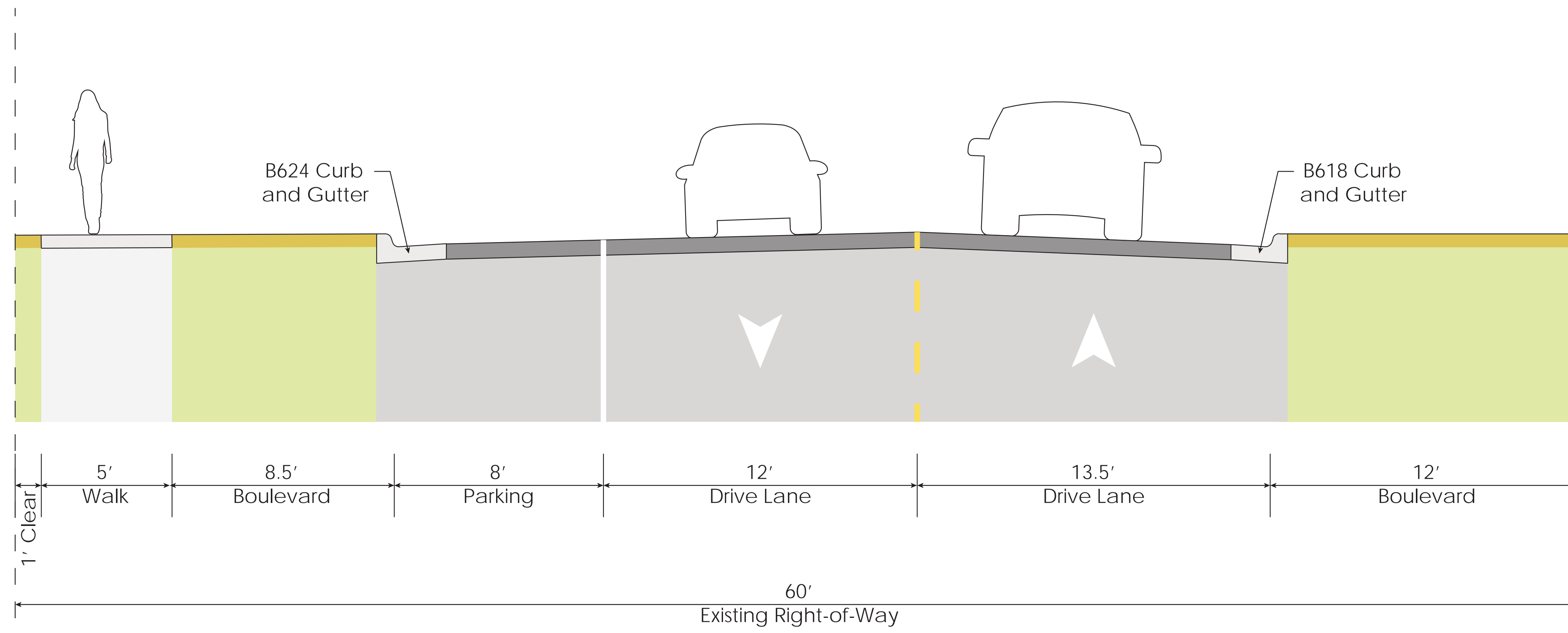
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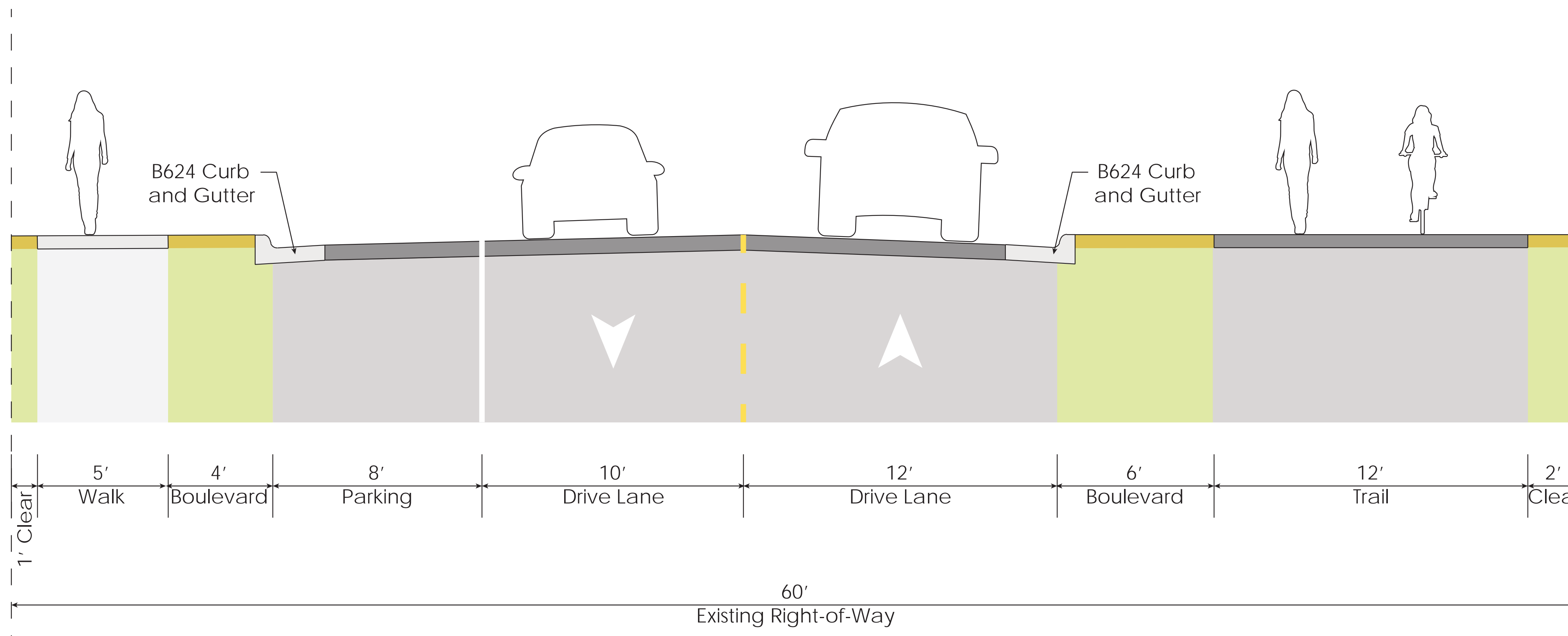
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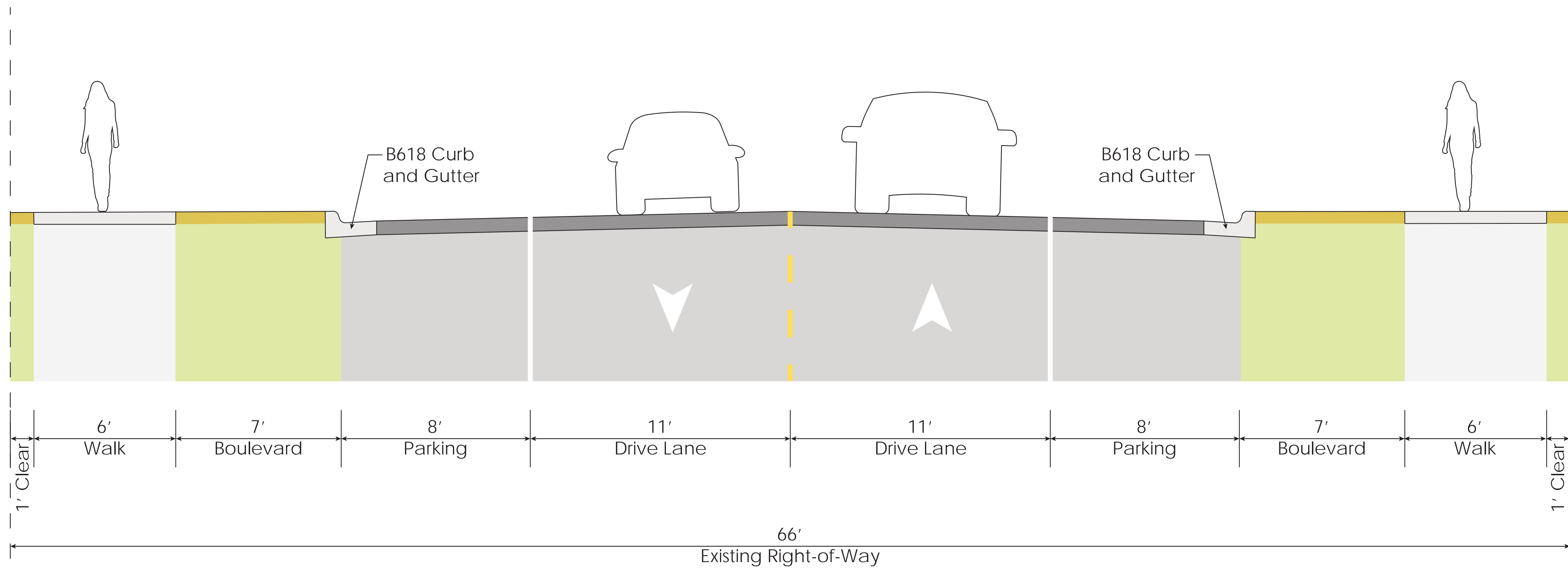


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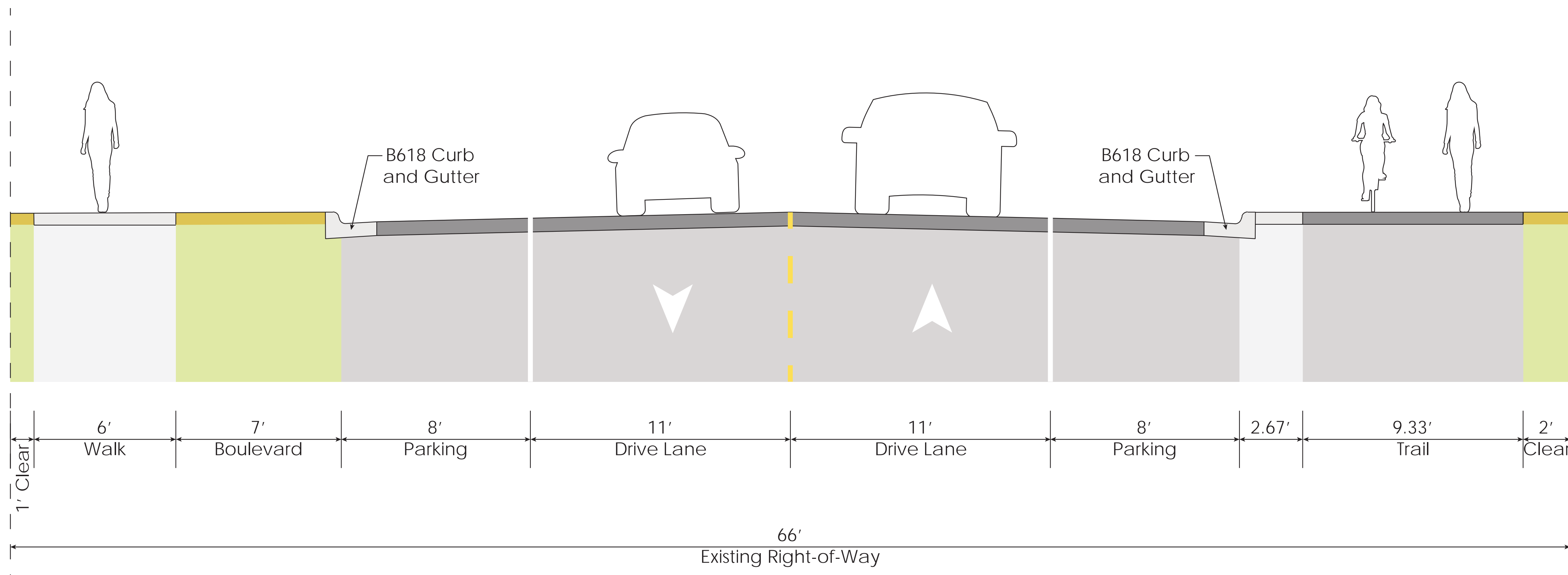


**LINCOLN AVE: PROPOSED**



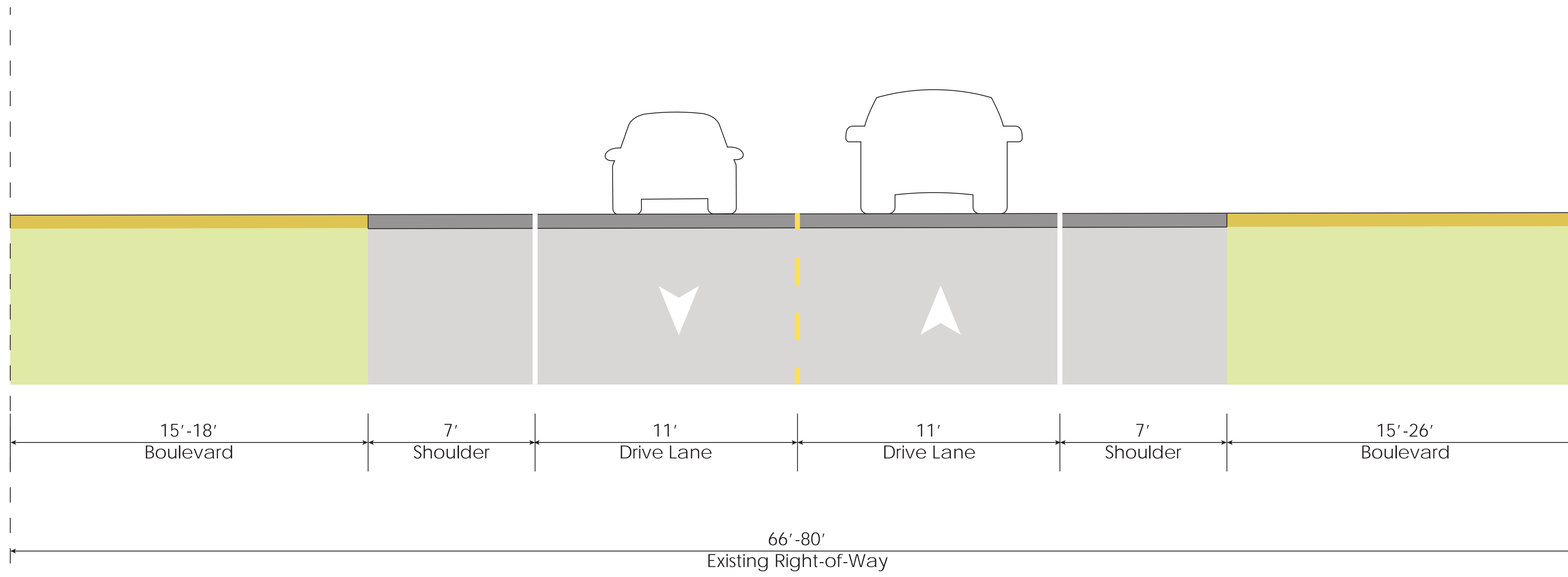


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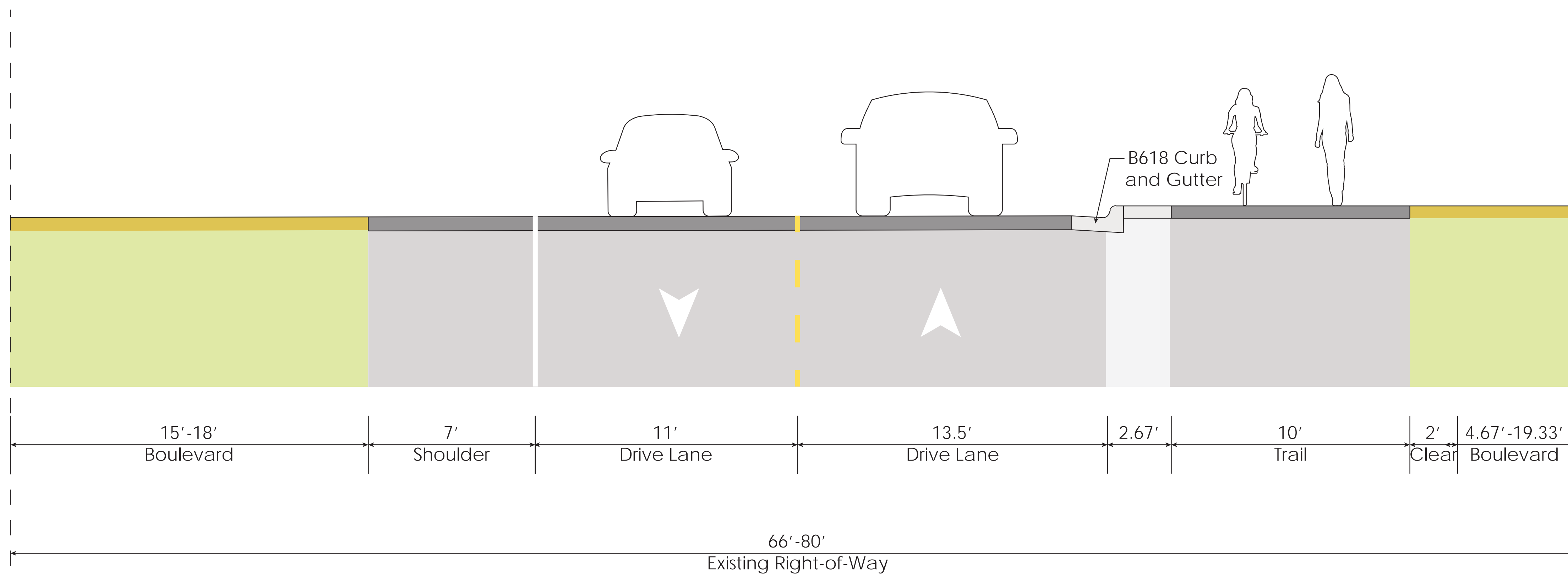


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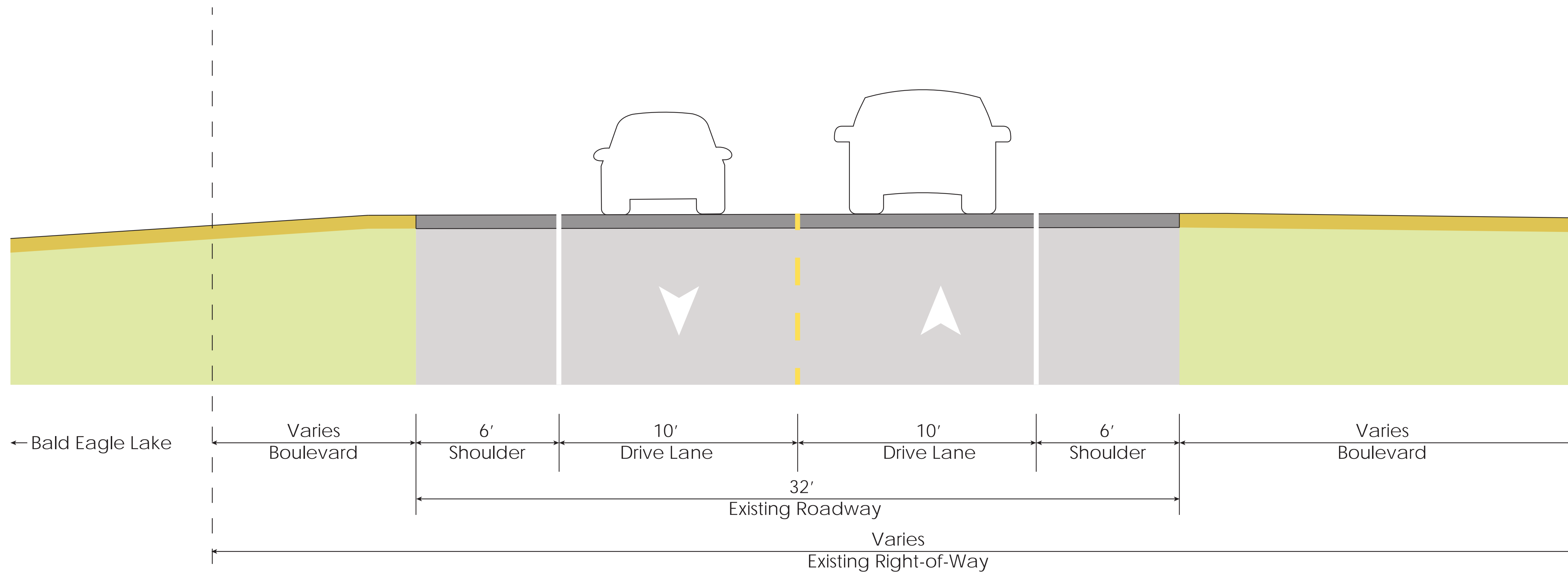


**BALD EAGLE AVE - WBLHS TO BALD EAGLE LAKE: EXISTING**

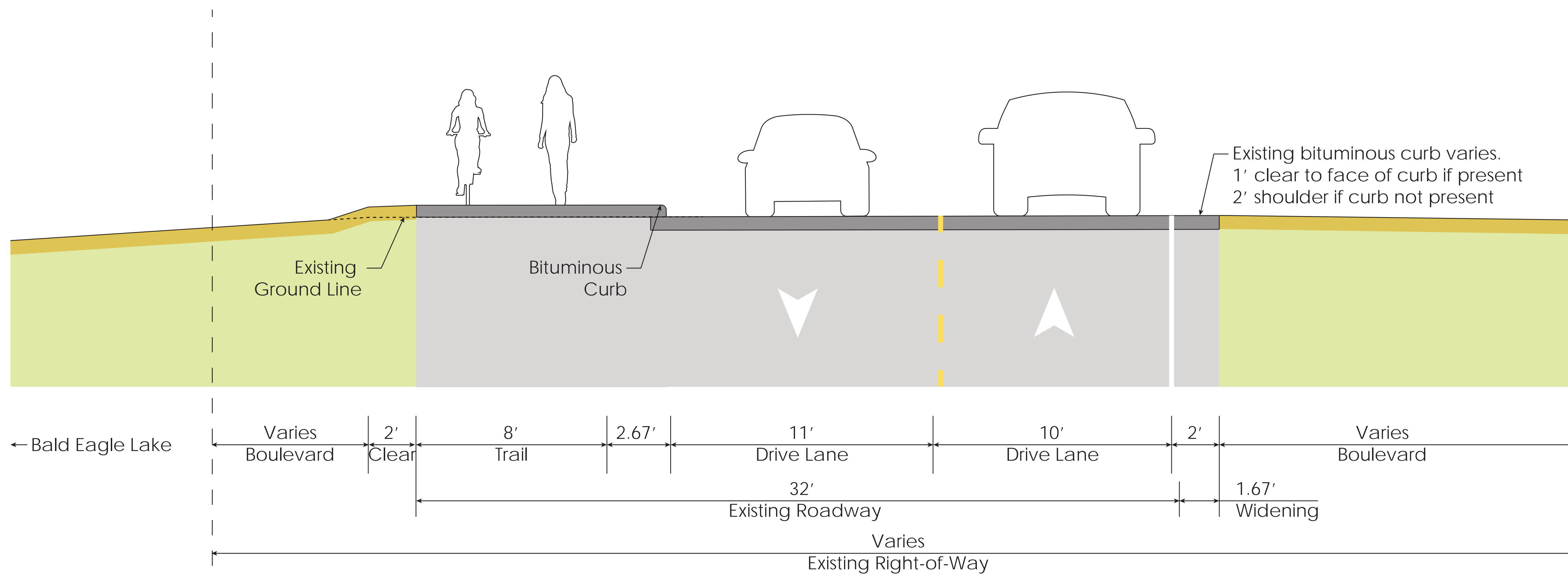


**BALD EAGLE AVE - WBLHS TO BALD EAGLE LAKE: PROPOSED**



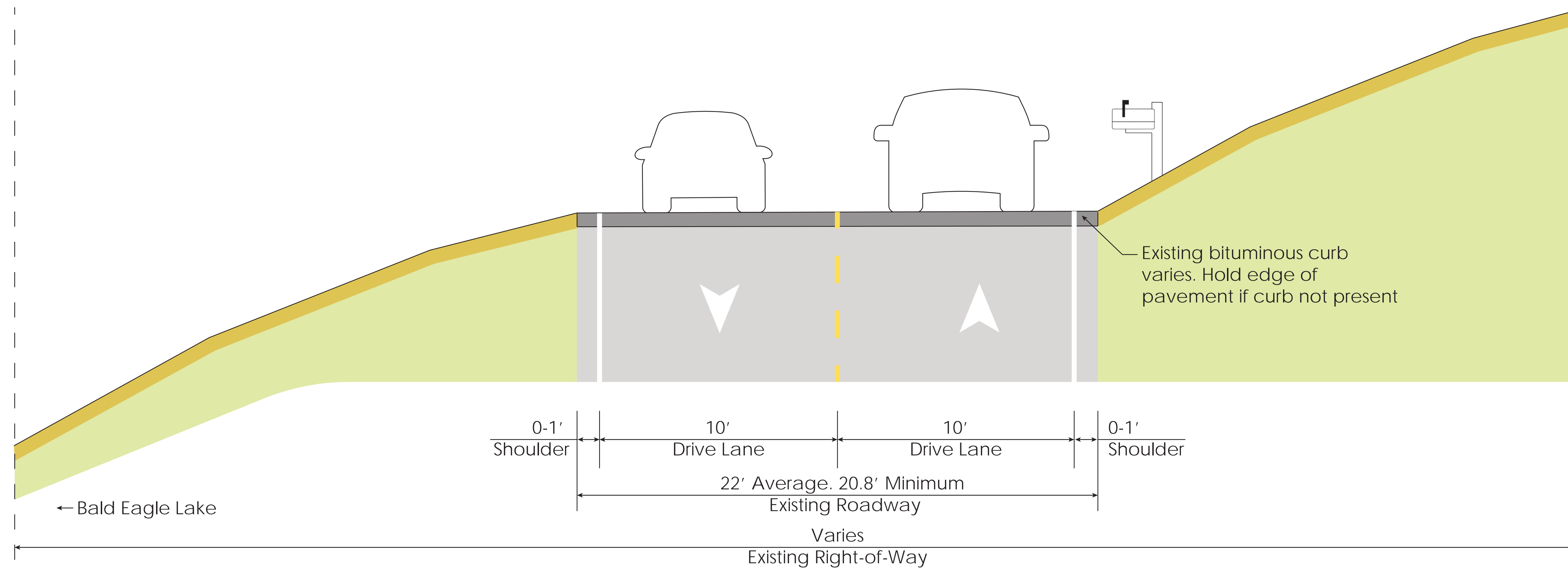


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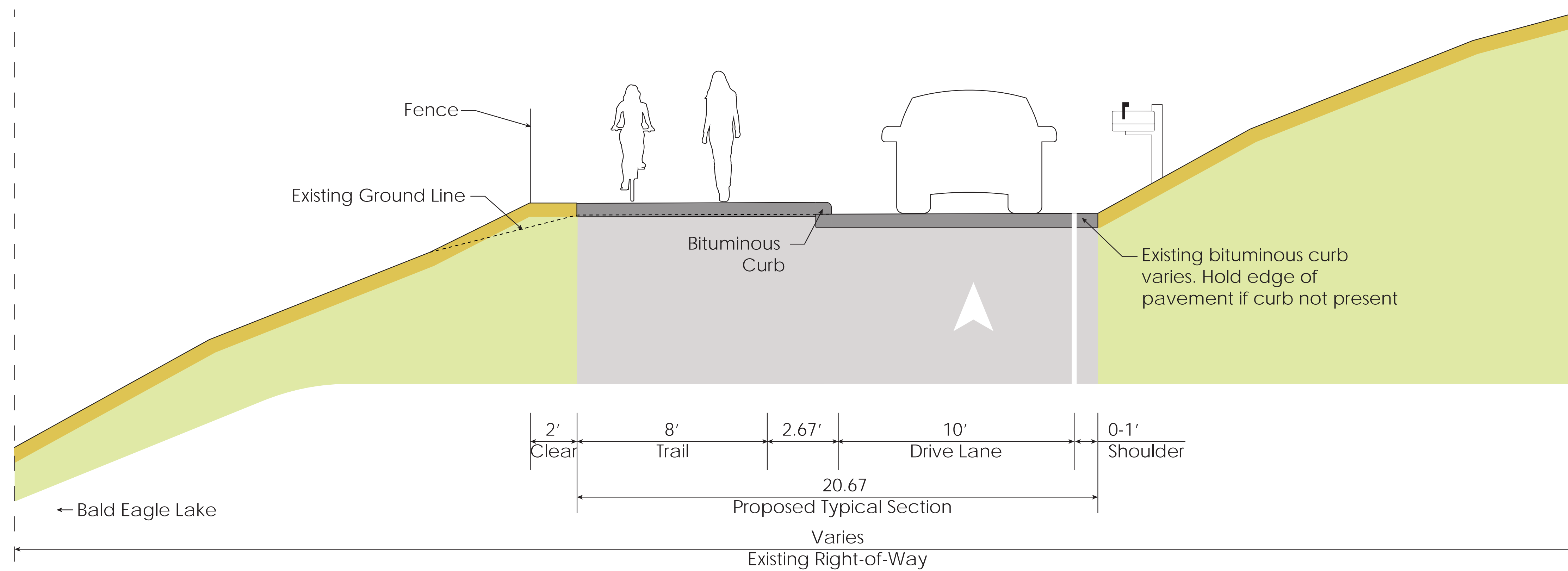


**BALD EAGLE BLVD- BALD EAGLE AVE TO PARK AVE: PROPOSED**



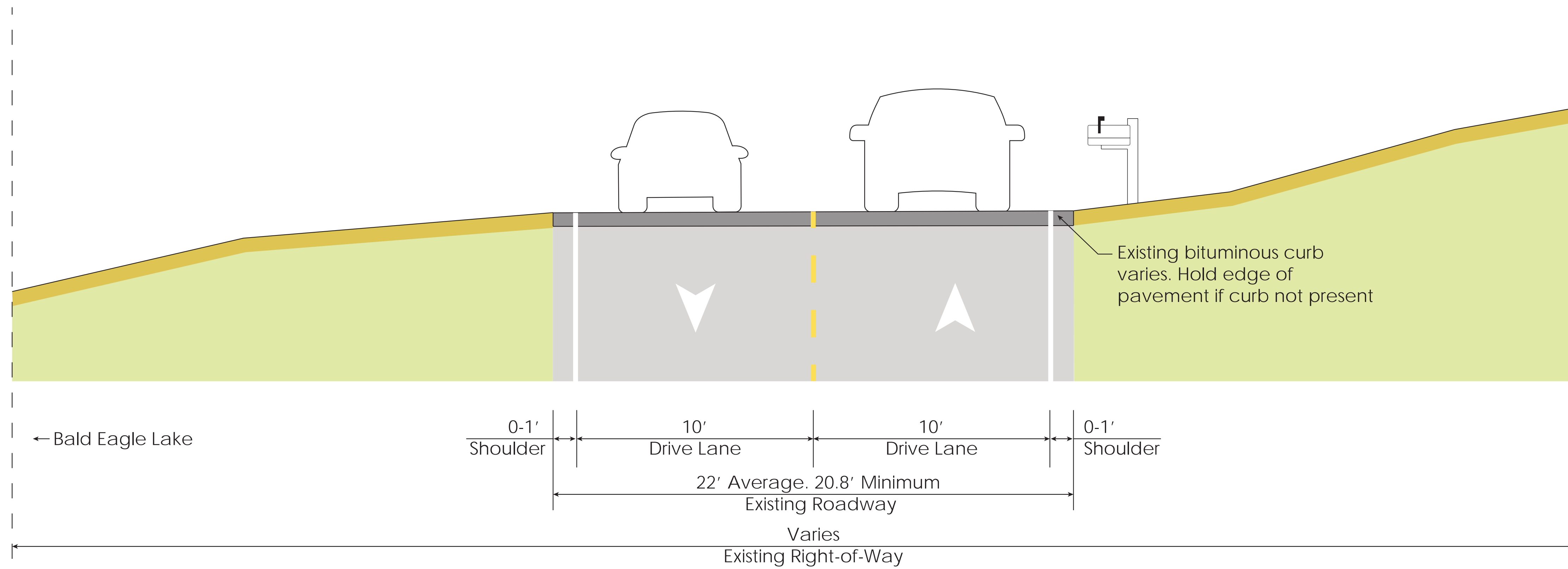


**BALD EAGLE BLVD- PARK AVE TO BUFFALO ST: EXISTING**

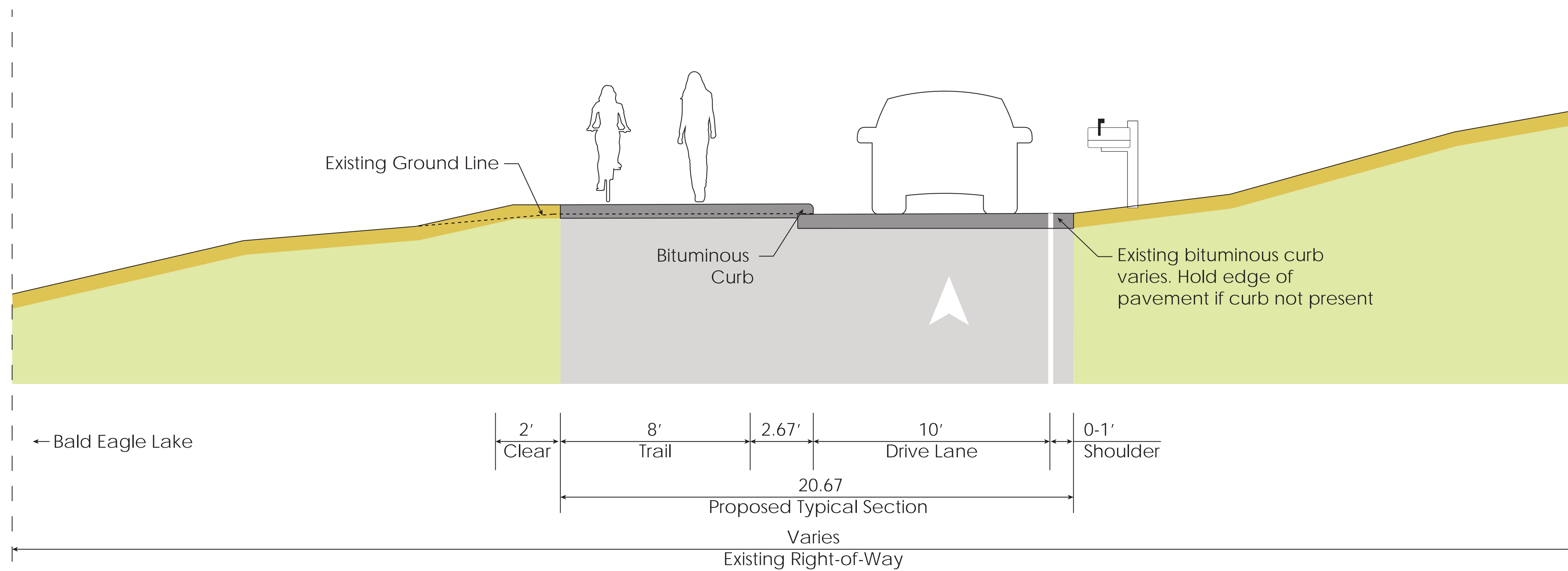


**BALD EAGLE BLVD- PARK AVE TO BUFFALO ST: PROPOSED**



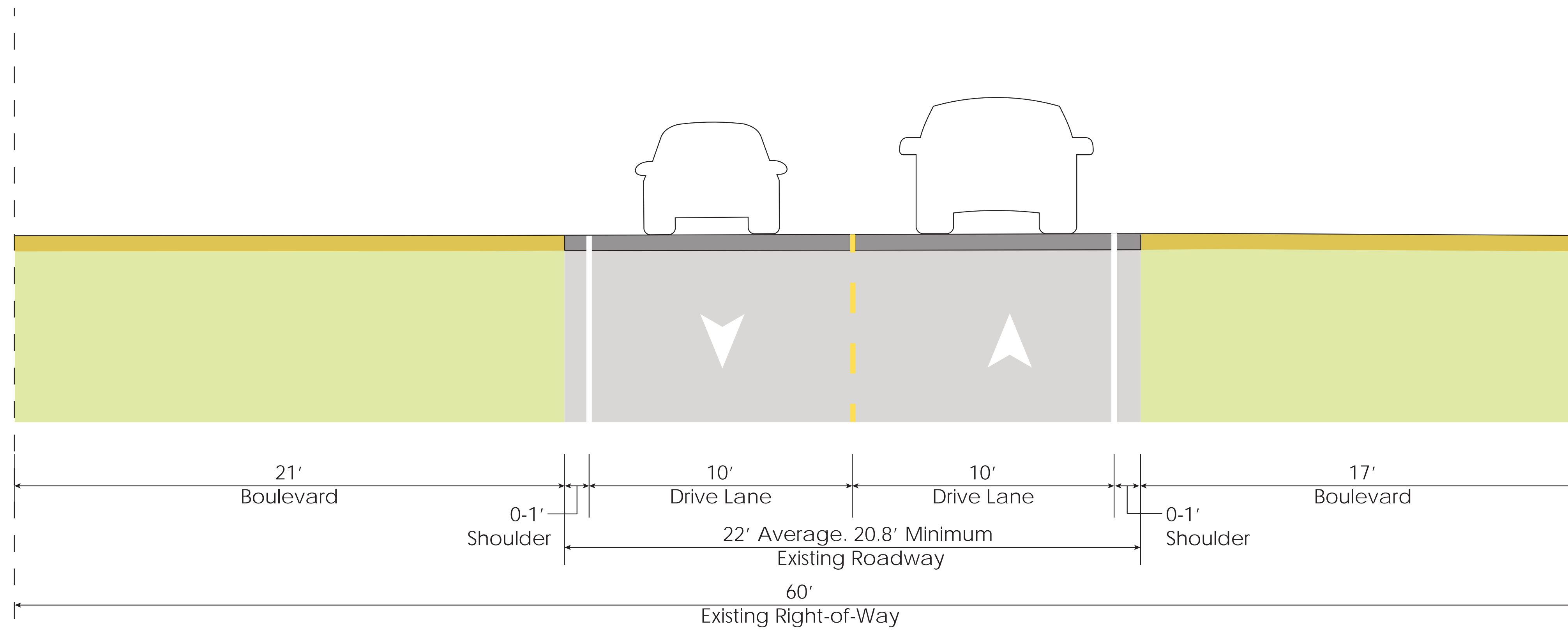


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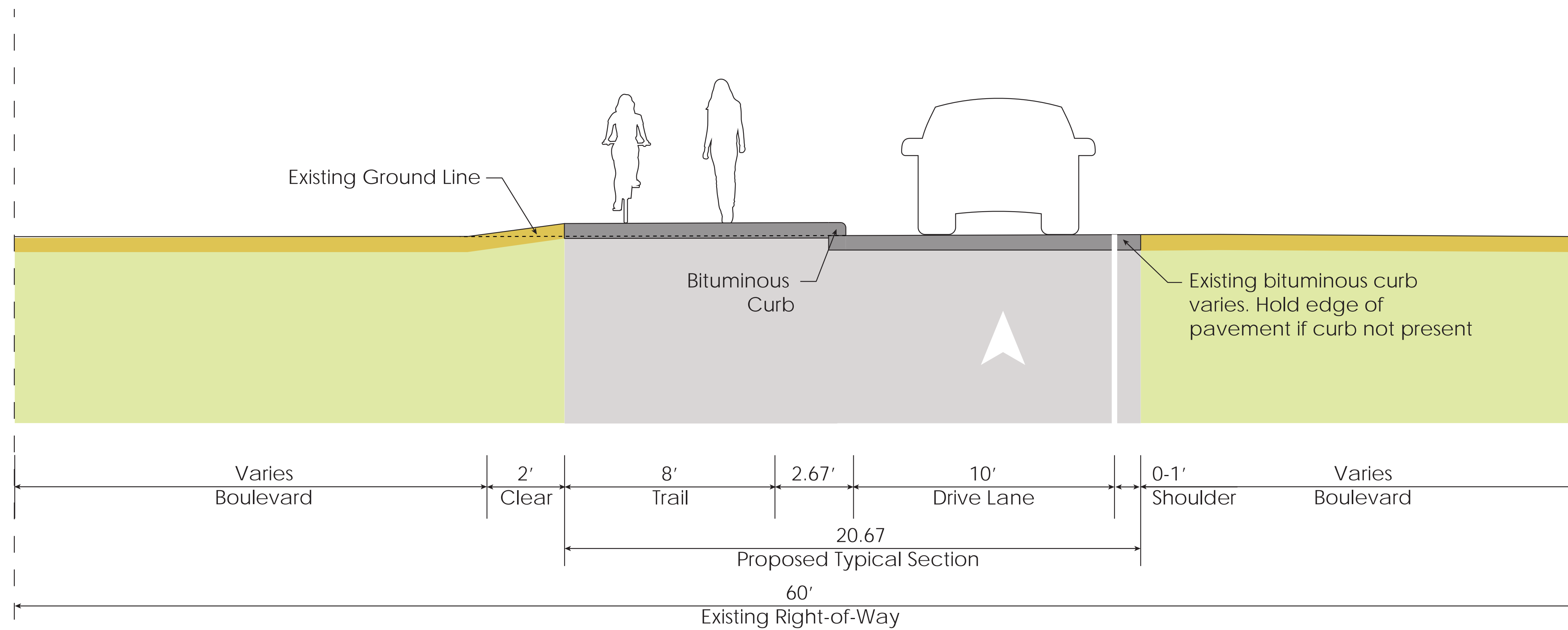


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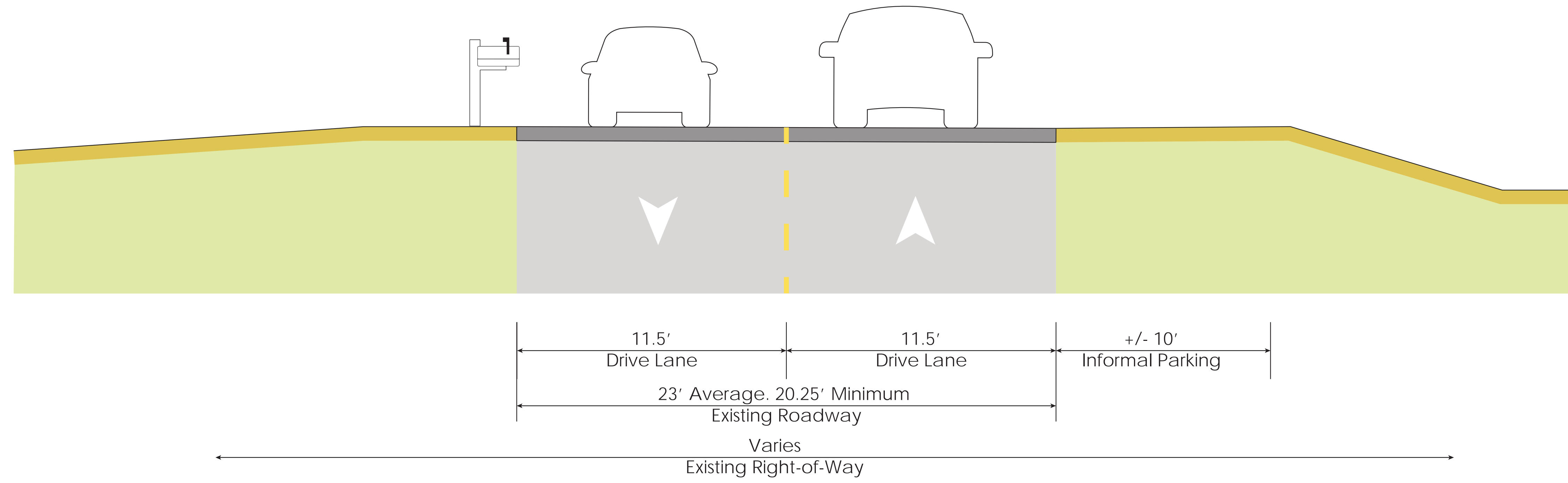


**TAYLOR AVE - BALD EAGLE BLVD TO HUGO RD: EXISTING**

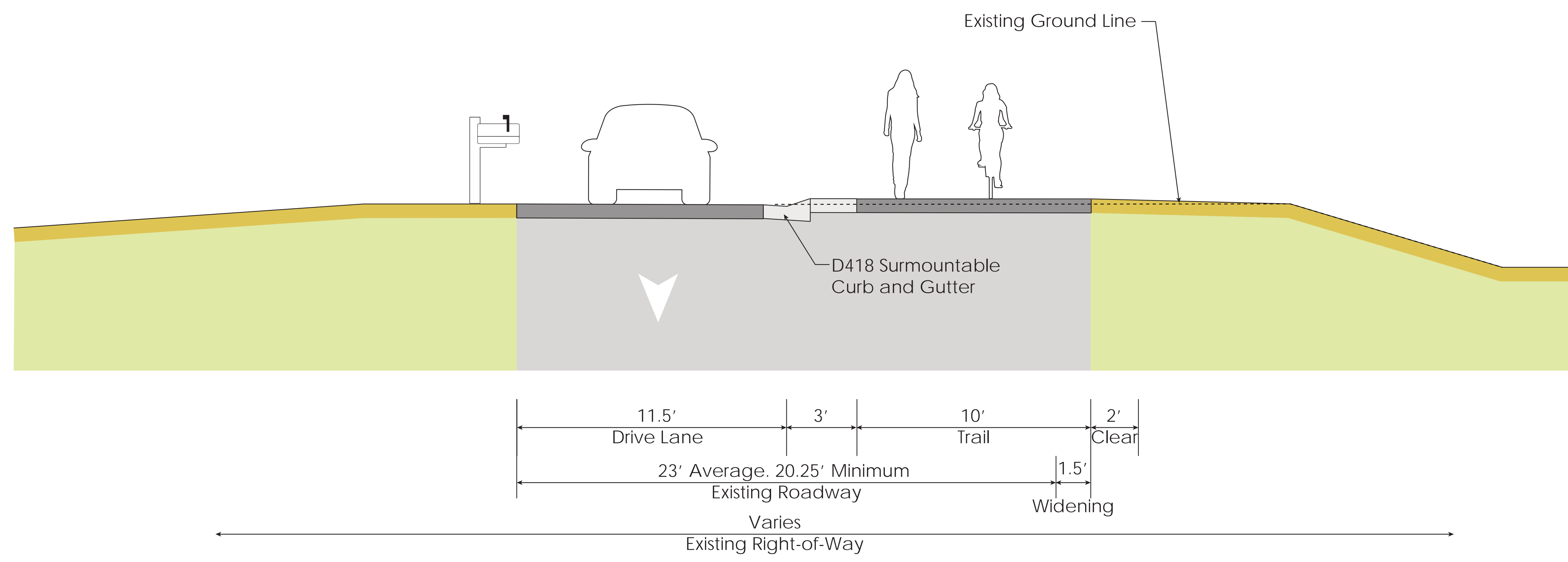


**TAYLOR AVE - BALD EAGLE BLVD TO HUGO RD: PROPOSED**



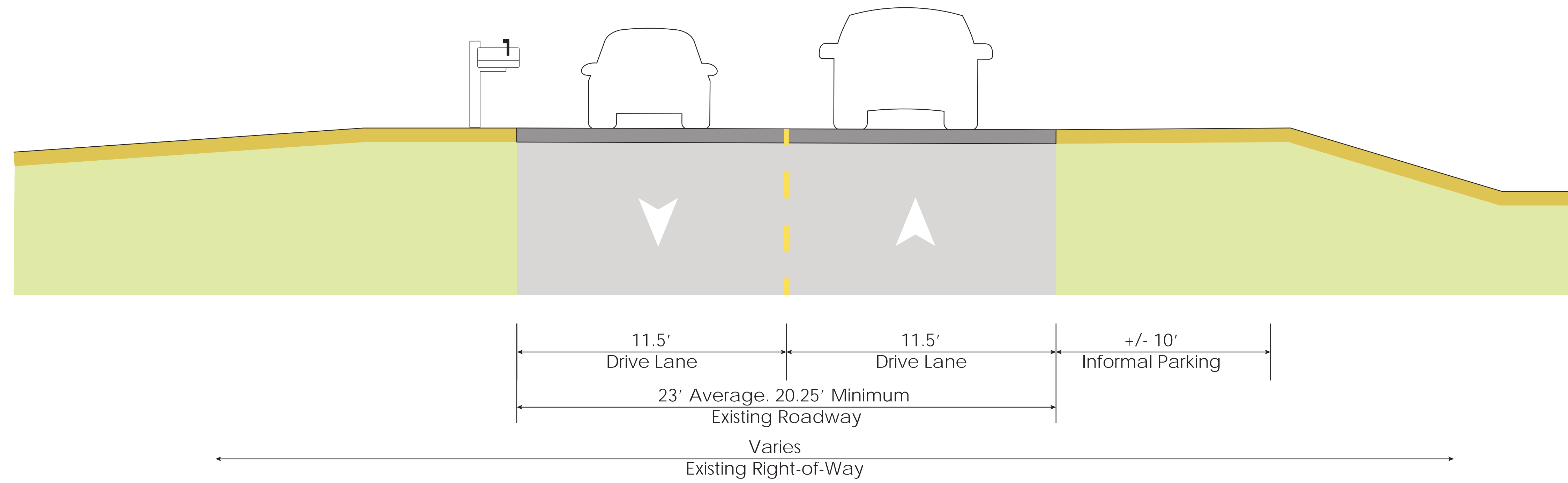


**HUGO RD - TAYLOR AVE TO BALD EAGLE RECREATION AREA: EXISTING**

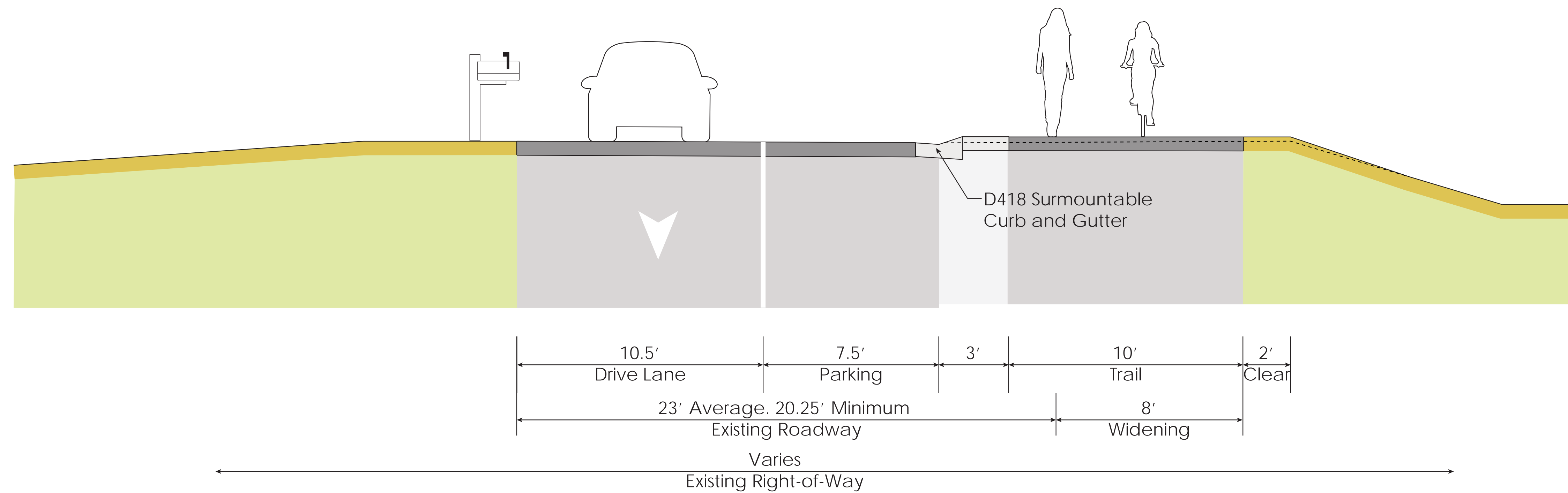


**HUGO RD - TAYLOR AVE TO BALD EAGLE RECREATION AREA:  
PROPOSED ONE WAY ALTERNATIVE**



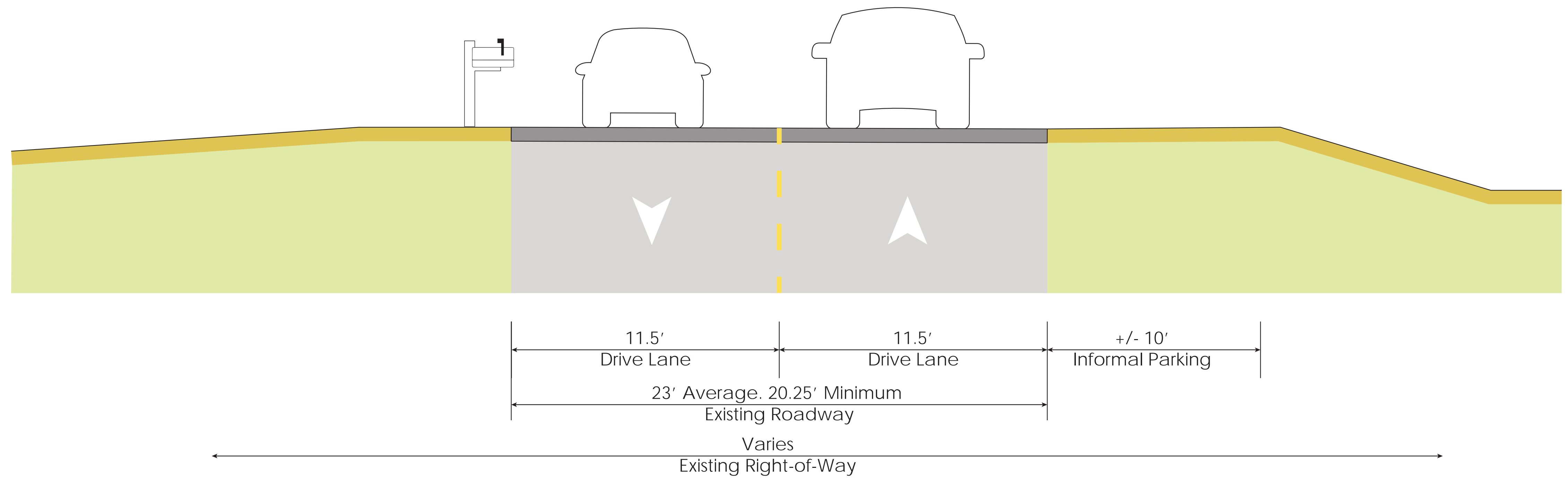


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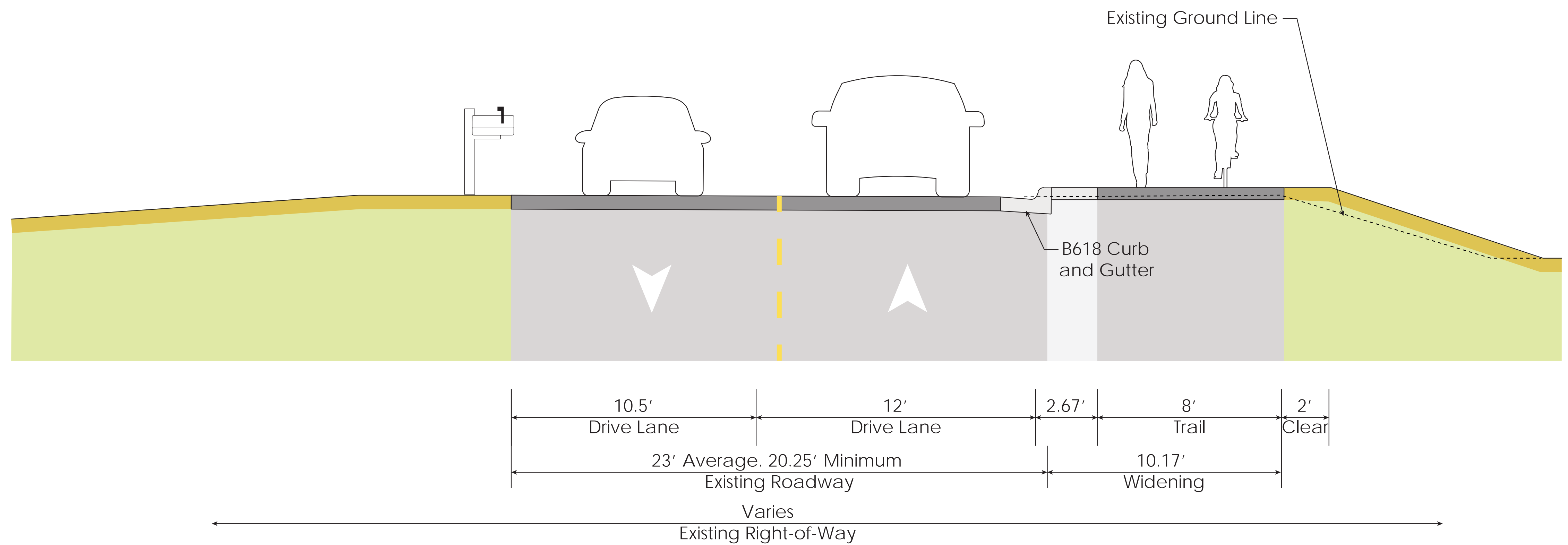


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PROPOSED ONE WAY ALTERNATIVE AT PARKING BAY**



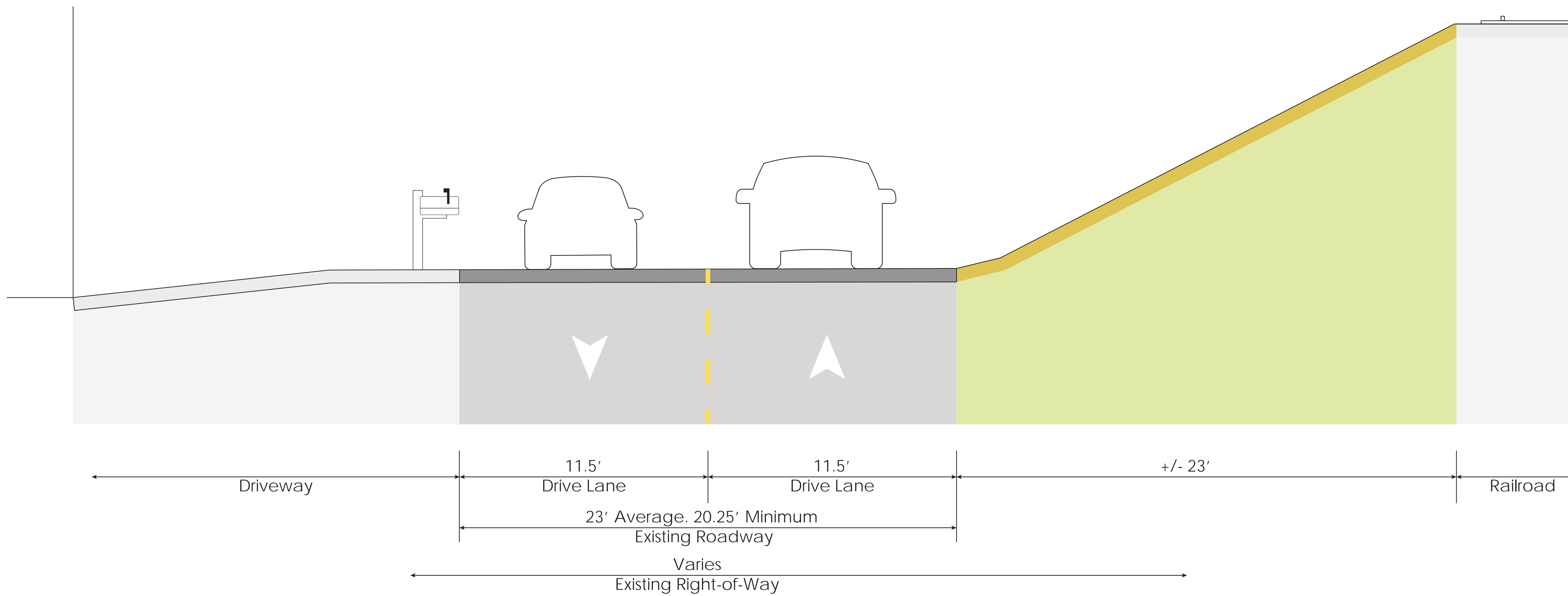


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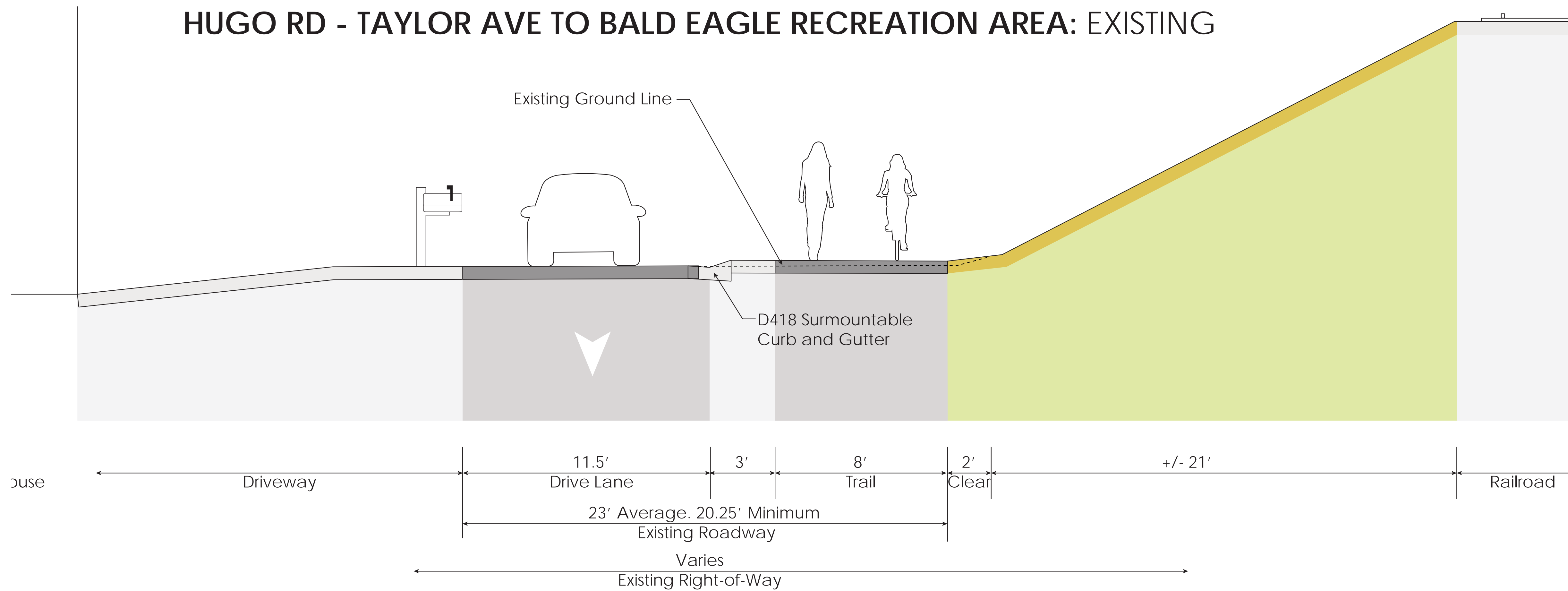


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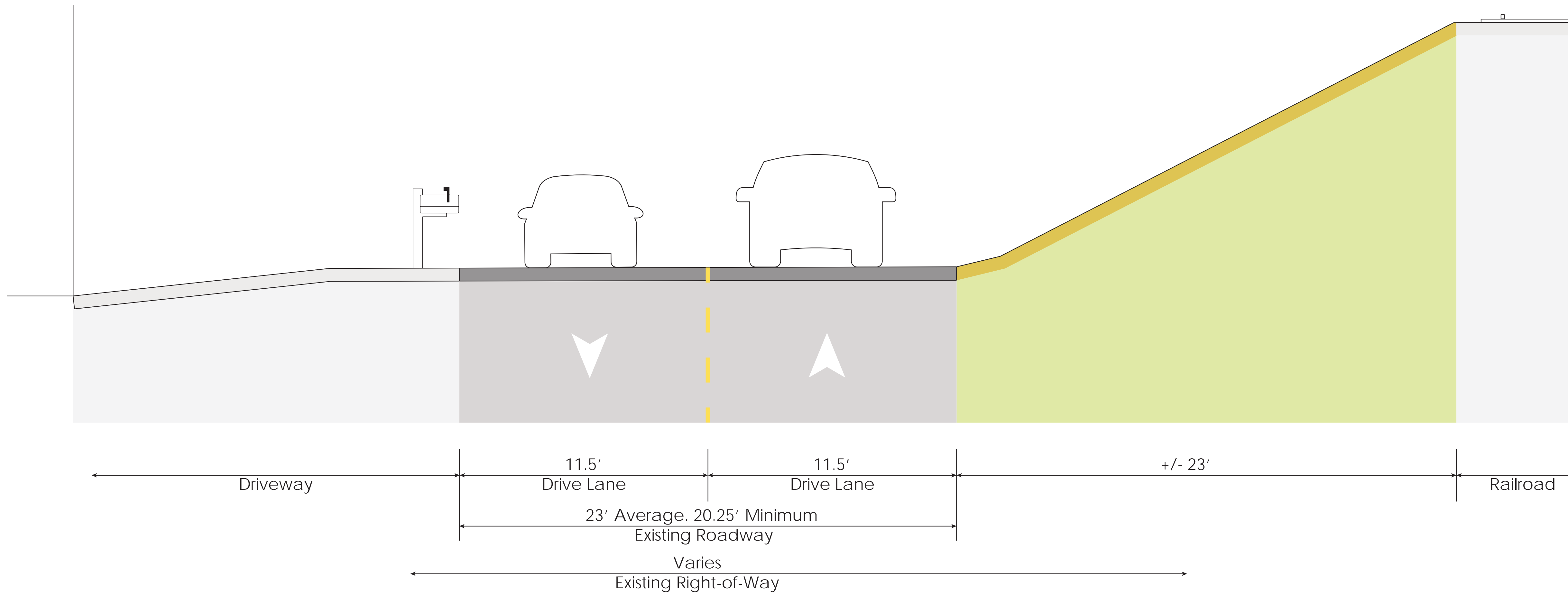


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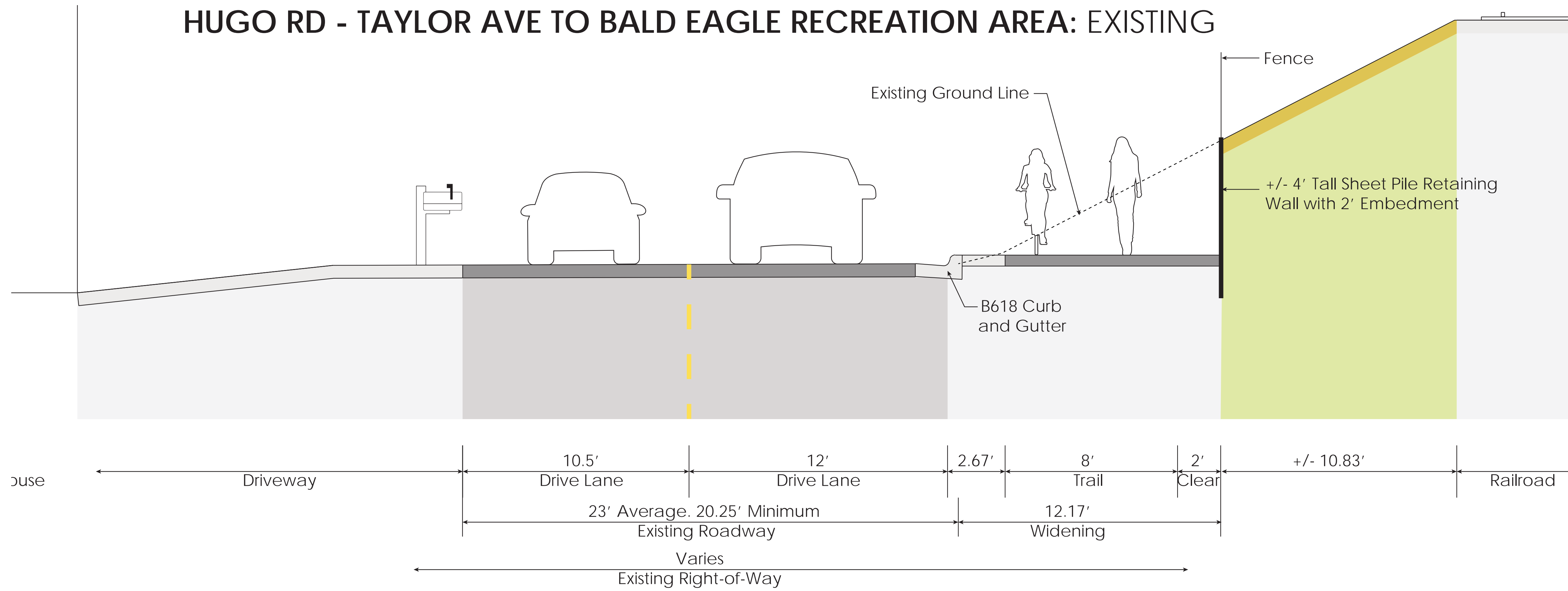


**HUGO RD - TAYLOR AVE TO BALD EAGLE RECREATION AREA:  
PROPOSED ONE WAY ALTERNATIVE AT PINCH POINT**



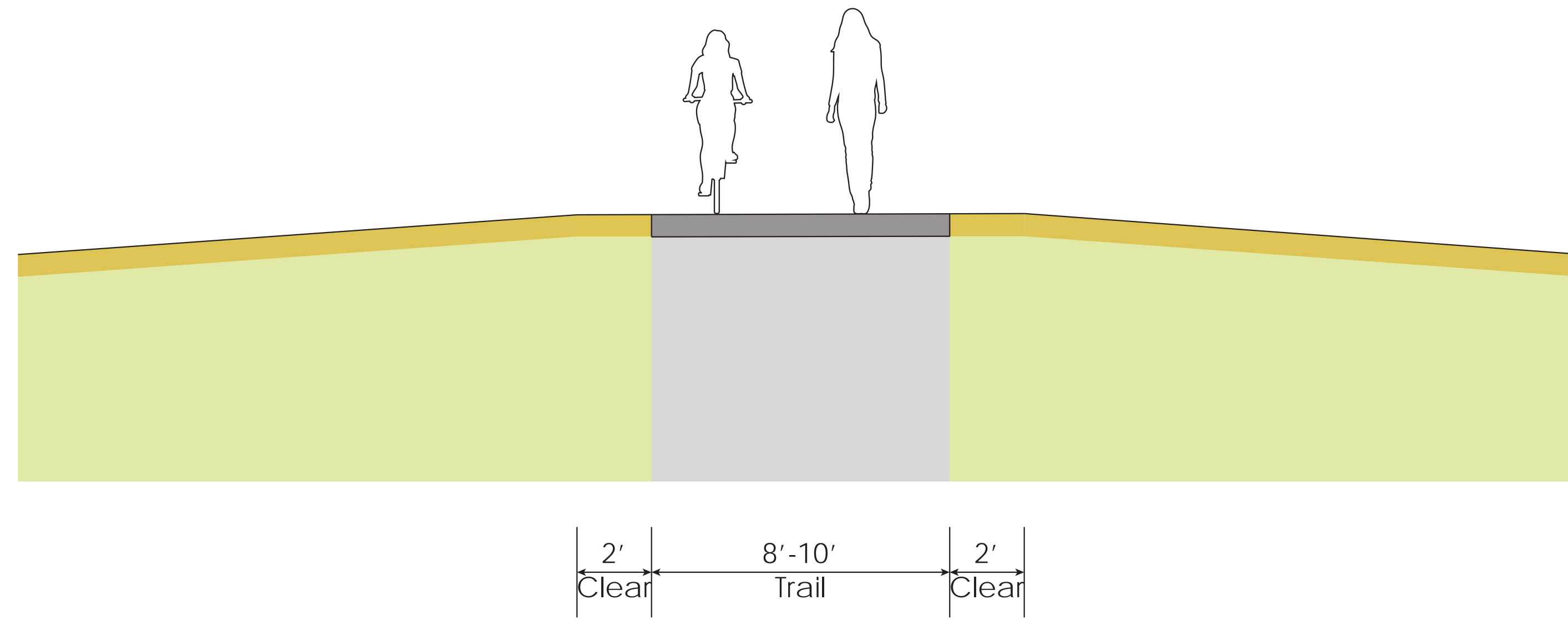


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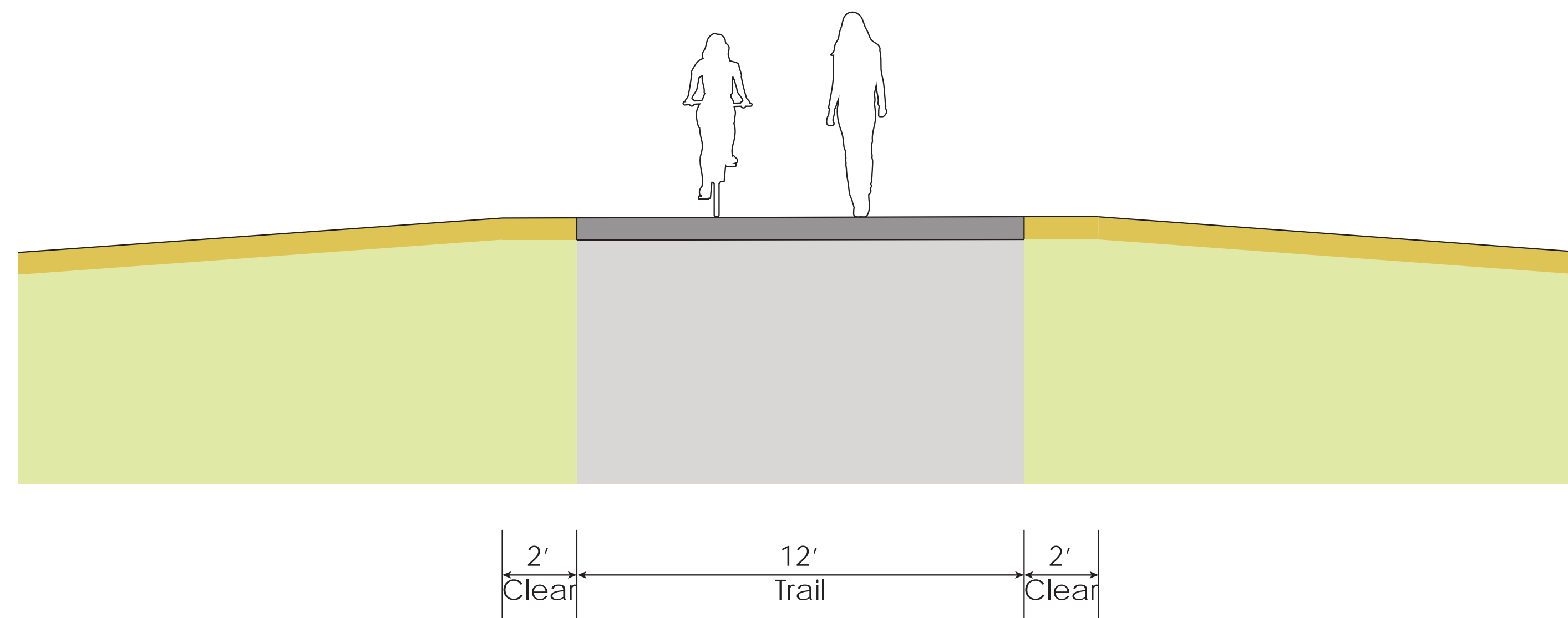


**HUGO RD - TAYLOR AVE TO BALD EAGLE RECREATION AREA:  
PROPOSED TWO WAY ALTERNATIVE AT PINCH POINT**



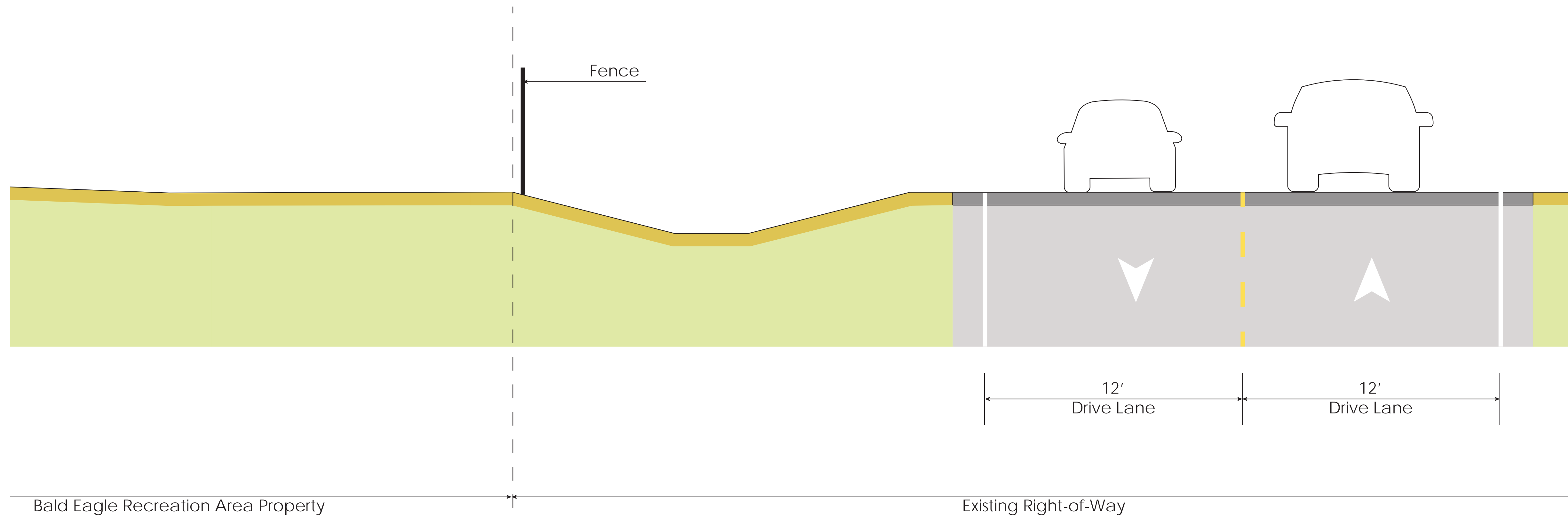


**TRAIL WITHIN BALD EAGLE RECREATION AREA: EXISTING**

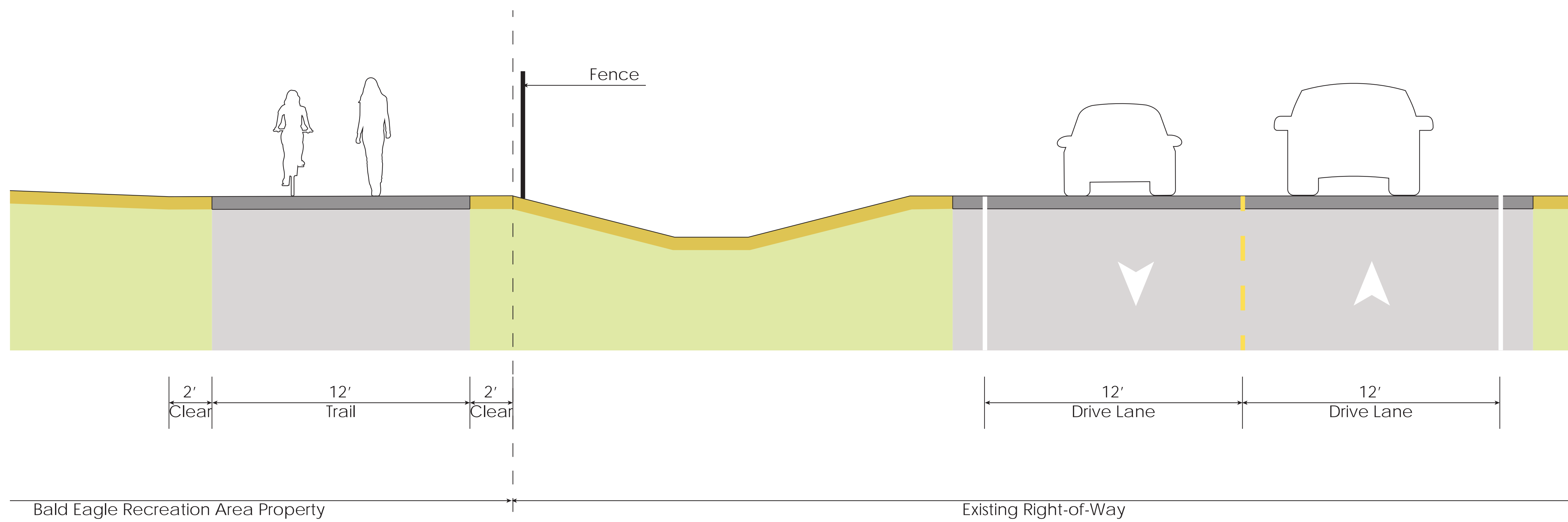


**TRAIL WITHIN BALD EAGLE RECREATION AREA: PROPOSED**





**HUGO RD - BALD EAGLE RECREATION AREA TO COUNTY ROAD J: EXISTING**



**HUGO RD - BALD EAGLE RECREATION AREA TO COUNTY ROAD J: PROPOSED**



**Appendix F - Cost Estimates**





Bruce Vento Regional Trail - Phase 2 - One Way Alternative on Hugo Road

Concept Cost Estimate (based upon 2020 bid price information)

Prepared By: SRF Consulting Group, Inc., June 14, 2022

			Lincoln Ave Start to Hwy 96		Bald Eagle Ave Hwy 96 to WBLHS		Bald Eagle Ave WBLHS to Bald Eagle Blvd		Bald Eagle Blvd Bald Eagle Ave to Park Ave		Bald Eagle Blvd & Taylor Ave Park Ave to Hugo Rd - TRAIL		Bald Eagle Blvd & Taylor Ave Park Ave to Hugo Rd - SIDE STREETS		Hugo Road Taylor Ave to Bald Eagle Lake Park		Bald Eagle Lake Park Parking Lot to Cty Road J		TOTAL			
ITEM DESCRIPTION	UNIT	UNIT PRICE	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT		
<b>PAVING AND GRADING COSTS</b>																						
GrP 1a	2106 Excavation - common & subgrade	cu. yd.	\$10.00	5,880	\$58,800					240	\$2,400	120	\$1,200	840	\$8,400	1,080	\$10,800			8,160	\$81,600	
GrP 2d	2106 Granular Subgrade (CV)	cu. yd.	\$15.00	3,960	\$59,400					240	\$3,600	120	\$1,800	600	\$9,000	720	\$10,800			5,640	\$84,600	
GrP 3g	Local & Frontage Road Pavement	(1) sq. yd.	\$20.00	4,900	\$98,000					200	\$4,000	100	\$2,000	650	\$13,000	850	\$17,000			6,700	\$134,000	
GrP 4a	Concrete Walk / Trail / Median	(2) sq. yd.	\$75.00	750	\$56,250	450	\$33,750									50	\$3,750			1,250	\$93,750	
GrP 4b	Bituminous Walk / Trail	(2) sq. yd.	\$40.00	2,000	\$80,000	5,800	\$232,000	3,350	\$134,000	1,350	\$54,000	4,900	\$196,000			3,500	\$140,000	4,800	\$192,000	25,700	\$1,028,000	
GrP 4c	ADA Pedestrian Curb Ramp	each	\$1800.00	10	\$18,000	14	\$25,200	8	\$14,400	2	\$3,600	3	\$5,400			2	\$3,600	6	\$10,800	45	\$81,000	
GrP 5	Concrete Curb and Gutter	lin. ft.	\$27.00	3,250	\$87,750	250	\$6,750	2,700	\$72,900	1,150	\$31,050	2,300	\$62,100	2,450	\$66,150	3,200	\$86,400			15,300	\$413,100	
GrP 8a	Removals - Pavement	sq. yd.	\$6.00	6,100	\$36,600	4,150	\$24,900	2,600	\$15,600	1,500	\$9,000	5,700	\$34,200	800	\$4,800	3,900	\$23,400	1,550	\$9,300	26,300	\$157,800	
<b>SUBTOTAL PAVING AND GRADING COSTS:</b>					<b>\$494,800</b>		<b>\$322,600</b>		<b>\$236,900</b>		<b>\$107,650</b>		<b>\$302,700</b>		<b>\$101,350</b>		<b>\$295,750</b>		<b>\$212,100</b>		<b>\$2,073,850</b>	
<b>DRAINAGE, UTILITIES AND EROSION CONTROL</b>																						
Dr 3	Water Quality Ponds & Other BMPs	l.s.		\$50,000		\$50,000		\$20,000											\$80,000		\$200,000	
Dr 5	Drainage - urban	l.s.		\$100,000		\$150,000		\$150,000		\$25,000		\$25,000		\$25,000		\$25,000		\$25,000			\$500,000	
Dr 6	Drainage - rural	(3) mile	\$100,000															0.70	\$70,000	1	\$70,000	
Dr 7	Turf Establishment & Erosion Control	10%		\$50,000		\$33,000		\$24,000		\$11,000		\$31,000		\$11,000		\$30,000				\$22,000	\$212,000	
Dr 8	Landscaping & Site Amenities	10%		\$50,000		\$33,000		\$24,000		\$11,000		\$31,000		\$11,000		\$30,000				\$22,000	\$212,000	
<b>SUBTOTAL DRAINAGE, UTILITIES AND EROSION CONTROL</b>					<b>\$250,000</b>		<b>\$266,000</b>		<b>\$218,000</b>		<b>\$47,000</b>		<b>\$87,000</b>		<b>\$47,000</b>		<b>\$85,000</b>		<b>\$194,000</b>		<b>\$1,194,000</b>	
<b>RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS</b>																						
RW 1	Box Culvert -	(4) lin. ft.	\$1,000																			
RW 32	Steel Sheet Pile Wall & Safety Fencing	sq. ft.	\$125																			
<b>SUBTOTAL RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS:</b>																						
<b>SIGNAL AND LIGHTING COSTS</b>																						
SGL 1	Railroad Signal System Relocation/Upgrade on Bald Eagle Ave	each	\$250,000					1	\$250,000											1	\$250,000	
<b>SUBTOTAL SIGNAL AND LIGHTING COSTS:</b>									<b>\$250,000</b>													<b>\$250,000</b>
<b>SIGNING &amp; STRIPING COSTS</b>																						
SGN 1	Mainline Signing (C&D)	mile	\$17,500	0.3	\$5,250	0.9	\$15,750	0.5	\$8,750	0.3	\$5,250	0.9	\$15,750	0.1	\$1,750	0.5	\$8,750	0.7	\$12,250	4.2	\$73,500	
SGN 2	Mainline Striping	mile	\$2,500	0.3	\$750	0.9	\$2,250	0.5	\$1,250	0.3	\$750	0.9	\$2,250	0.1	\$250	0.5	\$1,250	0.7	\$1,750	4.2	\$10,500	
<b>SUBTOTAL SIGNING &amp; STRIPING COSTS:</b>					<b>\$6,000</b>		<b>\$18,000</b>		<b>\$10,000</b>		<b>\$6,000</b>		<b>\$18,000</b>		<b>\$2,000</b>		<b>\$10,000</b>		<b>\$14,000</b>		<b>\$84,000</b>	
<b>SUBTOTAL CONSTRUCTION COSTS:</b>					<b>\$750,800</b>		<b>\$606,600</b>		<b>\$714,900</b>		<b>\$160,650</b>		<b>\$407,700</b>		<b>\$150,350</b>		<b>\$390,750</b>		<b>\$420,100</b>		<b>\$3,601,850</b>	
<b>MISCELLANEOUS COSTS</b>																						
M 1	Mobilization	10%		\$76,000		\$61,000		\$72,000		\$17,000		\$41,000		\$16,000		\$40,000		\$43,000		\$366,000		
M 2	Non Quantified Minor Items	10%		\$76,000		\$61,000		\$72,000		\$17,000		\$41,000		\$16,000		\$40,000		\$43,000		\$366,000		
M 7	Utility Relocation (Bald Eagle Ave & Hugo Road)	10%		\$76,000		\$61,000		\$72,000		\$17,000		\$41,000		\$16,000		\$40,000		\$43,000		\$366,000		
M 8	Traffic Control	3%		\$23,000		\$19,000		\$22,000		\$5,000		\$13,000		\$5,000		\$12,000		\$13,000		\$112,000		
<b>SUBTOTAL MISCELLANEOUS COSTS:</b>					<b>\$251,000</b>		<b>\$202,000</b>		<b>\$238,000</b>		<b>\$56,000</b>		<b>\$136,000</b>		<b>\$53,000</b>		<b>\$132,000</b>		<b>\$142,000</b>		<b>\$1,210,000</b>	
<b>ESTIMATED TOTAL CONSTRUCTION COSTS without Contingency:</b>					<b>\$1,001,800</b>		<b>\$808,600</b>		<b>\$952,900</b>		<b>\$216,650</b>		<b>\$543,700</b>		<b>\$203,350</b>		<b>\$522,750</b>		<b>\$562,100</b>		<b>\$4,811,850</b>	
1	Contingency or "risk"	25%		\$251,000		\$203,000		\$239,000		\$55,000		\$136,000		\$51,000		\$131,000		\$141,000		\$1,207,000		
<b>ESTIMATED TOTAL CONSTRUCTION COSTS PLUS CONTINGENCY:</b>					<b>\$1,252,800</b>		<b>\$1,011,600</b>		<b>\$1,191,900</b>		<b>\$271,650</b>		<b>\$679,700</b>		<b>\$254,350</b>		<b>\$653,750</b>		<b>\$703,100</b>		<b>\$6,018,850</b>	
<b>INFLATION COST (CURRENT YR. TO YR. OF OPENING)</b>																						
	Years	3%	5	\$200,000	5	\$170,000	5	\$190,000	5	\$50,000	5	\$110,000	5	\$50,000	5	\$110,000	5	\$120,000		\$1,000,000		
<b>TOTAL PROJECT COST (OPENING YEAR DOLLARS)</b>					<b>\$1,452,800</b>		<b>\$1,181,600</b>		<b>\$1,381,900</b>		<b>\$321,650</b>		<b>\$789,700</b>		<b>\$304,350</b>		<b>\$763,750</b>		<b>\$823,100</b>		<b>\$7,018,850</b>	

NOTE: (1) Includes aggregate base class 5 and PASB or OGAB, as appropriate.  
 (2) Includes aggregate base class 5 and earthwork.  
 (3) Does not include pavement edge drains, see separate item.  
 (4) Does not include excavation or backfill.  
 (5) Assumes Mn/DOT Standard Plan Sheet designs; does not include excavation or backfill.  
 (6) Does not include Moment Slab.  
 (7) Includes moment slab and concrete barrier.  
 (8) Tie back system required for exposed retained heights greater than 15 Feet. This cost is additional to the item for temporary steel sheet pile.

Utility Agreements based upon:  
 R/W Acquisitions based upon:





Bruce Vento Regional Trail - Phase 2 - Two Way Alternative on Hugo Road

Concept Cost Estimate (based upon 2020 bid price information)

Prepared By: SRF Consulting Group, Inc., June 14, 2022

			Lincoln Ave Start to Hwy 96		Bald Eagle Ave Hwy 96 to WBLHS		Bald Eagle Ave WBLHS to Bald Eagle Blvd		Bald Eagle Blvd Bald Eagle Ave to Park Ave		Bald Eagle Blvd & Taylor Ave Park Ave to Hugo Rd - TRAIL		Bald Eagle Blvd & Taylor Ave Park Ave to Hugo Rd - SIDE STREETS		Hugo Road Taylor Ave to Bald Eagle Lake Park		Bald Eagle Lake Park Parking Lot to Cty Road J		TOTAL			
ITEM DESCRIPTION	UNIT	UNIT PRICE	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT	EST. QUANTITY	EST. AMOUNT		
<b>PAVING AND GRADING COSTS</b>																						
GrP 1a	2106 Excavation - common & subgrade	cu. yd.	\$10.00	5,880	\$58,800					240	\$2,400	120	\$1,200	840	\$8,400					7,080	\$70,800	
GrP 2d	2106 Granular Subgrade (CV)	cu. yd.	\$15.00	3,960	\$59,400					240	\$3,600	120	\$1,800	600	\$9,000					4,920	\$73,800	
GrP 3g	Local & Frontage Road Pavement	(1) sq. yd.	\$20.00	4,900	\$98,000					200	\$4,000	100	\$2,000	650	\$13,000					5,850	\$117,000	
GrP 4a	Concrete Walk / Trail / Median	(2) sq. yd.	\$75.00	750	\$56,250	450	\$33,750									50	\$3,750			1,250	\$93,750	
GrP 4b	Bituminous Walk / Trail	(2) sq. yd.	\$40.00	2,000	\$80,000	5,800	\$232,000	3,350	\$134,000	1,350	\$54,000	4,900	\$196,000			2,800	\$112,000	4,800	\$192,000	25,000	\$1,000,000	
GrP 4c	ADA Pedestrian Curb Ramp	each	\$1800.00	10	\$18,000	14	\$25,200	8	\$14,400	2	\$3,600	3	\$5,400			2	\$3,600	6	\$10,800	45	\$81,000	
GrP 5	Concrete Curb and Gutter	lin. ft.	\$27.00	3,250	\$87,750	250	\$6,750	2,700	\$72,900	1,150	\$31,050	2,300	\$62,100	2,450	\$66,150	2,550	\$68,850			14,650	\$395,550	
GrP 8a	Removals - Pavement	sq. yd.	\$6.00	6,100	\$36,600	4,150	\$24,900	2,600	\$15,600	1,500	\$9,000	5,700	\$34,200	800	\$4,800			1,550	\$9,300	22,400	\$134,400	
<b>SUBTOTAL PAVING AND GRADING COSTS:</b>					<b>\$494,800</b>		<b>\$322,600</b>		<b>\$236,900</b>		<b>\$107,650</b>		<b>\$302,700</b>		<b>\$101,350</b>		<b>\$188,200</b>		<b>\$212,100</b>		<b>\$1,966,300</b>	
<b>DRAINAGE, UTILITIES AND EROSION CONTROL</b>																						
Dr 3	Water Quality Ponds & Other BMPs	l.s.		\$50,000		\$50,000		\$20,000										\$80,000			\$200,000	
Dr 5	Drainage - urban	l.s.		\$100,000		\$150,000		\$150,000		\$25,000		\$25,000		\$25,000		\$25,000		\$25,000			\$500,000	
Dr 6	Drainage - rural	(3) mile	\$100,000															0.70	\$70,000	1	\$70,000	
Dr 7	Turf Establishment & Erosion Control	10%		\$50,000		\$33,000		\$24,000		\$11,000		\$31,000		\$11,000		\$19,000		\$22,000			\$201,000	
Dr 8	Landscaping & Site Amenities	10%		\$50,000		\$33,000		\$24,000		\$11,000		\$31,000		\$11,000		\$19,000		\$22,000			\$201,000	
<b>SUBTOTAL DRAINAGE, UTILITIES AND EROSION CONTROL</b>					<b>\$250,000</b>		<b>\$266,000</b>		<b>\$218,000</b>		<b>\$47,000</b>		<b>\$87,000</b>		<b>\$47,000</b>		<b>\$63,000</b>		<b>\$194,000</b>		<b>\$1,172,000</b>	
<b>RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS</b>																						
RW 1	Box Culvert -	(4) lin. ft.	\$1,000													5	\$5,000			5	\$5,000	
RW 32	Steel Sheet Pile Wall & Safety Fencing	sq. ft.	\$125													4,800	\$600,000			4,800	\$600,000	
<b>SUBTOTAL RETAINING WALLS &amp; OTHER MINOR STRUCTURAL COSTS:</b>																<b>\$605,000</b>					<b>\$605,000</b>	
<b>SIGNAL AND LIGHTING COSTS</b>																						
SGL 1	Railroad Signal System Relocation/Upgrade on Bald Eagle Ave	each	\$250,000					1	\$250,000											1	\$250,000	
<b>SUBTOTAL SIGNAL AND LIGHTING COSTS:</b>									<b>\$250,000</b>													<b>\$250,000</b>
<b>SIGNING &amp; STRIPING COSTS</b>																						
SGN 1	Mainline Signing (C&D)	mile	\$17,500	0.3	\$5,250	0.9	\$15,750	0.5	\$8,750	0.3	\$5,250	0.9	\$15,750	0.1	\$1,750	0.5	\$8,750	0.7	\$12,250	4.2	\$73,500	
SGN 2	Mainline Striping	mile	\$2,500	0.3	\$750	0.9	\$2,250	0.5	\$1,250	0.3	\$750	0.9	\$2,250	0.1	\$250	0.5	\$1,250	0.7	\$1,750	4.2	\$10,500	
<b>SUBTOTAL SIGNING &amp; STRIPING COSTS:</b>					<b>\$6,000</b>		<b>\$18,000</b>		<b>\$10,000</b>		<b>\$6,000</b>		<b>\$18,000</b>		<b>\$2,000</b>		<b>\$10,000</b>		<b>\$14,000</b>		<b>\$84,000</b>	
<b>SUBTOTAL CONSTRUCTION COSTS:</b>					<b>\$750,800</b>		<b>\$606,600</b>		<b>\$714,900</b>		<b>\$160,650</b>		<b>\$407,700</b>		<b>\$150,350</b>		<b>\$866,200</b>		<b>\$420,100</b>		<b>\$4,077,300</b>	
<b>MISCELLANEOUS COSTS</b>																						
M 1	Mobilization	10%		\$76,000		\$61,000		\$72,000		\$17,000		\$41,000		\$16,000		\$87,000		\$43,000			\$413,000	
M 2	Non Quantified Minor Items	10%		\$76,000		\$61,000		\$72,000		\$17,000		\$41,000		\$16,000		\$87,000		\$43,000			\$413,000	
M 7	Utility Relocation (Bald Eagle Ave & Hugo Road)	10%		\$76,000		\$61,000		\$72,000		\$17,000		\$41,000		\$16,000		\$87,000		\$43,000			\$413,000	
M 8	Traffic Control	3%		\$23,000		\$19,000		\$22,000		\$5,000		\$13,000		\$5,000		\$26,000		\$13,000			\$126,000	
<b>SUBTOTAL MISCELLANEOUS COSTS:</b>					<b>\$251,000</b>		<b>\$202,000</b>		<b>\$238,000</b>		<b>\$56,000</b>		<b>\$136,000</b>		<b>\$53,000</b>		<b>\$287,000</b>		<b>\$142,000</b>		<b>\$1,365,000</b>	
<b>ESTIMATED TOTAL CONSTRUCTION COSTS without Contingency:</b>					<b>\$1,001,800</b>		<b>\$808,600</b>		<b>\$952,900</b>		<b>\$216,650</b>		<b>\$543,700</b>		<b>\$203,350</b>		<b>\$1,153,200</b>		<b>\$562,100</b>		<b>\$5,442,300</b>	
1	Contingency or "risk"	25%		\$251,000		\$203,000		\$239,000		\$55,000		\$136,000		\$51,000		\$289,000		\$141,000			\$1,365,000	
<b>ESTIMATED TOTAL CONSTRUCTION COSTS PLUS CONTINGENCY:</b>					<b>\$1,252,800</b>		<b>\$1,011,600</b>		<b>\$1,191,900</b>		<b>\$271,650</b>		<b>\$679,700</b>		<b>\$254,350</b>		<b>\$1,442,200</b>		<b>\$703,100</b>		<b>\$6,807,300</b>	
<b>INFLATION COST (CURRENT YR. TO YR. OF OPENING)</b>																						
	Years	3%	5	\$200,000	5	\$170,000	5	\$190,000	5	\$50,000	5	\$110,000	5	\$50,000	5	\$230,000	5	\$120,000			\$1,120,000	
<b>TOTAL PROJECT COST (OPENING YEAR DOLLARS)</b>					<b>\$1,452,800</b>		<b>\$1,181,600</b>		<b>\$1,381,900</b>		<b>\$321,650</b>		<b>\$789,700</b>		<b>\$304,350</b>		<b>\$1,672,200</b>		<b>\$823,100</b>		<b>\$7,927,300</b>	

NOTE: (1) Includes aggregate base class 5 and PASB or OGAB, as appropriate.  
 (2) Includes aggregate base class 5 and earthwork.  
 (3) Does not include pavement edge drains, see separate item.  
 (4) Does not include excavation or backfill.  
 (5) Assumes Mn/DOT Standard Plan Sheet designs; does not include excavation or backfill.  
 (6) Does not include Moment Slab.  
 (7) Includes moment slab and concrete barrier.  
 (8) Tie back system required for exposed retained heights greater than 15 Feet. This cost is additional to the item for temporary steel sheet pile.

Utility Agreements based upon:  
 R/W Acquisitions based upon:



**Appendix G- Public Engagement Plan**



# Public Engagement Plan

*Ramsey County  
Phase 2 Bruce Vento Preliminary Design*



Prepared by:



April 8, 202



# Table of Contents

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Introduction.....	<b>2</b>
Purpose of the public engagement plan .....	2
Community understanding .....	2
Engagement Approach.....	<b>4</b>
Engagement during the COVID-19 pandemic:.....	5
Stakeholder Identification and Engagement .....	5
Resident Engagement Strategies .....	<b>5</b>
Community Engagement Meetings: .....	6
Focus Groups: .....	7
Pop-up Meetings and Passive Engagement:.....	7
Communication Strategies.....	<b>7</b>
Ramsey County Social Media Policy:.....	8
Promotional Strategies and Resources: .....	8
Engagement Timeline .....	<b>9</b>



# Introduction

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## Purpose of the public engagement plan

This public engagement plan (PEP) articulates the engagement goals, strategies, key audiences, and schedule for three rounds of community outreach and engagement. The PEP will promote adaptable and dynamic engagement strategies that can be refined throughout the process.

Engaging residents and stakeholders will be critical to the success of this project. The project will look to build upon the county's robust public engagement efforts and to integrate the input from multiple agencies and community perspectives into the design. **Through respectful conversations with stakeholders and the public, we can better understand the needs of the community and respond with creative, constructible solutions that will help to ensure successes for the Bruce Vento Regional Trail.** Successful engagement will help to ensure that regional trail improvements are high quality community spaces for all people, all ages, and all abilities.

Public engagement is a dynamic process; as such, this plan is considered a living document and will be updated to reflect input received from project partners, stakeholders, and the public as the project progresses. This document will be developed with the Project Management Team's (PMT) input and serve as a guide for all phases of engagement.

## Community understanding

Bruce Vento was a long-time congressman from the 4th District who was a passionate environmental advocate for Minnesota and the nation. The Bruce Vento Regional Trail was established to celebrate his advocacy and share his passion with others in the community.

The Bruce Vento Regional Trail is envisioned as a 13.3-mile trail from east side of downtown Saint. Paul to the north County boundary in White Bear Township. This project would complete a major gap in Metropolitan Council Regional Bike Transportation Network and the National US Bike Route 41 (USBR 41) by furthering the design process for the final 3.5 miles of the corridor.

The preferred conceptual route between Whitaker Street and County Road J primarily utilizes Bald Eagle Avenue, Bald Eagle Boulevard, Taylor Avenue and Hugo Road; an alternative route would traverse the White Bear Lake High School property and use a section of Division Street and Park Avenue to connect to Hugo Road. **Ramsey County will work to**



**incorporate input from residents and stakeholders with a racial equity lens to develop this critical link of regional trail to connect pedestrian and bicycle facilities in the County between Whitaker Street and County Road J.** This is a challenging project with many traffic, design, utility, and stakeholder issues to work through.

Some of the key objectives of this project include:

- Preparing schematic and preliminary plans to strengthen the County's chances for future grant requests.
- To obtain meaningful input from stakeholders living near the project corridor through highly interactive and engaging public involvement activities.
- Create high quality community spaces that promote user experiences for all people, all ages, and all abilities.
- To provide a regional trail facility and site amenities that complement the areas natural resources and incorporate sustainable and resilient design.
- Provide trail connections to Rush Line BRT and downtown White Bear Lake.



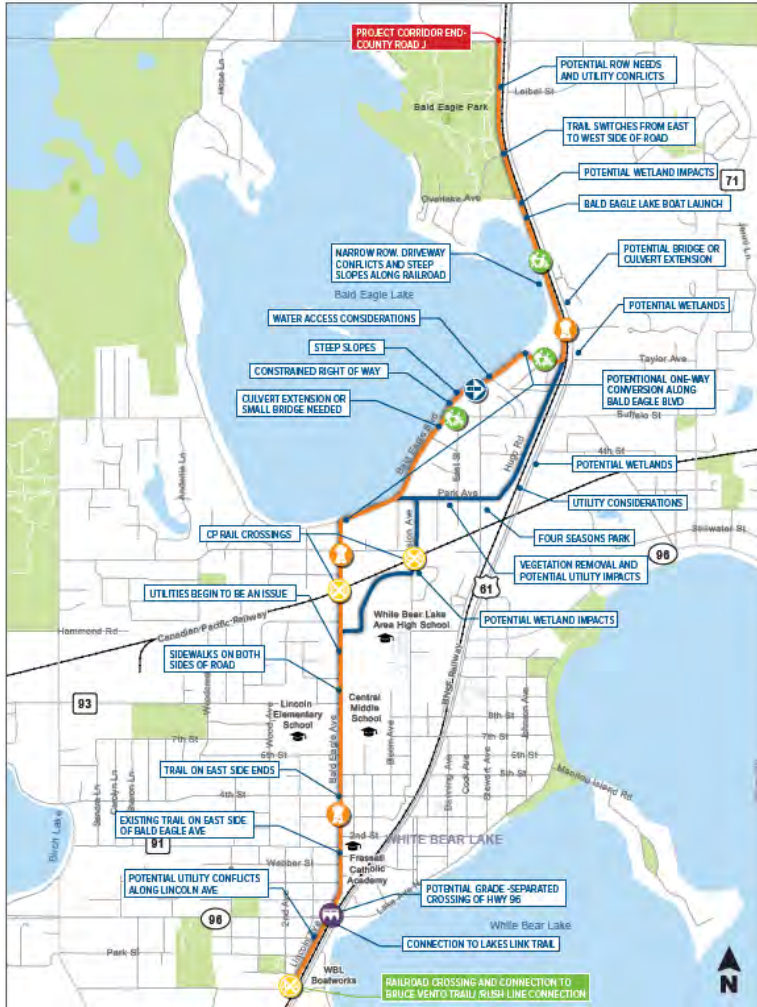


Figure 1: Project Context Map

## Engagement Approach

We understand the Bruce Vento Regional Trail is an important community asset, and we want to ensure new trail connections benefit residents and businesses, and neighborhoods within project corridor while also drawing new audiences to the regional trail system. Throughout the process, we are committed to advancing the best practices of the International Association for Public Participation, propose a visually appealing engagement approach designed to inform, engage, and involve a wide variety of community stakeholders.

SRF will prepare for and facilitate three rounds of outreach and engagement at key phases of the project. Engagement will be threaded throughout the project to build community awareness and participation as well gather key input needed to develop community supported trail alternatives. Each round of engagement will include a community meeting, online engagement opportunities, targeted outreach, and comprehensive project promotion.



- Round 1 engagement – Project awareness, challenges, and opportunities.
- Round 2 engagement – Seek input on potential trail alignment design concepts.
- Round 3 engagement – Seek input on a preferred trail alignment design concept.

Inclusive engagement strategies are critical to reaching the diverse community and business stakeholders in the project area. Ultimately, our engagement approaches will remain responsive and context-sensitive to the needs of Ramsey County residents, Bruce Vento trail users, and a broad set of area stakeholders.

## **Engagement during the COVID-19 pandemic:**

During the COVID-19 pandemic, we have learned that engagement must be adaptable and outreach efforts need to go the extra mile to ensure broad representation and accessible opportunities to participate. We believe engagement is most effective when it blends in-person interactions with online information sharing opportunities. As engagement progresses, the project will look to pair interactive online engagement platforms with options to conduct socially distant in-person engagement in various formats.

We will continue to monitor State and local health guidelines and will be prepared to refine engagement to best reach stakeholders in a safe, fun, and effective way. Across engagement activities, our public engagement toolbox is adaptable to ensure similar experiences in-person and online at each phase of outreach for the Bruce Vento regional trail improvements.

## **Stakeholder Identification and Engagement**

The following list includes stakeholders that have been identified as key agencies that will need to be engaged during the project. Anticipated meetings will include, but are not limited to:

- Ramsey County Parks and Recreation (2 meetings)
- Ramsey County Public Works (2 meetings)
- Municipal meetings (2 meetings)
- Watershed Districts (RCWD & VLAWMO) (2 meetings)
- BNSF/CP Railroads (1 meeting)
- Ramsey County Regional Rail (1 meeting)
- MnDOT (1 meeting)

In addition to monthly PMT meetings, SRF will set up, attend, facilitate, and summarize project meetings with key agencies and project stakeholders.

## **Resident Engagement Strategies**

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The project will utilize a suite of online engagement tools to inform, involve, and collaborate with the public. All engagement materials will be translated to ensure accessibility. Tools and strategies that will be used to engage the public include:

- Community Meetings (virtual, hybrid or in-person)
- Pop-Up Events (in-person or passive engagement)
- Focus Groups (virtual or in-person)
- Online engagement (website/surveys/social media)

All initial engagement activities will be conducted virtually due to COVID-19 restrictions. Our approach can be tailored to respond to current health guidelines and to ensure that a wide variety of community members are informed and able to participate.

## **Community Engagement Meetings:**

We recommend at a minimum three open house meetings (virtual or in-person). Community meetings will include informational displays, handouts, input opportunities (i.e., surveys or comment forms) and presentations to facilitate thoughtful conversations with the public. These events will be planned in coordination with the County and their partners to promote input and feedback opportunities. Promotional strategies include a project newsletter, press release, social media campaign, and website content for each round of engagement. Meeting summaries, including attendee lists and comments received (verbal and written), will be distributed electronically within one week.

SRF will prepare for and facilitate three community meetings at key phases of the project. Our engagement staff will prepare visually appealing project materials as well as traditional and digital promotional strategies. We have successfully facilitated a wide variety of community meetings since the start of the pandemic and our hybrid approaches are designed to ensure accessibility for all, including translation services, accessible web design standards, and targeted outreach.

- **Community Meeting 1** – Project Awareness, Challenges and Opportunities.
  - Focus on gathering early community input while also communicating project goals.
  - Important considerations include the potential trail alignment, safety considerations, and opportunities to ensure a high-quality community amenity for all.
- **Community Meeting 2** – Seek Input on Schematic Graphic Plan Concepts.
  - Develop up to three plans showing alternative trail options, identifying site impacts and major project elements while providing estimate costs for each alternative.
  - Create a matrix to illustrate the development of these plans.



- **Community Meeting 3** – Seek Input on Preferred Graphic Plan.
  - Share a preferred trail alignment and an associated project cost estimate. Preferred trail alignment is based on feedback received from technical discussions with project stakeholders and input received through the public engagement process.

## **Focus Groups:**

In addition to hosting virtual or hybrid community meetings, the project will also facilitate four focus group meeting with stakeholders and residents representing specific topic areas. We propose two sets of two focus group meetings with area businesses and trail advocates at key phases of the project. Specific focus group topics and engagement strategies will be determined with input from the PMT.

Focus group meetings are anticipated to be hosted virtually. Zoom has proven to be an effective and comprehensible online meeting platform used to host a variety of remote calls and video conferences. The platform includes features like an interactive “white board” and online polling to help facilitate engagement.

These meetings will help to inform the design of the project and seek to build upon existing stakeholder relationships to communicate project goals, objectives, and outcomes with key audiences. SRF staff will prepare for and facilitate all components of the focus group meetings.

## **Pop-up Meetings and Passive Engagement:**

The project will also look to host two socially distant outdoor pop-ups and/or prepare fun and visually appealing passive engagement materials along the Bruce Vento trail. Pop-ups and vibrant passive engagement materials have proven to be low cost and highly effective strategies for communicating with key audiences. We have had great success pairing pop-up engagement events with in-person and online project surveys to ensure meaningful, inclusive, and timely public input opportunities.

## **Communication Strategies**

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We understand promoting engagement opportunities is more important than ever. We recommend sharing regular project updates and promoting input opportunities via community newsletters, traditional media, website updates, and targeted social media. SRF will offer translations of key materials into Hmong and Spanish to better facilitate community understanding of the project and its goals. Communications will correspond with engagement events, with regular updates to the project website. SRF staff will develop project communications in collaboration with our in-house graphic designers.



## Ramsey County Social Media Policy:

Content and communication with the public will adhere to Ramsey’s County’s [social media policy](#), which is outlined below.

Ramsey County maintains social media pages to share information of public interest with our residents, businesses, and visitors. As part of our effort to be a transparent and inclusive organization that is accountable to its residents, we encourage you to engage with us. You have a voice in your government and your participation helps us be more efficient and effective at providing services and information.

The county’s social media pages are considered a moderated discussion and not a public forum. In the spirit of community, please note we reserve the right to hide or delete comment(s) that include:

- Profanity or hate speech.
- Personal attacks of any kind.
- Advertisements or solicitation.
- Deceptive or misleading content.
- Promotion of illegal activity.
- Comments that are clearly off topic in response to a particular post.

All engagement materials, including social media and promotional content, will follow Ramsey County brand guidelines: [link](#)

Throughout the project, we look forward to developing fun, inviting, and vibrant engagement materials that encourage public participation and advance a welcoming atmosphere for all current and future trail users.

## Promotional Strategies and Resources:

Ramsey County	City of White Bear Lake	White Bear Township
<a href="#">Facebook.</a>	<a href="#">Facebook</a>	<a href="#">Facebook</a>
<a href="#">Twitter.</a>	<a href="#">City Website</a>	<a href="#">Township Website</a>
<a href="#">LinkedIn.</a>	<a href="#">Twitter</a>	
<a href="#">Instagram.</a>		

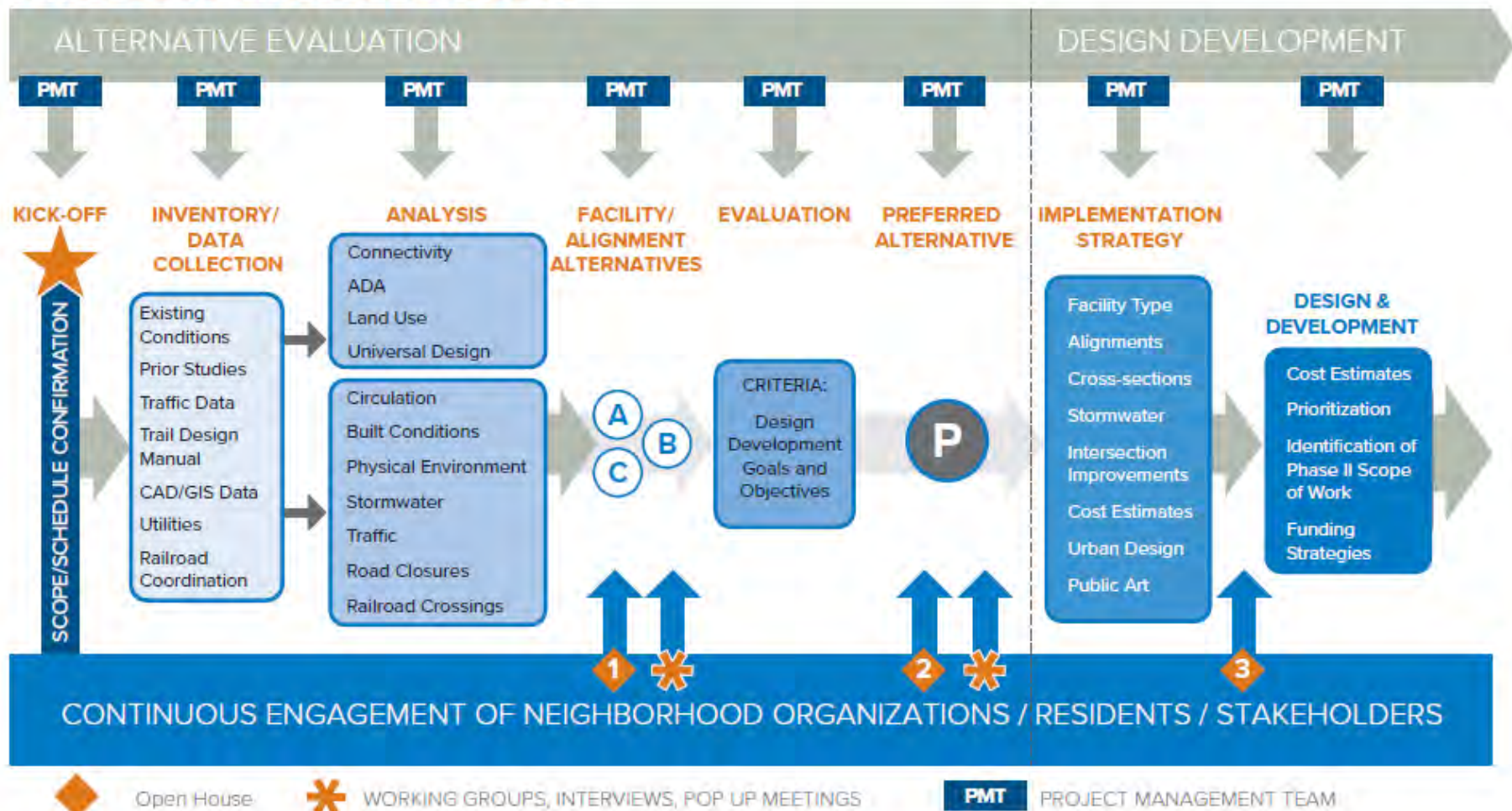


# Engagement Timeline

A detailed engagement timeline will be prepared for each round of engagement. The timeline will include a schedule for developing and reviewing materials, promotional strategies, web content, notification periods as well as public input opportunities.

\*Round 1 engagement anticipated to begin in June 2021.

## PROPOSED PROJECT PROCESS





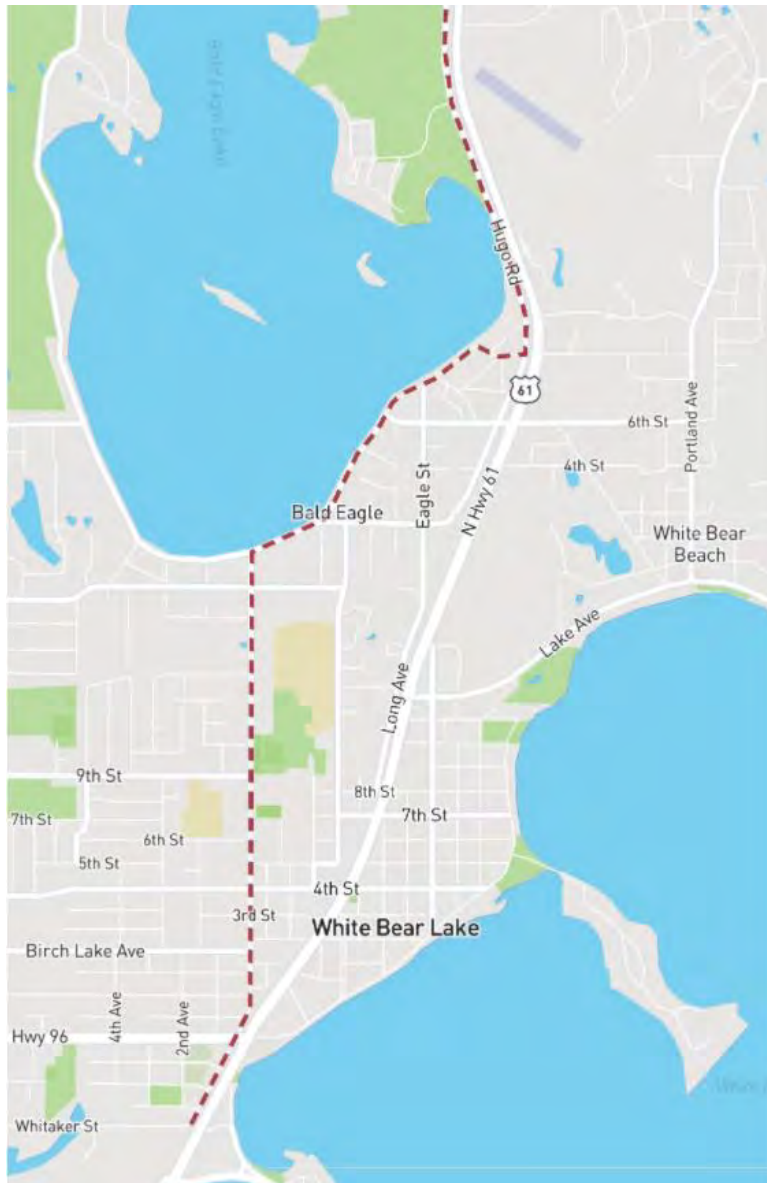
**Appendix H - Sample Survey #1**



**Bruce Vento Regional Trail Extension – Phase 2 Survey**  
October 2021

**We would like your input as we work to develop the preliminary design of an extension to the Bruce Vento Regional Trail. Please take this short, 12-question survey.**

Ramsey County is re-envisioning the Bruce Vento Regional Trail as a 13.3 mile trail from downtown Saint Paul to the county boundary in White Bear Township. This phase of the Bruce Vento Regional Trail extension will stretch the last 3.5 miles of the corridor between Whitaker Street and County Road J. The map below shows the preferred regional trail corridor for this extension that was selected as a result of the public engagement process from September 2018–2020.

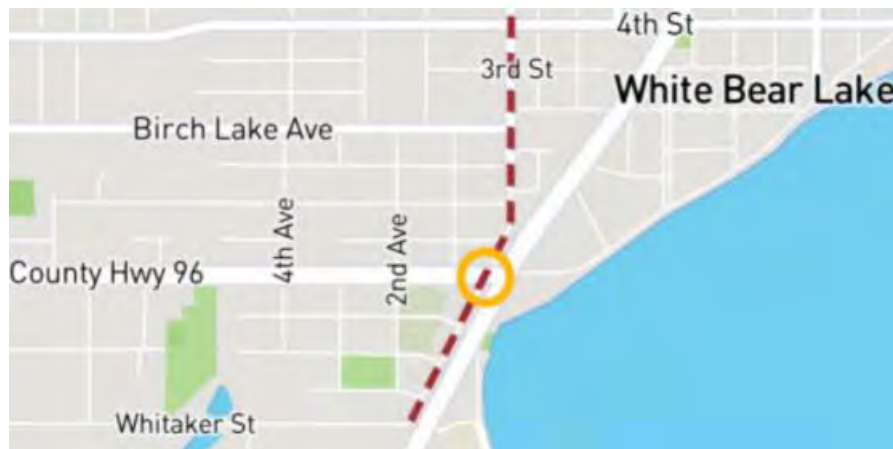




The current design intends to cross Highway 96 at the existing signalized intersection at Highway 61. A pedestrian bridge is being considered as a long-term safety improvement as part of a future phase.



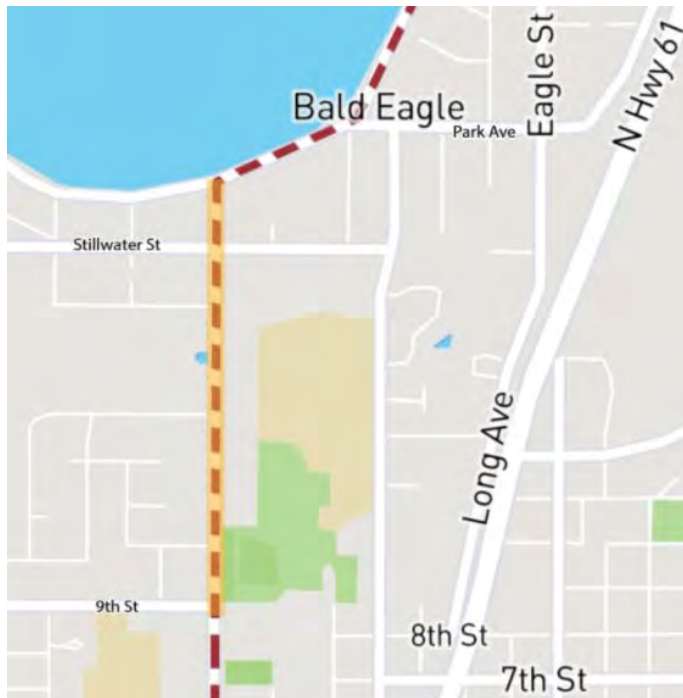
1. Would you be concerned about the narrowing of Lincoln Ave. and the potential loss of on-street parking (see map above, the segment of Lincoln Ave. is shown in yellow)?
  - Not concerned
  - No preference
  - Very concerned



2. In your opinion, if safe bicycle and pedestrian access can be accomplished with improvements at road-level, is a pedestrian bridge over Highway 96 still urgent? (See map above.)
  - Not very urgent
  - No preference
  - Very urgent
3. Please share any additional thoughts or comments to help shape the planning for this potential bicycle and pedestrian crossing improvement at Highway 96.



The project is evaluating the trail route along Bald Eagle Avenue and Bald Eagle Boulevard.



4. How important is it that existing road shoulders north of the high school remain in place once the project is constructed?
  - Prefer to maintain shoulders
  - No preference
  - Prefer to reduce property impacts

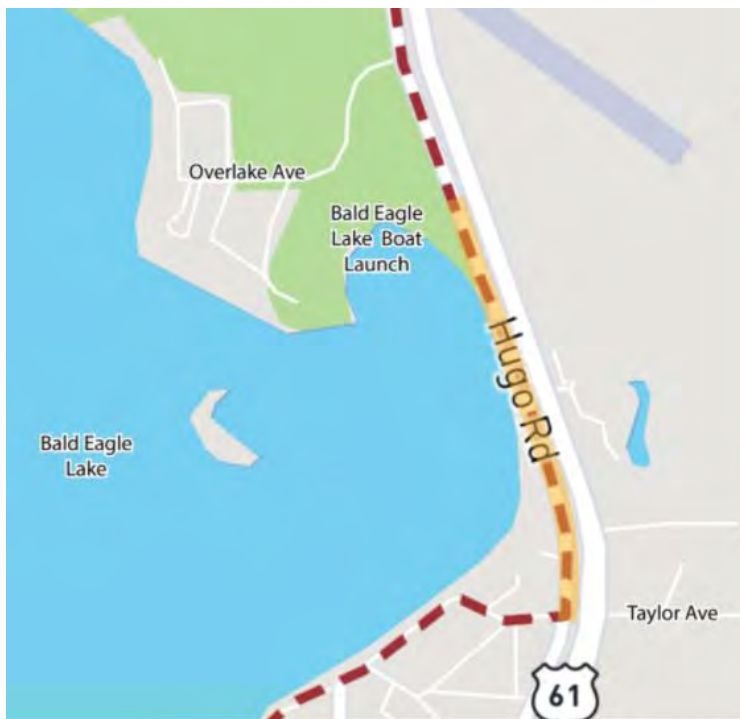
The project is evaluating several options for routing the Bruce Vento Regional Trail along Bald Eagle Boulevard.

5. Do you have a preference for which side of the road a trail would be located along Bald Eagle Blvd.?
  - Lake side
  - No preference
  - House side
6. How do you feel about converting a portion of Bald Eagle Blvd. to one-way? (Park Ave. to Taylor Ave. - see map above.) Please note: If converted, one-way traffic would travel in the northbound direction and impacts of the new trail would remain primarily within the boundaries of the existing roadway.
  - Strongly oppose
  - No preference
  - Strongly favor



7. Do you prefer that Bald Eagle Blvd. remain a two-way road with trail if that means impacts such as acquiring more road right of way to fit a trail will occur?
- Yes, I prefer a two-way road with additional impacts
  - No preference
  - No, I prefer a one-way segment with less impacts

Do you have any additional comments about routing a trail along Bald Eagle Blvd.?



8. In your opinion, is there a significant need for parking on the east side of Hugo Road, between Taylor Ave. and the Bald Eagle picnic area? (Segment shown in yellow on the map above.)
- Yes, there is a significant need for parking on east side of road
  - Unsure / No preference
  - No, there is not a significant need for parking on east side of road
9. An off-road trail will not fit in this segment (shown in yellow on map above). A concrete sidewalk could be placed on the east side for pedestrians. How do you feel about sharing the roadway with bikes along Hugo Road, between Taylor Ave. and the Bald Eagle picnic area?
- Okay with sharing roadway with bikes, pedestrians on sidewalk
  - Unsure / No preference
  - Prefer separated facility for bikes and pedestrians



10. What amenities would you like to see along the trail? *Check all that apply.*

- Benches
- Trash / recycling receptacles
- Bicycle repair stations
- Wayfinding signage
- Other: please specify \_\_\_\_\_

11. In addition to the Lake Avenue Trail and Highway 96 Regional Trail, are there other important trail connections you would like to see connected with the Phase 2 Bruce Vento trail extension?

12. Do you have safety concerns or comments about other bicycle and pedestrian roadway crossing locations along the proposed route of the Phase 2 Bruce Vento trail extension?

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### **About You**

*(Optional but appreciated.)*

ZIP code \_\_\_\_\_

Gender

- Female
- Male
- Non-binary
- Prefer not to say

Ethnicity

- White/Caucasian
- Black/African American
- Hispanic/Latinx
- Asian/Pacific Islander
- Native American
- Other \_\_\_\_\_

Please share your email if you would like to sign up for project updates.



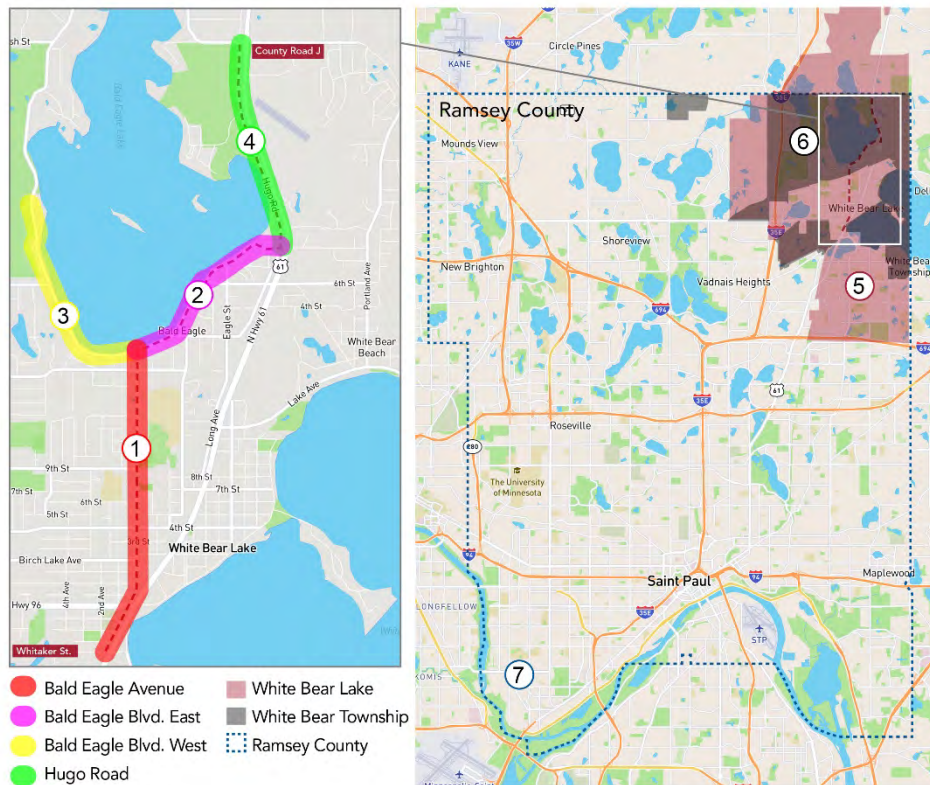
**Appendix I - Sample Survey #2**



Based on feedback from the community and discussions with stakeholders, Ramsey County has updated and finalized a proposed alignment for the Bruce Vento Phase 2 extension. The recommended alignment is available to review at [ramseycounty.us/VentoPhase2](http://ramseycounty.us/VentoPhase2).

1.) We'd like to know where you live in relation to the Bruce Vento trail. What option below best describes where you live? See the map graphic to help identify your location. (\*make this question required.)

- 1) Bald Eagle Ave (red)
- 2) Bald Eagle Blvd E (purple)
- 3) Bald Eagle Blvd W (yellow)
- 4) Hugo Rd. (green)
- 5) White Bear Lake, but outside of project area (pink)
- 6) White Bear Township, but outside of project area (grey)
- 7) Ramsey County, but outside of project area (dashed line shows county boundary)
- 8) Other:



The Phase 2 Extension of Bruce Vento Regional Trail will extend from the intersection of Whitaker St. and Lincoln Avenue up to County Road J. We have heard a range of perspectives about the































About You (Optional but appreciated.)

11.) ZIP code \_\_\_\_\_

12.) What is your gender?

- i. Female
- ii. Male
- iii. Non-binary
- iv. Prefer not to say

13.) What is your Ethnicity?

- i. White/Caucasian
- ii. Black/African American
- iii. Hispanic/Latinx
- iv. Asian/Pacific Islander
- v. Native American
- vi. Other \_\_\_\_\_



## **Appendix J - Compiled Public Comment Log**



Date	Event	Sentiment	Theme/Quest	Comment	Response date	Response text
Jul-21	Email Comments	Positive		<p>Hello Dan,</p> <p>It was nice meeting you at White Bear Lake Market fest last Thursday. As we talked the attached map shows results from a survey that several neighbors pulled together last June regarding one-way/safety measures on East Bald Eagle Ave.</p> <p>We have emails and addresses, including many on neighboring streets, from people who would like to see improved safety and high quality community experience become a reality.</p> <p>Please let me know how we can help with the Bruce Vento initiative. If you could send me an electronic copy of your flier, I will send to our list and encourage people to attend the virtual open house events on July 14th.</p>	7/14/2021	<p>I think for sending the comment below. I can give more reference in the later meeting tonight, but all this information has been listed on parks project page (see below for the link). All meeting dates, material, and graphics have been available for the public since fall 2018.</p> <p><a href="https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93-whitaker-street-county-road-j">https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93-whitaker-street-county-road-j</a></p> <p>Just so you are aware, the link above is the same as the shorten link on the meeting graphics, etc.</p>
Jul-21	Email Comments	Neutral		<p>5:00-6:00 Zoom Virtual Open Houses to contribute your opinions and opportunities for the route. <a href="https://www.ramseycounty.us/content/bruce-vento-regional-trail-phase-2-design-project-virtual-meeting-evening">https://www.ramseycounty.us/content/bruce-vento-regional-trail-phase-2-design-project-virtual-meeting-evening</a>.</p> <p>Make Trail Comments and Route Notes on Wikimapping <a href="https://wikimapping.com/Bruce-Vento-Regional-Trail.html">https://wikimapping.com/Bruce-Vento-Regional-Trail.html</a></p> <p>See Plan Background: <a href="https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension—whitaker-street-county-road-j">https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension—whitaker-street-county-road-j</a></p> <p>Route Plan: <a href="https://www.ramseycounty.us/sites/default/files/Parks%20and%20Recreation/Bruce%20Vento%20-%20Whitaker%20-%20County%20rd%20-%20Preferred%20Alignment.pdf">https://www.ramseycounty.us/sites/default/files/Parks%20and%20Recreation/Bruce%20Vento%20-%20Whitaker%20-%20County%20rd%20-%20Preferred%20Alignment.pdf</a></p> <p>Please continue to develop innovative strategic options that encourage direct access to the White Bear Lake Business District per the original Bruce Vento Trail Master Plan, preferred by bicyclists, visitors, walkers, handicapped and businesses accessing shopping and dining as their destination. The current proposed plan amendment bypasses the preferred destination of downtown, following Bald Eagle Avenue and Boulevard to Bald Eagle Lake and Park.</p> <p>Decision Rational:</p> <p>1) The Original Bruce Vento Trail Master plan and existing Route follows the Rail Line from St Paul to Hugo with no bypassing the route. This plan and route has been in place for over 50 years.</p> <p>2) The direct Bruce Trail Trail Master Plan paralleling highway 61, to and through White Bear Lake business</p>		
7/1/2021	Email Comments	Neutral		<p>I received a call from a resident who attended this morning's meeting (Bonita). Howard Blin is his name (his wife is Bonita).</p> <p>He was questioning the process, meetings, notifications prior to today's meeting. It may be helpful to provide the dates of previous meetings held regarding the trail route as well as notices which were provided regarding meetings.</p>		
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>Has funding come through for phase 1 for north of Buerkle rd?</p> <p>Planned start date for that extension?</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>What can be done to deal with the huge cracks in the asphalt south of CoRd D, especially south of Cope.</p>		Answered live
7/1/2021	Bruce Vento Lunch Q&A	Negative		<p>Don't understand why the trail would be on the house side along East Bald Eagle. Walkers and bikers want to connect with the lake (which is driving the ped/bike demand). It was mentioned that vehicles on the lake side would reduce impacts. This needs to be explained since the footprint is the same for either approach. Keeping vehicles on the house side matches what exists today and would not introduce new impacts to homeowners (would actually reduce impacts given traffic volumes would be cut in half).</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>There is a need for current road work on the east side of Bald Eagle Lake. How does that current need tie to the potential bike trail?</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>Start date of the BRT?</p>		Answered live
7/1/2021	Bruce Vento Lunch Q&A	Neutral		<p>Yes, the construction?</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Positive		<p>This doesn't need to be addressed live, but another benefit with the traffic staying on the house side is avoiding vehicles to/from sidestreets and driveways on East Bald Eagle from having to cross-over the trail. Want to avoid cars crossing over the trail.</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>What is the cost?</p>		Answered live
7/1/2021	Bruce Vento Lunch Q&A	Neutral		<p>Were there questions raised this AM not yet asked this session?</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>(or submitted to you independently of these open houses?)</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>Where is the funding coming from?</p>		Answered live
7/1/2021	Bruce Vento Lunch Q&A	Neutral		<p>Will this recording be placed on the website or on the internet? And how will one find it?</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>What is the time line?</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>Will the project be done by Met Council or by Ramsey Co?</p>		Answered live
7/1/2021	Bruce Vento Lunch Q&A	Neutral		<p>How many trees will be displaced?</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>Is there a mandate to replace all trees removed with similar trees?</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>How many bridges on the total project will be constructed?</p>		Answered live
7/1/2021	Bruce Vento Lunch Q&A	Neutral		<p>Does this go in front of the White Bear Council?</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>Both and when will that occur?</p>		Answered live
Jul-21	Bruce Vento Lunch Q&A	Neutral		<p>Will Ramsey Co. be hiring a consulting firm to proceed with this project? How will that work?</p>		Answered live
7/1/2021	Bruce Vento Lunch Q&A	Neutral		<p>What is the position of the Bicycle alliance on the design?</p>		Answered live
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>Has funding come through for phase 1 for north of Buerkle rd?</p> <p>Planned start date for that extension?</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>What can be done to deal with the huge cracks in the asphalt south of CoRd D, especially south of Cope.</p>		
7/1/2021	Bruce Vento Evening Q&A	Negative		<p>Don't understand why the trail would be on the house side along East Bald Eagle. Walkers and bikers want to connect with the lake (which is driving the ped/bike demand). It was mentioned that vehicles on the lake side would reduce impacts. This needs to be explained since the footprint is the same for either approach. Keeping vehicles on the house side matches what exists today and would not introduce new impacts to homeowners (would actually reduce impacts given traffic volumes would be cut in half).</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>There is a need for current road work on the east side of Bald Eagle Lake. How does that current need tie to the potential bike trail?</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>Start date of the BRT?</p>		
7/1/2021	Bruce Vento Evening Q&A	Neutral		<p>Yes, the construction?</p>		
Jul-21	Bruce Vento Evening Q&A	Positive		<p>This doesn't need to be addressed live, but another benefit with the traffic staying on the house side is avoiding vehicles to/from sidestreets and driveways on East Bald Eagle from having to cross-over the trail. Want to avoid cars crossing over the trail.</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>What is the cost?</p>		
7/1/2021	Bruce Vento Evening Q&A	Neutral		<p>Were there questions raised this AM not yet asked this session?</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>(or submitted to you independently of these open houses?)</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>Where is the funding coming from?</p>		
7/1/2021	Bruce Vento Evening Q&A	Neutral		<p>Will this recording be placed on the website or on the internet? And how will one find it?</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>What is the time line?</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>Will the project be done by Met Council or by Ramsey Co?</p>		
7/1/2021	Bruce Vento Evening Q&A	Neutral		<p>How many trees will be displaced?</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>Is there a mandate to replace all trees removed with similar trees?</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>How many bridges on the total project will be constructed?</p>		
7/1/2021	Bruce Vento Evening Q&A	Neutral		<p>Does this go in front of the White Bear Council?</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>Both and when will that occur?</p>		
Jul-21	Bruce Vento Evening Q&A	Neutral		<p>Will Ramsey Co. be hiring a consulting firm to proceed with this project? How will that work?</p>		
7/1/2021	Bruce Vento Evening Q&A	Neutral		<p>What is the position of the Bicycle alliance on the design?</p>		
10/16/2021	Email Comments			<p>I support the preferred route for the new portion of the vento trail around bald eagle lake to county rd j. thanks.</p>		Thank you for your feedback.
10/18/2021	Email Comments			<p>Greetings Mr. Yanke,</p> <p>My name is Jesse Bye and I live at 5226 Bald Eagle Blvd E, White Bear Township, MN. I received an informational postcard in the mail last week about the Bruce Vento trail phase 2 plans. I looked into the project a little online and have a question. What will the design potentially be for the stretch of Bald Eagle Blvd. E. between Bald Eagle Avenue and Park Ave? I ask because that is where our house is located.</p>		<p>Hi Jesse,</p> <p>Thank you for inquiring about the Phase 2 Bruce Vento project. The design for the roadway between Bald Eagle Avenue and Park Avenue is still being proposed as a two-way road with an off-road trail on the lake side. We are proposing the narrow the section of Bald Eagle Blvd between Park and Taylor to a one-way, like the Lake Avenue Trail. This will accommodate the road and trail (lake side) within the existing edges of the roadway (pavement area). We are working on getting all meeting materials (graphics, information, and online survey) uploaded to the project page in the next day or two so everything is also available online for review.</p>
				<p>Thanks,</p>		Thank you,



10/21/2021	Email Comments			I am out of town but want to voice my opinion of the Bruce vento expansion. I live on the south end of bald eagle lake I grew up on White Bear Lake and dealt with the one-way traffic which was great for a while but then it just turned into a constant public nuisance people walking by your house all the time sitting in your yard throwing garbage people stopping by your house constantly to talk to you it was not private at all the bike path is so overcrowded on Lake Ave., Baldeagle is a smaller lake by taking a lot of our land away by the lake it will take away the simple beauty. Will property taxes be absorbed by the public since there is a possibility we lose property? With all the building in Hugo, the traffic will have to remain on highway 61, which is already impossible to enter at times. Before living on white bear lake I lived on a side road off bald eagle lake. It has remained a quiet up north feel community and I wouldn't change it.	Hi Lori, Thank you for inquiring about the Phase 2 Bruce Vento Project and your comment below. Just so you are aware, all information that will be presented at the open house will also be available on the project website today prior to the meeting. There should also be an online survey available today as well. See below for the web link.  <a href="https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93whitaker-street-county-road-j">https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93whitaker-street-county-road-j</a>
10/21/2021	Public Open House - 2			Please feel free to call me or stop by to see what I am talking about. Scott Yonko, I am completely against the bike trail. I believe I am the person who would be most impacted by this project. I live on Taylor Ave as you come around the curve off of Bald Eagle Blvd. This is where the road becomes a 2-way road. I bought my house 5 years ago and fell in love with it due to privacy, beautiful trees and the quiet beauty of the lake. If my yard is impacted by the bike trail I can literally hand the bikers bottled water as they go by, out of my window. I worked very hard for my house. I am a single woman and feel very safe in my neighborhood, with an increase of bikers going by who can literally look into my living room that changes everything for me. We are a close neighborhood, people know and respect each other. I don't believe this right place for a bike path I don't know anyone in our neighborhood who is for this. I don't think people living by the lake would enjoy having their privacy impacted by these bikers either.	
10/21/2021	Public Open House - 2			I love it! Enthusiastic support. I have been in the neighborhood ten years and have often wished that the lake road would be turned into a one-way plus trail, like Lake Ave. on White Bear Lake. We often walk along the lake and there are places where you cannot get off the road when cars come. Good luck!	
10/21/2021	Public Open House - 2			No way - one way. Put the trail on a non-residential area like across Hwy 61 on the west side. No to Bald Eagle Lake with mature trees and houses too close to the street	
10/21/2021	Public Open House - 2			This proposal is not a good fit for our residential neighborhood. A single family quiet neighborhood is not designed to have a regional trail! Any trail should go on Long Ave. to Hugo Rd. Long Ave. has wide ROW and parallel to railroad tracks. Where would parking occur on side streets for people who want to walk segments of the trail? Regional Trails fit better on abandoned RR lines. Maybe it's too early to try and ram a trail into our community. Any one-way restricts access to many houses. Wait until you have an acceptable route!	
10/21/2021	Public Open House - 2			For those of us that live in White Bear Township this project with extension of the BRT is a NO. We don't want this trail in the township for many reasons. To list a few, negatively affects the Bald Eagle Lake with water quality changes, new impervious surface, water rate changes all that do not meet standards already. There is already flooding along the lake roads in the spring with runoff. Negatively impacts property, encroaching, taking people's land, removal of trees. Thousands of bikers into a small neighborhood. No alternative path. It should be declined in White Bear Township.	
10/21/2021	Public Open House - 2			In favor of one-way trail configuration between Park and Taylor. This will be safer for bikers and pedestrians, especially in winter. Provide a defined path for walkers and bikers (many individuals and bike groups pass in concern numbers along this stretch of road.) This will provide safer walking for peds, especially in winter. More year round foot/bike traffic. Bikes on pathway, not road.	
10/21/2021	Public Open House - 2			I would recommend making all of Taylor Ave a one-way. Current design has 2-way from Hugo Road to Shadyside Lane. One-way would be better. Thanks!	
10/21/2021	Public Open House - 2			I ride this route frequently- a couple times a week in the summer. Great choice for the Bruce Vento alignment. Please either widen to include bikes along Hugo Rd or maintain Hugo Rd as well as trail (surprised I haven't knocked a tooth out yet on that road!) Please also provide a welcoming and obvious connection to the high school campus. Thank you for all your hard work. GET 'ER DONE! ;)	
10/21/2021	Public Open House - 2			In recent years, a few options have been considered to extend the Bruce Vento Trail. This is the best option! Trails improve people's lives, physically and emotionally. It's time to do this.	
10/21/2021	Public Open House - 2			1. I favor the route along Bald Eagle Lake nearby residents have easy and convenient to south bound roads. 2. Even if you do nothing- RE PAVE HUGO ROAD!!!! Hugo road has to be the worst road in Ramsey County! So bad you can't ride a bike on it for fear of your teeth slamming into each other and shattering. 3. this trail w/the trail along the lake will be a boon to the township much as the lake ave path is the city of WBL is 4. Work with the state to add a crosswalk on the north side of county 11/20th and Hwy 61. For bikers is the current crosswalk and walk button- you end up riding against traffic on the west side of 61. Dangerous! Add a crosswalk button in the north side of Co J at HWY 61! Thanks!	
10/21/2021	Public Open House - 2			Thank you for the opportunity to review this information per our discussion with staff, we are very concerned with the loss of large trees around the lakeside which will negatively impact the eagles in the area. There are large Oak trees that the eagles nest from. Adding with the cotton wood & large trees also concern over the loss of all the lakeside parking. There are so many utilities - the old path would not be nearly as expensive to relocate	
10/21/2021	Public Open House - 2			My wife and I are in favor of this design. We have kids and are very athletic and would love to share the views and the road on BALD EAGLE. My only request would be to have the road on house side running north + trail on lakeside. So its not cross intersections & safe to travel. Also would be appered to widening road more than a few feet.	
10/21/2021	Public Open House - 2			NO,NO,NO, that should cover it!!!!	
10/21/2021	Public Open House - 2			Love the walking and bike trail on the R.R. tracks. No one way Streets!	
10/21/2021	Public Open House - 2			Plan as presented is far too tight on Bald Eagle Blvd. E. If you take as much shoreline as you are showing you will take out all of the trees. Comment from one of the consultants was it was right of way. Regardless, the impact is too great for the gain. A bike trail can go along route the does not have so much impact. The first portion of bald eagle blvd e will be two-way with the changes further up it is only going to increase traffic in this stretch. Cars already drive too fast. why can't trail go other side of high school where land already exists?	
10/21/2021	Public Open House - 2			1. We are accepting of the oneway from park to Taylor 2. We prefer that the bike trail & street not take any of our Blvd (requiring removal of granite steps) * We request you seriously consider the alternate routes Long Ave/Hugo Road.	
10/21/2021	Public Open House - 2			TABLE PROJECT----	
10/21/2021	Public Open House - 2			Please consider making Hugo (1 to Taylor) road a one way for cars so that you can have a separate bike path & walking path- could be shared.	
10/21/2021	Public Open House - 2			I think you need to hold a public forum to answer all the questions people have about what you are proposing.	
10/21/2021	Public Open House - 2			1.) This needs to be brought in front of the people who live on these streets, and hold a public forum where people can get questions answered. 2.) Please walk these streets and observe traffic and land in the area--see what is possible and what is note. A lot of this isn't possible. 3.) If all studies and funding are approved, consider a oneway on Hugo Road going north with trail closest to the homes, or better yet a culdesac at the park--this would also decrease drug/illegal activity in the park if they only have one way out.	
10/21/2021	Public Open House - 2			Who is funding this? Have these plans been approved by the Rice Creek Watershed for Impermeable Surfaces? Evaluated Ramsey County Road traffic on Hugo Road? Space allowances for a trail on Hugo Road? What groundwork has been done for this? This needs to be tabled until all of these things are figured out, along with the rush line and drainage and space for traffic lanes & a trail. No further discussion until these problems are solved.	
10/21/2021	Public Open House - 2			I think this is a great idea for walkers, bikers, and community. Personally the oneway will also help w/ traffic w/ these walkers.	
10/21/2021	Public Open House - 2			Like the one-way, like the project	
10/21/2021	Public Open House - 2			No need to do anything on Taylor Avenue. Literally makes no sense and will create congestion. Too many trees will be taken out. Utilize railroad space or Hugo Road.	
10/21/2021	Public Open House - 2			Please go back to the railroad and negotiate the land use for the trail. Fence the trail if necessary.	
10/21/2021	Public Open House - 2			This is a terrible project. I want nothing to do w/ Bruce Vento Trail. Prefer road turned back to township. No trail. No trees removed. No land taken. Contours not conducive to a trail. Prefer long Avenue, Eagle St, Hugo Road alignment.	
10/21/2021	Public Open House - 2			Good luck! Thank you [Anne?]. Need a bridge @ 96 and 61! The light at Whitebear? [SP?] will be problematic.	
10/21/2021	Public Open House - 2			Throughout this project, community engagement has been inadequate.	
10/21/2021	Public Open House - 2			It seems very complicated and impacts too many people's lives, land and ways around. Go back to the drawing board & find a less invasive plan. My home is my forever home, leave it alone.	
10/21/2021	Public Open House - 2			opposed. This is unnecessary. There is so much and so many other priorities.	
10/21/2021	Public Open House - 2			This trail should be on a natural corridor not in a residential area. The road is narrow and it would not be easy for anyone to try & shove a walking path through - property would be lost and all of us would be affected in many ways. NO WAY ONE WAY!! Leave us alone and follow a natural corridor--we need a public forum forum WTF come on.	



				The meeting format did a poor job of communicating all of the impacts this project will have on the residents of White Bear Township. In general the entire project has not done a good job of community engagement. The majority of people are just starting to understand the negative impacts this will have on the local community. People see through your check the box event. Removing 10 feet of green space along the south shore and replacing it with asphalt, the removal of trees is a terrible idea in so many ways we cannot believe it is even being considered. Environmental, erosion, runoff, tree removal, water quality, property values, etc...are not even discussed. Input is not being heard. This route comes within 30' of people's front doors on Bt Ave North of the RR tracks and moves power lines closer to their homes. One way only move traffic to neighboring side streets increasing traffic in areas that is not able to handle the increase. Loss of permeable land affects houses near the lake as everything from the H5 goes towards Bald Eagle. Increased traffic & congestion added to what's already increasing due to the H5. Portraying this as the preferred route is misleading. It isn't and never was. Portions of E Bald Eagle Blvd (Park to Taylor) can be made a one way without the BVT. Nowhere along the 13.3 miles are homeowners more negatively affected than in segment 4. Don't confuse safer streets with the need for the BVT.	
10/21/2021	Public Open House - 2			We strongly oppose using Bald Eagle and Taylor Avenues. (As do the comments added by local homeowners on the website). Using the alternate route makes the most sense, less impact on our neighborhood. Thanks for your consideration.	
11/10/2021	Community Survey 2 -October 2021	Q2		the trail at street level.	
11/10/2021	Community Survey 2 -October 2021	Q2		road level crossing must be safe	
11/10/2021	Community Survey 2 -October 2021	Q2		furthering punishing folks who must use 61.	
11/10/2021	Community Survey 2 -October 2021	Q2		accident, especially with increased pedestrian traffic. Parking should be a priority as it is limited already.	
11/10/2021	Community Survey 2 -October 2021	Q2		It's not at all clear how to provide safe bicycle and pedestrian access at road level here.	
11/10/2021	Community Survey 2 -October 2021	Q2		A bridge would definitely be safe but in my opinion it could be part of a later improvement.	
11/10/2021	Community Survey 2 -October 2021	Q2		Intersection of major trunk thoroughfares brings high rates of speed to intersection, and neighborhood knows dozens and dozens of intersections that create opportunity for interaction with automobile traffic, not just at the loss. This makes sense as a trail, wild life corridor and pollinator area.	
11/10/2021	Community Survey 2 -October 2021	Q2		abandon the plan to bring the path to bald eagle lake. Leave the township roads the way they are.	
11/10/2021	Community Survey 2 -October 2021	Q2		already gets backed up.	
11/10/2021	Community Survey 2 -October 2021	Q2		be heading straight into the blinding sun in early evening.	
11/10/2021	Community Survey 2 -October 2021	Q2		pedestrians.	
11/10/2021	Community Survey 2 -October 2021	Q2		That is an extremely busy intersection and drivers routinely "roll" through red lights.	
11/10/2021	Community Survey 2 -October 2021	Q2		pedestrian and bicycle access to downtown White Bear Lake and Linking with the Lake Avenue trail	
11/10/2021	Community Survey 2 -October 2021	Q2		Terrribly busy intersection already. Our only intersection that brings traffic coming from the west into downtown.	
11/10/2021	Community Survey 2 -October 2021	Q2		Too busy, too fast of an intersection with added occasional railway traffic.	
11/10/2021	Community Survey 2 -October 2021	Q2		pedestrian traffic should NOT be considered. The most safest designing should be considered first obviously	
11/10/2021	Community Survey 2 -October 2021	Q2		Need a bridge	
11/10/2021	Community Survey 2 -October 2021	Q2		would NOT be safe as drivers are already taxed here. A bridge is necessary.	
11/10/2021	Community Survey 2 -October 2021	Q2		96 is busy with speeders also. That is when they don't see pets and bikes.	
11/10/2021	Community Survey 2 -October 2021	Q2		Very busy intersection. A pedestrian AND bicycle bridge is critical!	
11/10/2021	Community Survey 2 -October 2021	Q2		Would like to see planning to keep bikes away from cars on such a busy intersection.	
11/10/2021	Community Survey 2 -October 2021	Q2		signal to cross hiway. But how would you merge bridge users safely with walkers on the path with all the plantings and in favor of some pedestrian improvements but concerned with scale of bridge in location. Tunnel better option?	
11/10/2021	Community Survey 2 -October 2021	Q2		that worse! Please consider better alternatives so that auto traffic will not be impacted.	
11/10/2021	Community Survey 2 -October 2021	Q2		Always busy with pedestrians. A bridge would keep flow of traffic safe and flowing.	
11/10/2021	Community Survey 2 -October 2021	Q2		It would be nice to keep bikers off hwy 61, it isn't safe and they should not be riding there. A path could help this!	
11/10/2021	Community Survey 2 -October 2021	Q2		hard time giving pedestrians the right of way at this intersection.	
11/10/2021	Community Survey 2 -October 2021	Q2		That's a very busy intersection, especially at rush hour. People will use this trail to commute as well as exercise after work, creating road and trail traffic at the same time.	
11/10/2021	Community Survey 2 -October 2021	Q2		Long term a pedestrian bridge is ideal, but completing the trail and adding a bridge later if user volume warrants it seems like a reasonable use of funds.	
11/10/2021	Community Survey 2 -October 2021	Q2		Long term, having a bridge is preferred, but completing the trail first is more important. Depends on how much use the trail gets.	
11/10/2021	Community Survey 2 -October 2021	Q2		We have had no trouble on bikes crossing Hwy. 61 at that location.	
11/10/2021	Community Survey 2 -October 2021	Q2		No stop light crossings are safe. Statistics should already prove they are inefficient and do not allow clear visibility of pedestrians, bicycles, or motorcycles due to the poles, lights, and multiple lanes of traffic in every direction. They are at best distraction filled places that encourage dangerous accidents that lead to more property damage and deaths.	
11/10/2021	Community Survey 2 -October 2021	Q2		Roundabouts with pedestrian and bicycle bridges are a much safer alternative that removes the direct crossing of traffic.	
11/10/2021	Community Survey 2 -October 2021	Q2		Extremely busy intersection and crossing with a pedestrian bridge would improve traffic flow as well as protecting walkers and cyclists.	
11/10/2021	Community Survey 2 -October 2021	Q2		Concern is getting across at Whitaker	
11/10/2021	Community Survey 2 -October 2021	Q2		Safety is the biggest concern. It is also important not to make the automobile drivers upset with bicyclists. A pedestrian bridge would be my first choice but safety is above that.	
11/10/2021	Community Survey 2 -October 2021	Q2		There's a lot of people turning right from 61 south to 96 west and many people tend to look left for traffic as they turn right and don't always look for pedestrian crossing.	
11/10/2021	Community Survey 2 -October 2021	Q2		The AD of TH 96 and TH 61 would be cause for concern and further analysis. Would the phasing of the signals provide time for users of the trail to cross TH 96? Would the NB TH 61 left turn lane be extended? Would this cause queuing for more than 1 cycle? With the RR there, would the stop bar for the EB TH 96 be behind the trail crossing?	
11/10/2021	Community Survey 2 -October 2021	Q2		I am all in favor of the trail here, but I believe that it would need to be grade separated due to traffic volume.	
11/10/2021	Community Survey 2 -October 2021	Q2		Will there be a connection here that crosses TH 61 and provides access to the trail around WBL?	
11/10/2021	Community Survey 2 -October 2021	Q2		what is the proposed cost of this section of trail?	
11/10/2021	Community Survey 2 -October 2021	Q2		Can't answer without more info. What are costs for the different options? What are benefits of each option? How much will these changes cost? We need a public form to present information and get responses from residents!	
11/10/2021	Community Survey 2 -October 2021	Q2		in favor of increased [?] bike pathways. This in very poor now in White Bear & vicinity[?]	
11/10/2021	Community Survey 2 -October 2021	Q2		life safety	
11/10/2021	Community Survey 2 -October 2021	Q2		[from first page] the previous design is much more environmentally friendly. We will lose a great deal of majestic trees. Utilities will be very costly.	
11/10/2021	Community Survey 2 -October 2021	Q2		I would like to see some type of pedestrian improvements if a bridge is not an option.	
11/10/2021	Community Survey 2 -October 2021	Q2		Bridge is a good idea	
11/10/2021	Community Survey 2 -October 2021	Q2		This is a very busy and dangerous intersection already. Traffic back-ups are common & disruptive. Road level crossing would make this worse.	
11/10/2021	Community Survey 2 -October 2021	Q2		vehicle traffic & backups at this intersection already a significant problem, both on Highway 61 & 96. Road level crossing would make much worse.	
11/10/2021	Community Survey 2 -October 2021	Q2		I'd like to know what the accident rate for cars and for pedestrians is at this intersection. This seems like a potentially dangerous intersection, even for cars. I'm not a resident of White Bear lake, and I would be using the trail recreationally. I could see residents using the trail on a day to day basis to get to school and shops. The safety of kids who might be using the trail on their own should be heavily considered here.	
11/10/2021	Community Survey 2 -October 2021	Q2		"safe" does not speak to uninterrupted cycling. A pedestrian bridge is important, especially over a well-traveled highway. A bridge could avoid having cyclists stop and dismount which is undesirable from a viewpoint of exercise benefits.	
11/10/2021	Community Survey 2 -October 2021	Q2		That intersection is such a mess I can't see drivers doing better with more variables introduced. Bicycles and peds should have safer options than road level crossing.	
11/10/2021	Community Survey 2 -October 2021	Q2		The bridge over Highway 96 would be an enormous safety improvement and should be accomplished as soon as practical. However, studying the safety of the initial crossing should inform the speed at which the crossing improvement is made.	
11/10/2021	Community Survey 2 -October 2021	Q2		This is a busy intersection. I don't see how any road level safety improvements can be made.	
11/10/2021	Community Survey 2 -October 2021	Q2		If looking at bike/walk mobility that includes the highest usage by the most individuals including elderly, children and wheelchairs, etc. having a very safe way to travel involving a busy intersection may prove beneficial to the community in terms of participant satisfaction with being able to move safely into town, the lake, etc. Safety also encourages use that leads to healthy outcomes in terms of quality of life, exercise(including children/teen) and safety for drivers as well (less confusion or obstruction by walkers/cyclists).	
11/10/2021	Community Survey 2 -October 2021	Q2		Families cross intersections. Trail is not just for adults.	
11/10/2021	Community Survey 2 -October 2021	Q2		Drivers don't pay attention. At grade is dangerous. Lives will be lost.	
11/10/2021	Community Survey 2 -October 2021	Q2		I'm ok with me, an adult, crossing 61 at street level, but I think children, and people with limited mobility should likely have a pedestrian overpass. If the engineers have a super, super safe way to cross at street level, great! But, as a trauma nurse, I get really squirmy/uncomfortable about people crossing high speed highways.	
11/10/2021	Community Survey 2 -October 2021	Q2		This is a very busy intersection and any way you can get pedestrian / bicycle traffic off of the automobile pavement is worth the cost. The extension of the Vento trail will increase traffic and it would be a mistake to dump them on Hwy 96.	
11/10/2021	Community Survey 2 -October 2021	Q2		A pedestrian bridge across 61 south of the 61/96 intersection is more urgent as there is now only a poorly marked pedestrian crossing. I cross there frequently and have had many a close call.	
11/10/2021	Community Survey 2 -October 2021	Q2		A safe crossing can be developed here utilizing the existing traffic signal.	
11/10/2021	Community Survey 2 -October 2021	Q2		There are always so many cars heading west on Highway 96 from both northbound and southbound on US Highway 61. Not only am I concerned about pedestrian and bicyclist safety, but also aware of the impact of changes to the vehicular traffic flow at that intersection. Vehicles will need to wait longer to allow bicyclists and pedestrians to cross causing traffic backups worse than are already there.	



11/10/2021	Community Survey 2 -October 2021	Q2	Average bike rider does a poor job of stopping for cross traffic when on a bike path, so bridge is needed.		
11/10/2021	Community Survey 2 -October 2021	Q2	I would prefer a crossing slightly farther west of hwy 61 that would be more perpendicular to the roads with a possible pedestrian crossing button if a bridge can't be installed.		
11/10/2021	Community Survey 2 -October 2021	Q2	The busy, complex intersection at 96/61 will be hard to make safe for pedestrians and cyclists at road-level.		
11/10/2021	Community Survey 2 -October 2021	Q7	Eastbound vehicles continuing east or turning south from Bald Eagle Blvd W onto Bald Eagle Ave will not anticipate southbound bike traffic if Bald Eagle Ave trail portion is on west side. Alternatively, if trail is on east side of Bald Eagle Ave, westbound vehicles on Bald Eagle Ave E Blvd continuing west or turning south onto Bald Eagle Ave will not anticipate southbound bike traffic.		
11/10/2021	Community Survey 2 -October 2021	Q7	Strongly opposed to Vento Trail along Bald Eagle Blvd		
11/10/2021	Community Survey 2 -October 2021	Q7	These are very leading questions. I much prefer the trail not be routed on Bald Eagle Blvd at all. The decision to route the trail on Bald Eagle Blvd. was the result of a flawed process in which most residents were unaware.		
11/10/2021	Community Survey 2 -October 2021	Q7	use park ave instead?		
11/10/2021	Community Survey 2 -October 2021	Q7	STOP! STOP!! STOP!!! This was a horrible, useless idea to begin with! What are you people doing? The busline?! The horrendous high-school too? Does no one care at all about the people that actually live in the area?! NONE of them signed on or chose to make their home in the middle of some traffic-muddled, metropolis. We want a quiet place to live just like everyone else, but that desire sends to come in 2nd to whatever plans you people seem to be making. PLEASE STOP!!!		
11/10/2021	Community Survey 2 -October 2021	Q7	Concerns about curbing that may impact water run off going into the lake Concerns about parking for bikers, side streets can't handle parking, also West Bald Eagle can't handle parking as well as the two small public areas on W. Bald Eagle at cross street West and the other at St. Anthony.		
11/10/2021	Community Survey 2 -October 2021	Q7	This segment of road is popular with pedestrians and cyclists despite how unsafe it is. It will remain popular if the trail is rerouted to avoid this stretch.		
11/10/2021	Community Survey 2 -October 2021	Q7	Residents currently utilize the available 'shoulders' for temporary parking of vehicles that are installing and removing docks and lifts during spring and fall due to the steep banks, which requires equipment to remove and install. Will temporary parking still be available along this segment?		
11/10/2021	Community Survey 2 -October 2021	Q7	Bald Eagle to Park is a vastly different road than Park to Taylor. I suggest the County keep Bald Eagle to Park a two-way, but take as little as property as possible from Bald Eagle to Park, and reduce speed limits all along the entirety of the trail. If the County is going to turn the modified road over to the Township anyway, lower speeds should not be an issue, and should reduce the amount of land necessary to safely build a trail and road in close proximity.		
11/10/2021	Community Survey 2 -October 2021	Q7	Get off the Boulevard. Do not cut trees, take land. This project is so wrong on so many levels. The wind on the road 8 months out of the year is cold and miserable. The amount of bike traffic BVT will generate in a neighborhood is a safety hazard for people backing out of driveways and crossing to docks. You had a better route off the boulevard, justify why you changed? You also ask loaded questions. Never the option of not interested in this project at any cost. Shame on you for the lack of transparency, arrogant attitude in taking peoples properties, and complete lack of consideration.		
11/10/2021	Community Survey 2 -October 2021	Q7	Definitely shouldn't make wider there. Also, shouldn't have two way in the earlier section you didn't ask about.		
11/10/2021	Community Survey 2 -October 2021	Q7	Bald Eagle Blvd E is NOT a place for a trail. It is hilly, has some blind curves and has no place to add roadway. Some homes are already close to the street and to think you can add a trail on the lake side between Park Ave and Taylor streets, means people planning this trail have not set foot in the area. There are drop off cliffs up by Taylor street that are already treacherous. The asphalt is caving down the hill. I currently/ regularly walk and bike in that area and it is treacherous, even if it were one-way. This is NOT the preferred route. Then you add in the "purple/road" line basing proposal and the expanded 4 grade highschool off of Bald Eagle Avenue which will increase car AND school bus traffic and you have created a nightmare. It feels like these 3 conditions aren't being discussed as part of the entire picture. I am all for walking and biking trails, but your "preferred" location is not smart. You also need to consider the impact on Bald Eagle Lake water quality and street run off. Tens of thousands of dollars have been spent on alum treatments to improve water clarity. Homeowners and the Rice Creek Watershed shared the expenses. Homeowners agreed to be taxed an additional amount for 10 years to pay for this. You have a community invested in maintaining lake quality. Additional hard surfaces will not enhance that. SAFL baffles and a nearby iron/sand filter are all in place for the benefit of Bald Eagle Lake and the entire Rice Creek Watershed. Hugo Road is no place for a trail either. Houses have very shallow lots between the lake and the street. Their only option for having guests is parking along Hugo Road, as their driveways are often quite short. If you widen the road towards the railroads tracks, trees/vegetation would have to be removed, leaving the homeowners with less of a buffer for Hwy 61 noise. I really don't believe the planners have walked on Bald Eagle Blvd E between Park and Taylor or this wouldn't be a consideration for a trail. And the high school car/bus congestion, the purple bus line and current conditions along Bald Eagle Blvd E and Hugo Road need to be addressed as an entire system. STOP this trail location.		
11/10/2021	Community Survey 2 -October 2021	Q7	If Bald Eagle Blvd becomes one way then Stillwater St. should also become one way the opposite direction.		
11/10/2021	Community Survey 2 -October 2021	Q7	I feel it should follow the rail road bed and not impact public roads		
11/10/2021	Community Survey 2 -October 2021	Q7	This trail should not be on Bald Eagle Blvd at all. It is a silly idea that will ruin our neighborhood.		
11/10/2021	Community Survey 2 -October 2021	Q7	Do the trail on Bald Eagle is idiotic. It should go on the west side of 61 and not impact home owners or impeded traffic.		
11/10/2021	Community Survey 2 -October 2021	Q7	I think it's a bad idea. No room unless you convert part of boulevard to one way, leaving many of us and our visitors to go miles out of our way to get to arts district, downtown WBL, etc. Cd cause big impact on adjacent neighborhoods as we all seek to find shortcuts. And also concerned about fire trucks and other emergency access.		
11/10/2021	Community Survey 2 -October 2021	Q7	I think it's a bad idea. No room unless you convert part of boulevard to one way, leaving many of us and our visitors to go miles out of our way to get to arts district, downtown WBL, etc. Cd cause big impact on adjacent neighborhoods as we all seek to find shortcuts. And also concerned about fire trucks and other emergency access.		
11/10/2021	Community Survey 2 -October 2021	Q7	This is an essential segment that realistically will only happen if Bald Eagle Blvd is converted to a one-way. There is a lot of history to this issue. A door to door survey of residents along Bald Eagle Blvd between Park and Taylor a couple years ago indicated 95% support for the one-way (with only two property owners not contacted). Follow the same strategy that worked for South Shore Blvd which includes funding and maintenance agreements between the County and Township. It can be done and would represent a significant community and safety improvement for all residents.		
11/10/2021	Community Survey 2 -October 2021	Q7	A one way trail is preferred by 95% of residents on East Bald Eagle Blvd. (most recent survey of home owners, 2019) the shoulders along this stretch are very steep on the lakeside. The only way you can make a safe bike/walk trail would be to do a one-way here.		
11/10/2021	Community Survey 2 -October 2021	Q7	The trail should NOT be routed along Bald Eagle Boulevard. This is like converting a two lane country lane to a 4 lane interstate. The residents of White Bear Township were not given sufficient opportunity for engagement on this change. The trail should go through White Bear Lake.		
11/10/2021	Community Survey 2 -October 2021	Q7	Go the other way around Bald Eagle lake, to the west.		
11/10/2021	Community Survey 2 -October 2021	Q11	Tamarack / Otter Lake trail		
11/10/2021	Community Survey 2 -October 2021	Q11	Should be routed on Lake Ave from the beginning		
11/10/2021	Community Survey 2 -October 2021	Q11	This question presupposes the preferred alignment which again was arrived at through a flawed process.		
11/10/2021	Community Survey 2 -October 2021	Q11	No traffic with public landing and use of lake are already high enough.		
11/10/2021	Community Survey 2 -October 2021	Q11	Hugo Road needs to be addressed, as stated the road is far too narrow to support a separate sidewalk. If E Bald Eagle road is being considered for a 1 way, then Hugo road deserves this as well. It is unsafe and unfair to take away parking from residents and to mark a road as a trail system when it has been presented to the White Bear Township safety committee in conjunction with Ramsey County Speed Studies that the 85% percentile of speed on Hugo Rd is 39 mph in a posted 30 mph zone.		
11/10/2021	Community Survey 2 -October 2021	Q11	Stop this project, it's time to re access it mission and negative impact to the area.		
11/10/2021	Community Survey 2 -October 2021	Q11	No - The trail needs to add parking otherwise there is additional Parkin on side streets that impacts emergency vehicles and neighborhoods		
11/10/2021	Community Survey 2 -October 2021	Q11	Please stop impacting home owners for the sake of this trail. The trail could go all along 61 from downtown White Bear to Hugo.		
11/10/2021	Community Survey 2 -October 2021	Q11	I want the trail to eventually connect to the cities of Hugo and Forest Lake.		
11/10/2021	Community Survey 2 -October 2021	Q11	It is complete folly to have relocated the trail extension along Bald Eagle Avenue and Boulevard. The trail should connect to downtown White Bear Lake and The Rush Line. That was the preference expressed in every so called engagement session.		
11/10/2021	Community Survey 2 -October 2021	Q11	There is no parking on Hugo Road, anybody paying attention?		
11/10/2021	Community Survey 2 -October 2021	Q11	It would be nice to see the sidewalk/trail on County Rd H2 connect Otter Lake Elementary to the Bruce Vento Trail (trail could be extended along the entire south side of Bald Eagle Lake)		
11/10/2021	Community Survey 2 -October 2021	Q11	Connection to WBL trail and beach park.		
11/10/2021	Community Survey 2 -October 2021	Q11	Very concerned as a local walking dogs daily on Lake Ave on how to keep from having speeding bukers not collide with us.		
11/10/2021	Community Survey 2 -October 2021	Q11	Gateway (I know it's a long way on the north end but it would be nice)		
11/10/2021	Community Survey 2 -October 2021	Q11	Will it connect to Hardwood Creek Trail in Hugo?		
11/10/2021	Community Survey 2 -October 2021	Q11	Will it connect with Hardwood Creek Trail?		
11/10/2021	Community Survey 2 -October 2021	Q11	I would like to see additional signage north of County Road J guiding bicyclists off of Hwy 61, which has its share of car/bicyclist accidents, over to Fenway Blvd. and north to reconnect with the Hardwood Creek Trail. This signage is missing, and is clearly not Ramsey County's purview, but it is needed.		
11/10/2021	Community Survey 2 -October 2021	Q11	A bridge for bikes and pedestrians to downtown White Bear at 4th street to reduce the pedestrian traffic and connect the two sides of 61. Another at 2nd Ave and 96 would complete both sides of White Bear for people who live on either side.		
11/10/2021	Community Survey 2 -October 2021	Q11	Out of Ramsey County, but pedestrian connection north on TH 61 to the trail on the west side of 61, north of 140th St N. There is RR on this side, but could cross over to the east side of TH 61 to connect the gap and give access to North Branch, Forest Lake, WBL, Maplewood to Saint Paul.		



11/10/2021	Community Survey 2 -October 2021	Q11	Connect to the Hardwood Creek Regional Trail, please!		
11/10/2021	Community Survey 2 -October 2021	Q11	Please DO NOT cut down trees along on Falcon Ave N. Road in Hugo that runs parallel to Hwy 61. The trees provide much needed home to wildlife! Also a buffer to car/ truck traffic!!!		
11/10/2021	Community Survey 2 -October 2021	Q11	Please DO NOT cut down trees along on Falcon Ave N. Road in Hugo that runs parallel to Hwy 61. The trees provide much needed home to wildlife! Also a buffer to car/ truck traffic!!!		
11/10/2021	Community Survey 2 -October 2021	Q11	Trail connection to Tamarack Nature Center. Trail extension along more of Bald Eagle Lake.		
11/10/2021	Community Survey 2 -October 2021	Q11	(question 9) is illogical due to prior question which asks if you want parking when roadway does not have enough room.		
11/10/2021	Community Survey 2 -October 2021	Q11	To all bike commuter trail systems and their feeders. Links please		
11/10/2021	Community Survey 2 -October 2021	Q11	Connecting to the Lake Avenue Trail/path in the city of WBL.		
11/10/2021	Community Survey 2 -October 2021	Q11	eventual reach to Hardwood Creek Trail in Hugo		
11/10/2021	Community Survey 2 -October 2021	Q11	[from question 9] Bikes and walkers could share a path, but needs to be wider!		
11/10/2021	Community Survey 2 -October 2021	Q11	Run the trail along the RR tracks on Long Avenue and Hugo Road. Stay out of the (?) neighborhood.		
11/10/2021	Community Survey 2 -October 2021	Q11	eventually connect with the gateway trail north		
11/10/2021	Community Survey 2 -October 2021	Q11	It seems like the original plan was to run the Bruce Vento trail along 61. It seems like all the controversy is stemming from this new plan to run it up along Bald Eagle Lake, which would be pretty, but is it convenient for people who live in White Bear Lake and would be using it often?		
11/10/2021	Community Survey 2 -October 2021	Q11	Hardwood		
11/10/2021	Community Survey 2 -October 2021	Q11	Build Lake Links east to Gateway and Brown's Creek.		
11/10/2021	Community Survey 2 -October 2021	Q11	Connect to hardwood creek trail.		
11/10/2021	Community Survey 2 -October 2021	Q11	This survey is worthless. Your route is planned and nothing anyone says will change your minds. Just like all the opposition to the choo choo train along university Ave when it was to be next to 94.		
11/10/2021	Community Survey 2 -October 2021	Q11	A trail to Tamarack Nature Center. Otter Lake dog park, Polar Lakes Park area would be awesome!!!		
11/10/2021	Community Survey 2 -October 2021	Q12	Bald Eagle Ave and 4th St intersection Bald Eagle Ave and Bald Eagle Ave E, W intersection Bald Eagle Ave and WBLAHS parking lot intersection		
11/10/2021	Community Survey 2 -October 2021	Q12	The proposed route is not a feasible solution for the Bruce Vento Trail. The alternate route by the high school connecting to Long Ave and Hugo Rd would be a much better solution.		
11/10/2021	Community Survey 2 -October 2021	Q12	Yes road in front of my house 5661 is too narrow to share with vehicles & bikes would like to see this trail on the other side of railroad tracks along same side down highway 61 a lot more room for a separate bike path. Make a call da sack last house on Hugo by park, making accessible only to school bus or emergency vehicles.		
11/10/2021	Community Survey 2 -October 2021	Q12	These roads were NOT meant to carry all this traffic, but people who live there shouldn't have to pay for it with their yards. Run these through parks and other public land, but please stay out of people's yards because "Plan A" didn't work out.		
11/10/2021	Community Survey 2 -October 2021	Q12	Many safety concerns with use of Hugo road. This should be one way going north to increase safety.		
11/10/2021	Community Survey 2 -October 2021	Q12	Concerns of public parking on narrow side streets and emergency vehicles.		
11/10/2021	Community Survey 2 -October 2021	Q12	None. I think there is a safety issue now without the trail, especially along the lake. I support this fully and I live on Beaver St and could be impacted by additional traffic but I would rather have a trail.		
11/10/2021	Community Survey 2 -October 2021	Q12	The intersection/crossing at Taylor Road to Hugo Road has very poor sight lines due to existing terrain. There is potential for car/bike accidents at this location.		
11/10/2021	Community Survey 2 -October 2021	Q12	Make Hugo Road a culdesac at the park. I think it a mistake to try to wedge all of what the County thinks is necessary into such a limited space. The plan, as is, will require extensive engineering and retaining walls. Dead end the road and you will eliminate 90% of the vehicle/pedestrian interaction without any additional expense and create a very appealing neighborhood/trail/park interaction.		
11/10/2021	Community Survey 2 -October 2021	Q12	No, only concern is Hugo Road, this section is VERY unsafe unless a one way is made of it to allow for a bike path as well on the east side. If this was done parking would not be lost for residents, safe passage of pedestrians, cyclists, and cars could happen simultaneously with a cross walk over to BE recreational park at the boat launch.		
11/10/2021	Community Survey 2 -October 2021	Q12	Bicyclists don't stop at stop signs. Traffic control on 61 at Buffalo and 8th.		
11/10/2021	Community Survey 2 -October 2021	Q12	Taking the trail along Bald Eagle after adding all of the High School traffic is asking for problems.		
11/10/2021	Community Survey 2 -October 2021	Q12	I feel it should follow the existing railroad area and not impact the already busy roads Safety is my biggest concern! Hugo Road has the "pinch point" area from 5641 Hugo to the BE Regional Park. That area cannot accommodate the trail without cause Multiple other issues. I recommend the safer issue of putting the on the east side of the railroad tracks.		
11/10/2021	Community Survey 2 -October 2021	Q12	I have huge safety concerns for the bikers who will be traveling with the drivers along Hugo Road. This road already sees people consistently exceeding the speed limit (89th percentile of speed is 39 in a 30 mph zone - Ramsey county study). There will be injuries and/or deaths if this road is not made a one-way like other narrow sections of the trail. Speed will become a bigger problem if the road is fixed (right now people speed and it is signed as a "rough road" - imagine what a smooth road will bring) I also have concerns that residents along Hugo road will not have parking on the east side of the road (that is the only place to park if not in a driveway). Snow removal will be an issue - residents generally move snow across Hugo onto the east side of the street - there is not room between homes to store snow. Who is responsible for keeping the sidewalk that is proposed clear? If trees are taken out, noise from why 61 will be dramatically increased. The boat traffic coming south-bound on Hugo from the public launch is also an issue, having the stretch of Hugo that is residential become a one-way would also solve that - create a barrier so that boats can only enter/exit the launch area from the north. If this road cannot become a one-way, I don't think Hugo is a good fit for it from a safety perspective.		
11/10/2021	Community Survey 2 -October 2021	Q12	Significant concerns about safety and property encroachments along the proposed route.		
11/10/2021	Community Survey 2 -October 2021	Q12	Very concerned about traffic along the lake. Steep embankments and very narrow		
11/10/2021	Community Survey 2 -October 2021	Q12	From HWY 61, HWY 96E to 244 for biking or pedestrian traffic safety is an area that needs to be highly considered for safer paths in either direction.		
11/10/2021	Community Survey 2 -October 2021	Q12	Get this project off the Bald Eagle Boulevard. There is bias in this survey, it's not a done deal and you should be more honest and transparent.		
11/10/2021	Community Survey 2 -October 2021	Q12	Need access to downtown		
11/10/2021	Community Survey 2 -October 2021	Q12	Yes already feel unsafe when fast bikers ride up too close and yell at us		
11/10/2021	Community Survey 2 -October 2021	Q12	Yes. Bald Eagle will be a mess for bicyclists and for vehicle travel. The road isn't that wide in the first place and the thought of taking away property from homeowners isn't the American Way. Will their property taxes go down because you are taking away what they have? Will the property taxes go up because now they have to maintain that portion of the trail? Will homeowners be on the hook for maintaining the area in front of their homes that is now on their property? What if they aren't able to clean off leaves and someone falls onto what's left of their property? Will they be responsible for any injury? As far as crossing 96... ha... if you make it at street level... how many accidents will occur because of bicyclists' ignoring the stop? It happens... I have been on trails where bicyclists just plow right ahead without bothering to look both ways expecting the driver to "see" them - same on Bald Eagle. This trail through WBL, though it could be exciting to have access, isn't going to be used as much as you think and people will get hurt.		
11/10/2021	Community Survey 2 -October 2021	Q12	Yes, any busy intersection on roads with speed limits of 30mph or greater.		
11/10/2021	Community Survey 2 -October 2021	Q12	Concerned when new high school opens. Thus will bring a lot of traffic into the area on quiet Bald Eagle lake that we pay a lot of taxes for.		
11/10/2021	Community Survey 2 -October 2021	Q12	How will it cross hwy 61?		
11/10/2021	Community Survey 2 -October 2021	Q12	How will it cross hwy 61?		
11/10/2021	Community Survey 2 -October 2021	Q12	I think Ramsey County is ready for a SHARROW along Old Hugo Road. I've used multiple facilities like this in the Netherlands, and if striped and signed appropriately, I think we could make this happen here! The concrete sidewalk might work well to delineate a safe space for pedestrians and bicyclists, but striping seems to suffice in Europe.		
11/10/2021	Community Survey 2 -October 2021	Q12	Bridges are a must. With the rail line already being an extra factor we cannot make the intersections more difficult to navigate for everyone. Stop lights are already an ineffective solution to safely move traffic with all the distractions and cross traffic.		
11/10/2021	Community Survey 2 -October 2021	Q12	Whitaker and 61 altho i suggest an overpass rather than another light along that section. Or have bike route go to the light at white bear avenue and 61 and turn north to Lincoln Avenue from there.		
11/10/2021	Community Survey 2 -October 2021	Q12	Nothing but safety concerns go away and find another route		
11/10/2021	Community Survey 2 -October 2021	Q12	A do-nothing alternative is not viable for this area. Many pedestrians are walking the east side to Bald Eagle Lake and Hugo Rd and the existing conditions are not safe.		
11/10/2021	Community Survey 2 -October 2021	Q12	YES, running the trail any where in vicinity of the new HS is an accident waiting to happen. Park next to South Campus when school lets out and watch the chaos, add to that 9 and 10 graders and then Central, Bald Eagle Ave (and all surrounding side streets) will be a nightmare		
11/10/2021	Community Survey 2 -October 2021	Q12	As much as possible, it'd be ideal to have a trail separated from the roadway by a curb.		
11/10/2021	Community Survey 2 -October 2021	Q12	Please DO NOT cut down trees along on Falcon Ave N. Road in Hugo that runs parallel to Hwy 61. The trees provide much needed homes to wildlife & create buffer to traffic noise!!!		
11/10/2021	Community Survey 2 -October 2021	Q12	Please DO NOT cut down trees along on Falcon Ave N. Road in Hugo that runs parallel to Hwy 61. The trees provide much needed homes to wildlife & create buffer to traffic noise!!!		
11/10/2021	Community Survey 2 -October 2021	Q12	If you keep squeezing cars into narrower lanes, it becomes dangerous with snow and other debris, e.g. leaves.		
11/10/2021	Community Survey 2 -October 2021	Q12	Crossing any major roadway (96)		
11/10/2021	Community Survey 2 -October 2021	Q12	Yes! Probably a state issue - but the crossing at Co. J / 120th and Hwy 61 needs improvement. Currently a crosswalk aid button is only on the south side of J. Add one on the north side of J.		
11/10/2021	Community Survey 2 -October 2021	Q12	I believe moving to a one way will be needed for safety		
11/10/2021	Community Survey 2 -October 2021	Q12	bikers need to stop at stop signs		
11/10/2021	Community Survey 2 -October 2021	Q12	No. Thanks so much for your work.		
11/10/2021	Community Survey 2 -October 2021	Q12	This should not be accomplished along Hugo Road. We do not want this trail near us and there is not enough space along the RR tracks. There is along not enough space along the lake. In both places you would be cutting into people's properties. Not acceptable.		
11/10/2021	Community Survey 2 -October 2021	Q12	It is crazy that the county is ripping up 6 miles of the exiting trail for the Rush line, which is ill-conceived. Stop that problem before any extension to the trail.		



				<p>Hi Scott,</p> <p>Thank you for setting up and presenting the proposed Bruce Vento Trail Phase 2 information last week. As I had mentioned to you at that event, I am very concerned about the boat traffic on Hugo Road. Drivers with boat trailers fail to follow the posted signs and illegally enter and exit the Bald Eagle Boat Launch parking area:</p> <ol style="list-style-type: none"> <li>1) When exiting this area they frequently turn right (south) even though there are two signs posted which symbolize: "No Right Turn".</li> <li>2) When arriving from the south, they frequently enter the boat launch "exit", because it is the only access when coming from that direction. The sign (from the south) on Highway 61 does not communicate boat traffic to use County Road J. The sign on Hugo Road at Buffalo, is positioned so the motorist can only read it after they have already turned (or they may have missed it as they are watching that their trailer makes the corner). At that point they have very few options to correct their course, back onto Highway 61 (northbound) to County Rd. J.</li> </ol> <p>(After multiple calls and emails by both my wife and I, neither MNDOT nor Ramsey County can coordinate to correct these signs.)</p> <p>Ramsey County is well-aware that the section of Hugo Road south of the Bald Eagle Regional Park is not wide enough to support boat trailer traffic coming from the south. This is why, many years ago they set up the process for boat trailered motorists to enter and exit from County Road J. The difficulty has been in getting drivers to abide by these rules when it can not be effectively communicated or monitored.</p> <p>If this process could be corrected, there would certainly be a more positive response about the Bruce Vento Trail on Hugo Road. If the process cannot be corrected, and the trail is installed, the results would likely be very dangerous for bikers and pedestrian attempting to get to the playground from south of the park. EFFECTIVELY REMOVE THE BOAT TRAFFIC (as the county has already mandated) and you will have A MUCH SAFER CORRIDOR for this wonderful trail.</p> <p>Feel free to contact me for further clarification,</p>		<p>Hi Daryl,</p> <p>Thank you for inquiring about the Phase 2 Bruce Vento Project. I appreciate your insight and comments for Hugo Road. Your comments have been recorded as part of the engagement process. We are looking into several design considerations to help correct and make this a safe corridor for pedestrians, bicycles, and vehicles. There will be more information to come as the design progresses.</p>
10/27/2021	Email Comments					
4/7/2022	Email Comments			<p>Would you like someone to contact you? No Your comments or questions: I also think the railroad tracks is the better area but a one way street where the landowners don't lose any or much of their yards also works. There is a little dog on the corner of Taylor and the lake that is aloud to bully my German Shepard so I can't wait for this project to put this dog in check if you do lake side.</p> <p>Best wishes. I'm disabled so I love the area and the trail extension is my dream come true cause I'm from st paul cabins trailer park and would love to safely ride down there again. My roommate Robert Bland will be there to speak for us both as well as our grandkids who will love the trail</p>		<p>Hi Laura,</p> <p>Thank you for the feedback regarding the Phase 2 Bruce Vento Trail Project. I am glad that you will have a representative available to attend the public open us on April 21st. Just so you are aware, all material that will be available at the open house will also be available on the project page for individuals that cannot attend the meeting. This material will consist of project exhibits/graphics, presentation slideshow, project awareness video, and an online survey. We are still finalizing some of this information, but it will be available on the 21st at the link below:</p> <p><a href="https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93whitaker-street-county-road-j">https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93whitaker-street-county-road-j</a></p> <p>Hi Laura,</p> <p>Thank you for the feedback regarding the Phase 2 Bruce Vento Trail Project. I am glad that you will have a representative available to attend the public open us on April 21st. Just so you are aware, all material that will be available at the open house will also be available on the project page for individuals that cannot attend the meeting. This material will consist of project exhibits/graphics, presentation slideshow, project awareness video, and an online survey. We are still finalizing some of this information, but it will be available on the 21st at the link below:</p> <p><a href="https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93whitaker-street-county-road-j">https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93whitaker-street-county-road-j</a></p>
4/14/2022	Email Comments			<p>Hugo road south of county J is in Horrible condition! TWICE this spring futile tossing of asphalt fill lasted less than a week before monstrous potholes returned.</p> <p>This is relevant because if it was in better shape, the need for a bike lane along the road would be greatly diminished! While I am strongly in favor of option 1 for the trail route, any of the proposed routes would be great IF THE TRAIL IS MAINTAINED!</p> <p>As poorly as the county has maintained Hugo road, I worry that a parallel bike trail would be as poorly maintained, blocked by illegally parked cars, and still be unsafe.</p> <p>I am an Avid biker, cycling to work at St John's Hospital from Bald Eagle Lake as often as I can during the summer.</p>		<p>Hi Charles,</p> <p>Thank you for reaching out regarding pavement conditions on Hugo Road and potential trail improvements. Our Public Works department is planning to implement a pavement project for Hugo Road this year. The intent of this project is to improve pavement conditions on Hugo Road since the Phase 2 Bruce Vento Trail project is at least 8-10 years out. I have copied a representative from our Public Works department for any additional information regarding this project.</p> <p>Regarding the proposed Bruce Vento Trail, we are still in early stages of design and are proposing two design scenarios for Hugo Road. One scenario is proposing a two-way road with an off-road trail, and the other scenario is proposing a roadway conversion from two-way to one-way with an off-road trail. The Park department is holding an open house on April 21, 2022, at the White Bear Lake City Hall from 5pm-7pm to solicit additional feedback for the Phase 2 Bruce Vento Design Project. There will be a variety of materials for the public to provide project feedback such as a project presentation, overview video, plan graphics, illustrative sections, and online survey. All materials presented at the meeting will also be on the project page for additional feedback from the public.</p>
4/19/2022	Email Comments			<p>Hello,</p> <p>I'm not able to attend Thursday's meeting but I'm in support of the one way road with trail on bald eagle Blvd e, that I live on. I work from home most days and the number of people walk on the road and it really is so unsafe. I was excited to see this possibility happen in Ramsey county, considering I moved out of hennipen county that had lots of lake trails to explore. Let me know if you have any follow up questions.</p>		<p>Hi Doug,</p> <p>Thank you for your response. Just so you are aware, all material that will be presented at the open house on the 21st will also be available on the project website. Also, I would encourage you to take the online survey on the project page for additional feedback. This material and survey should be available for viewing on the 21st. Below is the link to the project page.</p> <p><a href="https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93whitaker-street-county-road-j">https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93whitaker-street-county-road-j</a></p>
4/21/2022	Email Comments			<p>I could not make it to the April 21st Bruce Vento Trail extension Segment 4 feedback meeting so I am sending this email.</p> <p>As a White Bear Township resident I am voicing my opposition to the current proposed route down Bald Eagle Ave and along Bald Eagle Lake. I do NOT want the trail going through this residential area. With the new High School addition being built it changes everything, especially traffic flow. I think it is premature to continue with any plan for this segment until the high school is completed and traffic routines are set.</p> <p>Also, several studies in the past regarding changing the traffic flow on Bald Eagle Blvd. have proven longer for emergency and safety vehicle. This as a resident I do not approve of. The safety of the community is the highest priority as a taxpayer.</p> <p>I love the White Bear Lake area and want to see it flourish for all, but not at the sake of safety. I CANNOT support the Bruce Vento trail through Bald Eagle area at this time. I support stopping the project at downtown or rerouting along Long Ave as was originally proposed.</p>		<p>Hi Sheila,</p> <p>Thank you for providing a response to the project. Just so you are aware, all the information presented at the meeting is also available on the project website. I would encourage you to review this information and take the online survey. In addition, I referred individuals to the project page Wiki Map last night. The intent of this exercise is for individuals to place a comment on their property to designate support for the project and proposed design.</p> <p>Project Page Link: <a href="https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93whitaker-street-county-road-j">https://www.ramseycounty.us/residents/parks-recreation/planning-construction-restoration/bruce-vento-regional-trail-extension-%E2%80%93whitaker-street-county-road-j</a></p>
5/12/2022	Email Comments			<p>I live in vadnais hts area. I use the vento trail for biking and walking. I would like to voice my support for the extension plans through white bear lake.</p>		<p>Hi Glen,</p> <p>Thank you for providing feedback regarding the Bruce Vento Regional Trail Project. Your comments have been recorded as part of the project.</p>







5/18/2022	Community Survey 2 - May 2022	Lincoln Ave Oppose	any trail design should not involve taking parking or widening any existing road, it should not be simply justified by being the road right of way. Any and all existing green space should be maintained.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	would like a wider buffer between the vehicle space and the trail. Reduce the parking lane and/or the driving lane.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Connecting this path to the school is a great idea for the safety of our students and encouraging them to cycle to school more safely.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Looks good. I like that parked cars will continue to help protect walkers and bikers somewhat. What can be done to make vehicle drivers slow down on this residential street and how does the design create safe and convenient crossings for walkers and bikers? currently this is a wide, straight road that encourages high speed even though it's a lot of family and student pedestrian traffic.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	This is appropriate given the constraints of adding a trail where there was none. Bald Eagle Avenue is a good choice for a route.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Make that path wide enough for bikers or the bikers will be on the road.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	I live near here. I walk here. I ride a bike and would welcome a safer ride. I drive here too.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	This is the logical place for the trail.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Unclear how 96 crossing is done safely. Bridge? Safer crossing lights/paths? Right turns from 61 southbound render crossers very vulnerable.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	A safe way for students to walk, bike to school.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Looks tight (7ft Blvd) on one side for utilities. All OH power is on that side, and they usually have an offset requirement for other utilities.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	I oppose this regional trail, it should wait to follow the railroad corridor as previously intended. At the very least, I think you can make it work on Long Ave. (even though you are saying that road is not an option).		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	should be downtown next to hwy 61		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	This is a residential area. You are planning a path meant for a park or wilderness area THIS IS NOT APPROPRIATE FOR A NEIGHBORHOOD ! By your own acknowledgement over 200,000 people used Phase 1 of the plan in 2019, we do not need that number of people passing through our street not to mention the traffic. The new High School is already set to add 500 cars a day to the traffic on Bald Eagle Ave. This will only decrease the value of our property along with the added garbage, cigarette butts and dog poop left for the homeowners to clean up. We are a family neighborhood not a park !!!		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Due to Covid, the number of parents driving their kids to and from Central and North has vastly increased, making it hard for locals to use Division and Bald Eagle Ave. This will only get worse with the new high school. We don't need additional congestion.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Goes through a residential Neighborhood		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Bald Eagle Avenue will already be overcrowded with the combined senior High school at north campus.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	A Regional Trail does not belong in a residential neighborhood with small businesses, a Catholic Church, a mortuary and a private school. There is a busy intersection and narrow corridor space at BE Ave and 4th. High accident and traffic location. Current road narrows at 6th and again at 10th.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Seems like this would be very disruptive to a predominantly residential area, in addition to the greatly increased number of students and staff in the area of the WBL high school and Central Middle School.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	This will run with the sidewalk, so no loss to homeowners, there, but the amount of traffic with the new HS and how busy Bald Eagle Ave has become and will likely continue to grow is a major cause for concern when you bring that many people together through a neighborhood.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Not in a residential neighborhood		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Use common sense and we can't stand Biden either		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	We do not need additional sidewalks or trails on our street. We bought our new home here without being given knowledge of these terrible projects to accommodate my wheelchair and to get away from the crime in East Saint Paul.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	The trail should be directed to pass along the shores of White Bear Lake and into/adjacent to downtown White Bear. Having the trail pass through dense residential areas and next to a busy High School is not where I want to see the trail installed.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Additional traffic from the new high school will make this an unpleasant route. It is not a good fit for the neighborhood. This is not the original route. This proposed trail should wait until the RR ROW is available through abandonment.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Regional Trail does not belong in a residential area.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	There will be added school buses and school traffic moving along this street with the enlarged high school. More bicycles and pedestrians would be chaos. Not to mention dangerous.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	The traffic is already a night mare, this just continues the hell for the neighbors.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Please find a different path that is not entering a highly residential area, that cannot support this volume of people and usage.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Adding this is going to add to traffic issues with the move of the high school and present dangerous conditions with new drivers mixing with walkers and bikers		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	This regional trail is a bad fit in the neighborhood of the high school and family homes. It will create traffic hazards. Already too much happening along Bald Eagle Ave., There is NOT an accurate assessment of traffic changes. The high school traffic study is only an estimate from previous data, this is NOT sufficient to measure future changes. AVOID this area for bike trail. Poorly managed, unsuitable studies.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	road should not be widened any more. Oppose trail. Align the trail through north of the high school to Long Ave. It should NOT come any further down Bald Eagle Ave.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	The residents of this neighborhood have already been terribly burdened by the expansion of the high school. Asking them to bear the destruction caused by this proposal is especially unfair. This trail should go through White Bear Lake and access downtown.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	This regional rail is not a good fit for this residential neighborhood. Maximum use of the right-of-way will negatively impact homes along this route by relocating existing amenities and features.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Having attended the meeting 4/21 I am now strongly opposed to the trail altogether.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	It's obvious the only sensible option is along the rail corridor. Anything else is an ugly kludge. As such it's better to play the long game and edit the railway out.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Homeowners are losing property for this and the added pedestrian traffic by an already busy road because of the school poses a safety concerns for all in that area.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Obviously there is too much money available to you all to spend foolishly. There are numerous more important expenditures that could and should be made.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	We don't need this bus line. There will be no extra riders. We have a fairly new park and ride in Maplewood already.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	You won't be gaining new ridership		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Get it out of the neighborhoods.		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	Scrap this entire project		
5/18/2022	Community Survey 2 - May 2022	Hwy 96 to White Bear Lake High S	similarly to not widen road, and better yet, this is not the previous suggested route. What happened to the rail road corridor for this project. at this point is a deviation from years of understanding where a trail would go, this is new. Since when? and why??		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	narrow the lanes (slow the cars). There's no protection or buffer space between the cars and pedestrians/cyclists, so at the very least offer design cues that slow the cars down.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	This road is in terrible condition, anyway, I rode it yesterday and I was all over it dodging potholes, etc. It NEEDS re-surfacing. The park on Bald Eagle would be a great place to stop with kiddos!		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	Connecting this path to the school is a great idea for the safety of our students and encouraging them to cycle to school more safely.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	This is a necessary and reasonable continuation of the route going north.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	Or park avenue all the way to Hugo road		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	Make that path wide enough for bikers or the bikers will be on the road.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	I live near here. I walk here. I ride a bike and would welcome a safer ride. I drive here too.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	Perfect.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	This section does not have existing sidewalk area. The trail will run too close to houses. Traffic will be insane with the mega high school. Adding bikes to this area would be very dangerous.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	Are you fricken nuts? Why take our road...why not use the obvious rail road right of way along 61.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	New drivers (high school students) and narrow roadways are not a good combo. I'm all for adding the trail, but keep the road width for safety.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	With the increased volume of traffic for the new High School, all roads will have to be expanded, not narrowed.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	I oppose this regional trail, it should wait to follow the railroad corridor as previously intended. At the very least, I think you can make it work on Long Ave. (even though you are saying that road is not an option).		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	should be downtown next to hwy 61		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	Goes through a residential Neighborhood		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	Bald Eagle Avenue traffic will be overcrowded with new combined north campus high school		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	Residential homes, driveways, mailboxes, school entrances, RR tracks. Narrow corridor space with utilities a close proximity to homes. Residents do not want a Regional Trail in front of or this close to their homes		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	Seems like this would be very disruptive to a predominantly residential area, in addition to the greatly increased number of students and staff in the area of the WBL high school and Central Middle School. Traffic could get much worse with the narrowing of the road.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	This would bring a regional bike path right through my front yard which I am strongly opposed to. I would lose green space, trees, and on-street parking on my side of the street all of which are significant downgrades to my property.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	Why? Leave this as is. Quite lovely.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	You are one awful awful group of people		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Ba	We do not need additional sidewalks or trails on our street. We bought our new home here without being given knowledge of these terrible projects to accommodate my wheelchair and to get away from the crime in East Saint Paul. Taking away parking will only make it more congested for the parents getting their children from school. And eliminate parking for guests at my home.		



5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	The trail should be directed to pass along the shores of White Bear Lake and into/adjacent to downtown White Bear. This would bring in undue amounts of traffic into a very residential, quiet area. It seems the trail system along White Bear Lake is a much better option.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	Additional traffic from the new high school will make this an unpleasant route. It is not a good fit for the neighborhood. Parking is needed on the east side of the street. This proposed trail should wait until the RR ROW is available through abandonment.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	Regional Trail does not belong in a residential neighborhood.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	This is our community. I do not want a parade of bikers, walkers etc crossing in front of my property. We purchased our property for its proximity to the water. It's tranquility and beauty. Bringing in thousands of people a year to our neighborhood, more noise, more trash and cutting down trees is something I am VERY opposed to. We pay higher taxes to live in this beautiful area. Please don't destroy it with a regional trail. If as you say this project is 10 years out. Use what will probably be an abandoned railway at that time. Also, the southern portion of the regional trail is separated from homes by green space and fencing. You are proposing a trail situated practically in our front yard with no separation. I can't imagine many homeowners would want this.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	Again densely populated, poor fit for a small residential neighborhood.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	Please find a different path that is not entering a highly residential area, that cannot support this volume of people and usage.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	Same as previous comment		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	Bad fit near high school and neighborhood. It will create traffic hazards.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	once again this is NOT an appropriate alignment. GO back to the drawing board.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	NO trail here at all. Absolutely opposite to this alignment.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	I oppose any plan traversing residential neighborhoods. It is especially dangerous to have walking/biking paths on the same side of road where cars are exiting driveways.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	Language such as 'typically remains within the road right of way' is misleading and murky. Loss of parking, especially near the lake presents an inconvenience for homeowner's entertaining guests.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	Homeowners are losing property.		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	Money, money, money, think about it!		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	Get it out of the neighborhoods. This exact street is already a mess due to the high school remodel. This neighborhood has seen enough bad government dealings already!		
5/18/2022	Community Survey 2 - May 2022	White Bear Lake High School to Bald Eagle Blvd E to Park Avenue	this must be a joke, at no time previous have neighbors been made aware of a plan to go down to Bald Eagle Lake. This has been a recent change, when over covid? Where is the transparency? Do not think this is an appropriate place to add a high traffic bike trail, or to remove existing infrastructure to do so		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Seems fine. Could this be a one-way and convert the lane closest to the trail to parking?		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	I trust your recommendations.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	This makes sense to me. Keeping the trail on the lake side would allow for great views of the lake.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	This is my preferred route. I know some neighbors oppose but many more of us prefer this route. We love this route.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	I would prefer a one way here so that it reduces traffic congestion, more safety near the water, and allows space for both bikers and walkers to safely enjoy the trail.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	This mimics the design of lake Ave on white bear lake but with two way traffic. I don't see a high need for parking there. It's a great design.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	This alignment makes sense to us.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Make it a sloped transition from path to road like lake drive in white bear in case biker has to take the road to get out of the way of the family of 5 with 3 dogs.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	I live near here. I walk a different part of the lake and it is an uncomfortable walk - this would seem to take discomfort away from those who walk here. I ride a bike and would welcome a safer ride. I drive here too. I pull a boat here too.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Will reduce traffic making walkers and bikers safer.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Nice!		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	I oppose this Regional trail, it should wait to follow the railroad corridor as previously intended.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	The path would be 10-12 feet away from my side window. My privacy - the reason I bought my home - would be GONE. We have a very quiet street and would like to keep it that way. Property value of my home would definitely go down. I DO NOT WANT THIS PLAN, I don't want my trees or lilac bushes removed either, they provide lots of privacy.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Not much shoulder room for bikes		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	This is a narrow roadway with steep pitch on the lake side. This is a neighborhood. A regional trail has no business in a quiet neighborhood. Please consider going back to the original route along the railroad tracks.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Your nuts...the only one that prefers bald eagle blvd are you nut cases. Not one person living in the area wasn't it around the lake...not one		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	I oppose this regional trail, it should wait to follow the railroad corridor as previously intended.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	should be downtown next to hwy 61		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	A regional trail in a residential area is not a good fit. This is where we live and cross to our beach.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	This will seriously alter the parklike feel of that part of the community. It will take out a lot of trees. From my walks it is hard to believe the existing roadway is 32 feet wide.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Once again a Regional Trail DOES NOT belong in a residential neighborhood. Residents do not want fencing and retaining walls to enter their lakeside property.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Again why in a residential neighborhood. Those people pay huge taxes to see people walking in front of their lake view		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	We don't want your **** in our Town. get it		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	This alignment would inert large amounts of foot traffic and dangerous amount of bicyclist onto a road that residents use frequently to cross to access the lake. Imagining a large amount of bicycles zipping past while young children are trying to access the beach is scary.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Additional traffic from the new high school will make this an unpleasant route. It is not a good fit for the neighborhood. Parking would be needed in this area to accommodate large numbers of Regional Trail users. Parking would need to be located here like a parkway in Minneapolis.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Disruption to personal property, existing infrastructure and not enough corridor space for a trail.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	NO, ABSOLUTELY NOT. There are turtles that cross, frogs that cross in huge numbers during certain times of the year. Lots of people walking and home owners walking back and forth to docks. This is a safety issue. Bicyclists are not well known for their love of safety. We had bike runs down the street before and they were fast and unsafe. This is a terrible idea. The numbers using are far too high for a neighborhood.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Please find a different path that is not entering a highly residential area, that cannot support this volume of people and usage.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	I live in this section of Bald Eagle Blvd E and am strongly opposed because the traffic is terrible and cars drive way too fast along here. It makes no sense to add/encourage a significant number of pedestrians, runners and bikers to this area. With the new high school this area will have increased numbers of cars-especially teenaged drivers coming from school, sports and other activities. It is already difficult and dangerous for property owners to cross the road to enjoy our lake and adding more foot/bike traffic will make it even more difficult. The trail would also take away all street parking which is utilized often for all residents in this area as many don't have much space in their driveways. A regional trail such as this is not intended for residential areas. It was always intended to be in the abandoned railroad bed and should wait until that becomes available. Or utilize the trail that has already been established on Lake Avenue along White Bear Lake. I know it is said that that was taken off the table because their residents screamed loud enough. So now you want to route it through a residential area where we are not set up for it and it will add many possible dangerous situations not to mention inconveniences. You should come sit and watch traffic during busy times and see cars driving 50mph and going through the stop sign I walk or run 5 miles everyday and believe me I know this is a horrible idea!! DO NOT PUT THIS TRAIL WHERE IT DOES NOT BELONG!!		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	In addition to the traffic issues, this takes away all street parking in this stretch		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Negatives of proposal: Removal of trees, seizing property, dramatically increasing traffic in neighborhood-traffic and pedestrian hazards. This regional trail should not be installed through a neighborhood, injecting tens of thousands of people into a neighborhood. Regional trails work best on or alongside abandoned railroad right of ways.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	NOT support at all, terrible idea. Have you asked the folks that are mostly impacted. Putting up a fence, and retaining wall? Are you kidding? How are home owners to access their lake shore? THIS IS NOT FEASIBLE		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	I pay 12 000 a year in property taxes this would take away the land I own on the lake side. Lake avenue is so crowded biking is difficult we don't want the same thing on bald eagle lake we want it to be a quiet beautiful lake!		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	NO trail here at all. Absolutely opposed to this alignment. Do NOT destroy the current walking and biking, parking area that exist. The road was recently repaired with narrowed lanes, this works well already, DO NOT need to have more impervious space that runs into the lake!! Do not take parking or encroach to lake access that would make it impossible for people to access the lake.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	I oppose any plan traversing residential neighborhoods. It is especially dangerous to have walking/biking paths on the same side of road where cars are exiting driveways. Other alternatives are available and far less disruptive and destructive.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Removing on-street parking is an extreme hardship for visitors along this segment. Retaining walls and fencing will also create more ongoing maintenance expenses, which seem to already be non-existent or will result in higher taxes.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	I currently walk this route and it is not safe with the little bit of pedestrian traffic already. Now add 200,000 + people and the privacy of the neighborhood is gone. Might as well install a Walmart in the neighborhood.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Wow, elaborate drawings, nothing better to do.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	Get it out of the neighborhoods.		
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E to Park Avenue	again, strongly oppose this plan. You are not using existing foot print if you are taking away parking and using fences!!! I highly doubt those people that live along that road would want a fence or some wall to prevent them from accessing the lake shore. Have you considered the possibility of litigation or liability with that, apparently not.		



5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Is there room for a marked "Bike lane"? I don't want to be too disruptive to neighborhoods, if possible.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Would like more shoulder for bikes.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I like this. I think it worked well on Lake Ave. I wish the trail had more separation from the cars, but if the cars are made to travel in the 15-20 mph range, I think it's fine.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I love the idea of a one way for this section. It is a quiet street, with most traffic moving in the proposed direction and very few driveways. A one way is ideal and as a resident who lives on this stretch I am very much in favor of this proposal.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	YES! This is a perfect idea for this neighborhood	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Love this route. Many of my neighbors agree that this route is perfect. I know a few are upset, but so many more people support this. Don't let a few NIMBY arguments get in the way of this trail!	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	How can we determine if a fence will be needed on our property?	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Converting this to a one way will reduce car traffic by the lake. Making the neighborhood quieter and the path safer. Five stars.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	This alignment makes sense to us.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I would have voted for Southbound One-way. Based on my driving habits. I am primarily using this to get to and from Highway 61 using Buffalo Street. Most of the time I continue East on Park to get to Hugo Road to get to Buffalo to 61. Wider for the boat. If I am coming East to West I can reverse my course or go to the lake and continue Southbound.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I would think a Southbound One-way would reduce automobile traffic for those who live on the lake and would think that they would welcome that.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	This is consistent with Lake Ave trail. This is proven to be acceptable and enjoyable for the trail users. Having 1 way traffic is STRONGLY FAVORED!	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I know many of the residents along this section want no changes. But it is a public right of way that belongs to everyone. Bald Eagle Blvd is not a private driveway.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I am so glad to see these improvements in the works. The intersection of Taylor and Hugo Rd is one of the most dangerous I experience on a bike in the area. Poor visibility and fast car traffic make this intersection an accident waiting to happen.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Great!	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	You will be destroying the peoples yards that live there and forcing them onto a one way street is not right.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Goes through a residential Neighborhood.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I live on Taylor. Too many people in a Residential neighborhood. I like my peace and quiet. Don't need to put up with people NOT FROM MY NEIGHBORHOOD!	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Please rethink this.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Too tight and confusing. better to go straight east on buffalo	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I oppose this regional trail, it should wait to follow the railroad corridor as previously intended.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	The trail should run along the railroad tracks.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	We pay substantial property taxes to live Bald Eagle Lake. The proposed trail would dramatically increase the flow of traffic. Also, we're concerned about private property encroachment and value of our house.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	You would need none of this or far fewer modifications to the landscape if you used the area adjacent to the rail rd tracks on 61. That means w/ less S. At the least you should compare costs of two different designs	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I oppose this regional trail, it should wait to follow the railroad corridor as previously intended.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	should be downtown next to hwy 61.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Retaining walls and fencing will change the quiet character of this residential neighborhood.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	This will cause a significant change to the feel of the community with lost trees and a fence. The picture shows the existing roadway in the middle of the easement which is not accurate.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	One way limits residents access and diverts traffic on to other side streets. Also increased high school traffic. This is a narrow corridor and does not accommodate a Regional Trail. Many residents are opposed to a one way for a Regional Trail. Road could be a one way without a Regional Trail if that's what the community decides	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Don't need more traffic issues in a family neighborhood for people who do not live here, but choose to walk, ride or bike here.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Get some smart people on that can think	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	This road should not be a one-way. It will only increase the traffic speeds and the trail should not be installed along Bald Eagle. A Regional Trail does not fit in a single family residential neighborhood.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	This does not even consider a two-way! You cannot restrict access to homes in this area. These are long blocks with limited intersecting streets for home owners to access their homes. This one way idea has been brought up many times in the Township and it has been rejected every time. Stop trying to jam this into our neighborhood again - you are creating significant discord in our neighborhoods. A Regional Trail does not fit in a single-family residential neighborhood.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Regional Trail does not belong in a residential neighborhood. Creating one way roads diverts traffic to other streets and this will become worse with increased high school	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	See previous answers. A one way is inappropriate in this area. This has been suggested previously. This would impact traffic speeds as one way drivers have no opposing traffic to watch out for. It would possibly create longer response times for emergency vehicles and limit access for homeowners in this area. Where will bikers and walkers park? On narrow side streets????	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	The difficulty for the people in this portion of the project is awful. This is a neighborhood with a lot of disabled people, houses are affordable and most are one level. Accessing their homes as others do becomes an issue. This is just cruel to this neighborhood.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Please find a different path that is not entering a highly residential area, that cannot support this volume of people and usage.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	On way is a nightmare for this section and two way has too many negative/expensive impacts	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	People in this community have NOT wanted a one-way. It has been discussed for over 20 years, and the answer is the same. NO. Stop trying to push this trail into an area where it is NOT wanted.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	NO, NO, NO	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I oppose any plan traversing residential neighborhoods. It is especially dangerous to have walking/biking paths on the same side of road where cars are exiting driveways.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Having residents have to "back-track" to access their residences with a one-way street in this neighborhood is absurd. I don't consider that minimizing impacts to residents.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	This project does not belong in a quiet, settled community of single family homes. The influx of thousands of people will severely impact our quality of life, wildlife, tree canopy, and property values. Those who already walk, bike, and wander the area love it here - and don't want this.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I like to drive around the lake in both directions. Having a one-way limits that. Also, the homeowners are losing some of their property and will have their homes within the required setbacks to the new trails/roads.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Really, major over reach and invasive to landowners, community in general. Common sense should prevail!	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	Get it out of the neighborhoods.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	apparently you are unaware of how many times a one way in this area has been opposed.	
5/18/2022	Community Survey 2 - May 2022	Bald Eagle Blvd E from Park Avenue	I don't love the idea of a one way on this road for the people the live there.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	But that being said, I'd prefer any option that gets a tail built!!!!	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Where's the shoulder for bikes?	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	If you are going to do this section, would prefer one-way for the least amount of construction and disruption to existing trees. I think along hwy 61 would be better if at all possible.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	With the width issues on Hugo (south of the park) one way north bound from Buffalo to the park is the really the only option. Two way from the park to J with a cul-de-sac on the south end of the park. All park and boat traffic would come in from J. A median would stop north bound traffic from entering the park or launch.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Will this one-way travel south? I could see this adding a little bit of distance to homeowners returning home, but it's mostly traveling at high speed on hwy 61 so it wouldn't add much time to their commute. narrow lanes could keep it safer. would like to see more of a physical barrier between the cars and pedestrians. short hedges would be enough to improve safety.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Removal of southbound lane may be safer due to curve in the road and/summer growth. Visuals on northbound traffic should be reviewed. My sis lives on Taylor and I recall it being hard to see vehicles	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	We like the one-way options best - why expand paved surfaces more than is needed.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	If this questions is asking about support for one way traffic, I support this.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	I don't have a dog in this fight, but feel that it should run with the railroad.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	There should be a cul-de-sac where Hugo Road meets the park - to stop ALL boat trailer traffic in front of Hugo Road homes. (Boat trailers is the most dangerous traffic we see daily in the summer as residents.) Entering Hugo Rd from the south end (Taylor Ave) is much better for residents than from the North end (park).	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	IT IS A RESIDENTIAL NEIGHBORHOOD!	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	NOT A GOOD IDEA!	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	No Regional Trail. Narrow corridor. Doesn't belong here	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Horrible horrible thinking horrible. Typical democrats want to wreck good neighborhoods	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	This proposed trail should wait until the RR ROW when it is abandoned. That is a hugely underutilized amount of land.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	A one-way has been discussed many time previously in the Township and has been rejected every time. This proposed trail should wait until the RR ROW is available through abandonment.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Regional Trail does not belong in a residential neighborhood. Creating one way roads diverts traffic to other streets and this will become worse with increased high school traffic.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Emergency vehicle access times would increase. Inconvenience to homeowners in this area.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Wait for the tracks or work with Lake Ave and Lake Links. Nobody wants a Gateway trail in front of their house or in their neighborhood. This is just nonsensical.	
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Please find a different path that is not entering a highly residential area, that cannot support this volume of people and usage.	



5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Again - one way is problematic and two way has many negative impacts and costs		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	NO TRAIL		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	I oppose any plan traversing residential neighborhoods. It is especially dangerous to have walking/biking paths on the same side of road where cars are exiting driveways.		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	This regional trail was intended to utilize the railroad corridor and until that corridor is available, I do not support an alternative alignment that routes the trail through a residential neighborhood.		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	How much of our regional parkland will we lose to accommodate this mess? Will it become a giant parking lot for the bike riders?		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Make Hugo rd a cul de sac is my preferred option. Much less traffic hence safe for shared use of roadway without impinging on residents.		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	This road is already too narrow for the existing traffic. When you add in the fact that these are lake front properties and the activities on these properties involve the use of trailers & boats, how will the residents be able to move around the streets with large loads when you have 200,000+ added pedestrians and fast moving traffic. I already have to block traffic anytime I bring a trailer to my property to load or unload. Also, anytime a contractor needs to come fix something at my property, their commercial trucks have to be parked on the street blocking traffic. If you make this a one-way, the pedestrian walkway will become a street.		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	No, no, no.		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Get it out of the neighborhoods.		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	same rationale, these are collector roads for the neighborhood, it is a very old area, homes are close, not meant for increases in traffic including bikes, etc. If your biggest reason is to provide a view, then this is a misguided attempt. People can walk or bike along the lake already without addition disruption of the area. It is not appropriate here. It belongs in an area that can handle it, such as the RR corridor.		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	I think it is odd to propose a Northbound one-way thru Taylor only to be met by a Southbound one-way to continue North. You are forcing all people to go to Buffalo and then to 61 to go North to J to come back South to the Lake.		
5/18/2022	Community Survey 2 - May 2022	One Way Taylor Avenue/Hugo Road	Too confusing and too tight		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	I like this choice better, but I put my answer as "no preference" because I'm not super picky. I just want you to get it done.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Reduce the parking lane and/or the driving lane.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	This seems to be a more costly option that might not have a good enough use case to justify, I would go sign the one way option.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	I prefer one way traffic, but am not concerned about impacting the Railroad embankment.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	I favor keeping Hugo road as a two way.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	There should be a cul-de-sac where Hugo Road meets the park - to stop ALL boat trailer traffic in front of Hugo Road homes. (Boat trailers is the most dangerous traffic we see daily in the summer as residents.) Entering Hugo Rd from the south end (Taylor Ave) is much better for residents than from the North end (park).		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Less impact on the neighborhood. Would prefer using the railroad track when abandoned.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	It's a big enough road/space to support the effort to keep it two-way, in my opinion. Keep it a two-way and you will still be safe for walkers/bikers AND you won't confuse the picnickers who come to the park via Buffalo/Hugo.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	2 way north of recreation area.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	1 way south of recreation area.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Also extend south on Hugo road if possible		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	The one-way option seems way better.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Difficult to see cars		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	We strongly prefer the one-way option.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	IT IS A RESIDENTIAL NEIGHBORHOOD!		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	NOT A GOOD IDEA!		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	No Regional Trail		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Goofy Biden fans		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	We do not want to write thousands of strangers into our quiet neighborhoods.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	This proposed trail should wait until the RR ROW is available through abandonment.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Disruption to personal property.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	It would be sad to encourage the numbers to this area, it's heavily used by the neighborhood, it was a saving grace during covid. The trails as they are are good. Please consider not everyone loves bicyclists as they electric bikes are faster and they aren't careful among walkers and dog walkers.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Please find a different path that is not entering a highly residential area, that cannot support this volume of people and usage.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	NO TRAIL		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	I oppose any plan traversing residential neighborhoods. It is especially dangerous to have walking/biking paths on the same side of road where cars are exiting driveways.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	This regional trail was intended to utilize the railroad corridor and until that corridor is available, I do not support an alternative alignment that routes the trail through a residential neighborhood.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Those who live at the "Pinch Point" have driveways that barely accommodate a parked vehicle in front of their OWN HOME. How are they going to be assured of access to their property?		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Just can't see that being viable.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Taking out the south-bound lane on Hugo road would require me and my neighbors to have to travel an additional 3 miles every time we need to go anywhere south of buffalo and highway 61 - that seems like an unreasonable hardship that would be unacceptable to anyone if they were in the same situation.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	This road is already too narrow for the existing traffic. When you add in the fact that these are lake front properties and the activities on these properties involve the use of trailers & boats, how will the residents be able to move around the streets with large loads when you have 200,000+ added pedestrians and fast moving traffic. I already have to block traffic anytime I bring a trailer to my property to load or unload. Also, anytime a contractor needs to come fix something at my property, their commercial trucks have to be parked on the street blocking traffic. If you make this a one-way, the pedestrian walkway will become a street.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	Get it out of the neighborhoods.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	this is a terrible idea, take out green space to add a retaining wall, increase runoff to homes and the lake. Pinch point for a reason, this road has way too many obstacles to overcome just to have a better view for 30 secs while riding a bike. Get real.		
5/18/2022	Community Survey 2 - May 2022	Two way Taylor Avenue/Hugo Road	I prefer Hugo Rd be a culdesac Ending at the recreational area		
			Hi, I was not able to make it to the WBL City Hall meeting last night due to illness.		
			It would be nice to know what the percentage of the impacting cities residents completed the engagement surveys from fall 2021 and other engagement activities. 135+ tells me only 136 people completed the survey. Why is 135+ used as a marker? If only WBL residents participated (WBL has approx. 20,000 adults, not children) less than 1% of its adult populous engaged. More published logistical demographics of the survey participants would be appreciated. If there is please send me the link.		
5/18/2022	Community Survey 2 - May 2022	General Comment	I hope this aspect will be published in regards to this survey. This transparency would make me feel more comfortable knowing that the local community (WB Twp specifically, since it is NOT ward lead) is supporting the extension phase and not being pushed into it.		
5/18/2022	Community Survey 2 - May 2022	General Comment	The concept a regional trail into the area with the changes going on at north campus that will already cause overcrowding from junior and senior students cars is ill thought out. You should wait to see how that works before forcing a trail in.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Put the trail somewhere beyond residential neighborhoods. We are not public domain.		
			I highly support the trail extension and my life and well being would improve from it being built. I hate that WBL doesn't have much for safe recreation. I just walked this winter with my aging parents on the short little lake trail. They need an even, safe surface to walk on. I remember us talking about how nice it was, but it would be better to have something a little longer that trail is so short. I also don't feel super safe taking my baby for a walk in the stroller in WBL bc there are so few sidewalks!		
5/18/2022	Community Survey 2 - May 2022	General Comment	Pretty please build this trail fast!!!!		
5/18/2022	Community Survey 2 - May 2022	General Comment	I oppose this Regional trail, it should wait to follow the railroad corridor as previously intended.		
5/18/2022	Community Survey 2 - May 2022	General Comment	It's about time this safe biking hiking trail is completed.		
5/18/2022	Community Survey 2 - May 2022	General Comment	A few loud residents along Bald Eagle should not be able to disrupt this project.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Is there an option for the trail to avoid the Bald Eagle Ave portion in central White Bear Lake, eg by the high school and central middle school? That seems like it is asking for trouble with the increased amount of people, vehicles and bikes being added to the area for the school expansion and trail extension.		
			I've read where some opposing thoughts consistently write about how extending the Trail into "neighborhood areas" will open them to "unsavory individuals". I believe that the State of MN. has more designated Bike trails than any other state except California I'd like to see some data on how Thieves, etc. have used bike trails to gain access to neighborhoods and create problems with that access, any more than how they may currently access those same areas with vehicle access.		
5/18/2022	Community Survey 2 - May 2022	General Comment	I would like to know more details about the fencing along Bald Eagle Blvd E. Where is it proposed and what type of fence will be used? Also, do you have projected bike/pedestrian usage volume? Thanks		
5/18/2022	Community Survey 2 - May 2022	General Comment	I understand there is controversy and concerns related to the proposal. I encourage the County Board to look beyond the vocal voices of opposition and do what is best for the greater community. The community needs safe space to walk, bike and run. Places to gather and see your neighbors and connect with new faces. We allow need to provide safe space for kids to walk and bike to and from school. Thank you.		



5/18/2022	Community Survey 2 - May 2022	General Comment	use the RR corridor. this is ridiculous to send it through these neighborhoods.		
5/18/2022	Community Survey 2 - May 2022	General Comment	I am very excited for the project over all. Seeing a regional trail come to this area is fantastic. Running the trail along the lake will afford fantastic views and improve the property values.		
5/18/2022	Community Survey 2 - May 2022	General Comment	As a longtime bicyclist I am in favor of safe bicycle trails that get people outside and exercising in a healthy way! I believe the addition to the community will have a positive effect and not bring excessive noise or congestion to the area. We live 3 blocks from Bald Eagle Lake and current Lake and return on shoulder of Bald Eagle Ave. We look forward to more safe and fun trails in the future!		
5/18/2022	Community Survey 2 - May 2022	General Comment	I am excited to see planning and work to safely connect some of the wonderful biking, walking and running options we enjoy in the NE metro.		
5/18/2022	Community Survey 2 - May 2022	General Comment	The proposed route is favored. The alternative alignment is a reduction in the end result.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Get it done sooner rather than later!		
5/18/2022	Community Survey 2 - May 2022	General Comment	Can't wait for this to actually happen!		
5/18/2022	Community Survey 2 - May 2022	General Comment	Utilize railways. Avoid doing more construction on Bald Eagle Avenue. Too much happening in that area with the high school.		
5/18/2022	Community Survey 2 - May 2022	General Comment	I believe the residents being affected by this trail should have been given the opportunity to vote for No trail. This is an expensive project benefiting bikers and walkers who primarily would use the trail in the warmer months of the year. Very few would bike along the lake in the winter and walkers tend to decrease as well. I believe the cost and disruption to our neighborhood should be considered before reinventing the wheel for the use of a trail for 6 or so months out of each year.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Thank you for your recent interaction at WBL City Hall. I was in attendance. I think your presenters did a great job. I think the audience did a great job listening but a poor job when it came to Q&A. You are probably prepared for that (not that it was egregious, just humans being "me oriented"). Keep up the good work, this is for the people of today and tomorrow.		
5/18/2022	Community Survey 2 - May 2022	General Comment	I would really like to know more about the One-way section(s). As state above, I think Southbound would be better, so would want to know why Northbound was recommended.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Don't put the trail in a quiet residential neighborhood. Even if the railroad wants to keep the tracks, they ought to have more flexibility with their rules (probably very outdated rules) and allow a quiet bike path alongside a track where trains operate slowly and very infrequently.		
5/18/2022	Community Survey 2 - May 2022	General Comment	I was told at the community meeting that there are no other alternatives to Bald Eagle Ave and around Bald Eagle Lake. I do not believe this. I feel like you need to think outside of the box and MAKE another alternative. Why can't the trail run along highway 61, so that it is not running in the middle of an already fairly busy residential neighborhood? I would love to have the trail nearby, but not along Bald Eagle Ave and the lake.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Can't wait to see it done.		
5/18/2022	Community Survey 2 - May 2022	General Comment	One way along the lake area makes everything more safe for pedestrians. White bear lake did one way, lake Harriet, the ability to reduce the cars and give both walkers, bikers, and runners adequate space and safety should be the number one concern.		
5/18/2022	Community Survey 2 - May 2022	General Comment	I am a user of the trail systems in the twin cities and I am really looking forward to being able to stay closer to home and ride my bike on a safe designated trail. Sharing the road is scary these days. Thank you!		
5/18/2022	Community Survey 2 - May 2022	General Comment	We're very excited to have the trail extended past our house on Whitaker St. heading north to the county line!		
5/18/2022	Community Survey 2 - May 2022	General Comment	This is a much needed trail connection especially with Washington County planning to extend their Hardwood Creek trail south from Hugo to County Rd 1.		
5/18/2022	Community Survey 2 - May 2022	General Comment	I'm really glad to see the trail design progressing. Lake Ave is heavily utilized (especially during the pandemic), and lots of people from the region enjoy being able to get outside and walk along the lake on a safe trail. Adding another trail option is great. I would also add that the people in the lake ave neighborhood can pretty painlessly manage a one-way street to accommodate a trail, and other neighborhoods will find they can manage it too.		
5/18/2022	Community Survey 2 - May 2022	General Comment	How many other trails run through residential neighborhoods? No one I have talked to lies the idea. We like our neighborhood. It is peaceful. The trail would change the dynamics of the area and not for the good.		
5/18/2022	Community Survey 2 - May 2022	General Comment	There has been a lack of engagement for this route with the residents that are most impacted. The recent meeting in WB made it clear the community does not support a route that places a Regional Bike Trail in a residential neighborhood. This position is also supported by the WB Township Board. A Regional Trail is a poor fit for a residential neighborhood. This is no different than why people on Lake Ave did not want this. End it at 96 and wait for the RR corridor to become available.		
5/18/2022	Community Survey 2 - May 2022	General Comment	This area cannot support the size and scope of this project with adding the new school. A residential area is not the place.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Please reroute the regional trail so it does not impact neighborhoods.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Stay out of the neighborhood, use abandoned railway or parallel hwy 61.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Oppose this regional trail coming into neighborhood, it should wait to follow the railroad corridor as previously intended		
5/18/2022	Community Survey 2 - May 2022	General Comment	Trump will win yes		
5/18/2022	Community Survey 2 - May 2022	General Comment	I don't believe the trail should be extended at all.		
5/18/2022	Community Survey 2 - May 2022	General Comment	A Regional Trail does not fit in a single family residential neighborhood. Stop trying to ram these projects into our quiet neighborhoods. We want to live in a quiet, safe, neighborhood where we know the people who walk through our neighborhoods and we do not want hundreds or thousands of strangers from the region that would use a regional trail. I strongly oppose the BV trail regional trail Segment 4 routing along Bald Eagle Blvd E. This is unacceptable routing, especially as the North Campus is significantly expanding and will have increased traffic in the area. The BV trail should follow the railroad corridor as originally intended.		
5/18/2022	Community Survey 2 - May 2022	General Comment	This project is invasive government over reach. I find it ironic Susan Vento MET COUNCIL represents this district. Wildlife will be disturbed pollinators removed and last but not least 270,00 people annually passing through an area that can not support that kind of traffic. No other regional trail passes through a residential area such as ours you know this is wrong when the township board does not support it. Last point you put the barge up the creek and then hand the road back to the township so taxpayers can maintain it not very cool at all.		
5/18/2022	Community Survey 2 - May 2022	General Comment	I do not support. There has been no engagement with WB Township residents until a Rer the route was chosen. We do not want this in our neighborhood. Go somewhere else		
5/18/2022	Community Survey 2 - May 2022	General Comment	Rethink this project. It's such a poor fit for a neighborhood. The tracks seem the better option. Population is down in Ramsey County, this money could be put to better use in other neighborhoods with less options enjoyed in this area.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Please use the other alignment that was presented before.		
5/18/2022	Community Survey 2 - May 2022	General Comment	With a goal to carry this trail through area why not either wait for railway access or use the existing trail along White Bear Lake? From a fiscal standpoint building this trail is a waste of taxpayer money given these two options. From a traffic perspective, this area will be undergoing tremendous change with the new high school. Adding a trail with many new drivers is asking for trouble. Taking away parking all along Bald Eagle Lake is a very negative impact. Finally, nothing has been said about environmental impact. Lots of great work is being done to improve water quality - what will this accomplish? These trails typically do not go through the middle of neighborhoods for two reasons - one is a desire to not disrupt the neighborhood feel and the other is to limit expense. Keeping this out of the neighborhood is the right answer.		
5/18/2022	Community Survey 2 - May 2022	General Comment	The proposed project seeks to inflict a high traffic regional trail into a neighborhood, dramatically impacting the safety and ambience of the neighborhood. Please find another alignment (abandoned railroad right of way) that is appropriate to this level of traffic.		
5/18/2022	Community Survey 2 - May 2022	General Comment	The trail is NOT wanted here for many reasons. The current proposed format that goes along the lake for "better viewing" is inappropriate. Lest you are familiar with the area, the wind is high, it is mostly winter in Minnesota, not conducive for biking here. Restore the bike path idea along the railroad corridor when it comes available. Have some patience!		
5/18/2022	Community Survey 2 - May 2022	General Comment	Do not do it don't take away the small town simple feel of bald eagle lake		
5/18/2022	Community Survey 2 - May 2022	General Comment	DO NOT place this trail anywhere else except the railroad corridor, that was the original plan, STOP pushing this into an area that does not support it! THE END		
5/18/2022	Community Survey 2 - May 2022	General Comment	Strongly oppose this project. With the new restructure of school and traffic flow bringing more traffic to the area is undesirable. Turning a residential area into recreational area is not wanted. I have attended open meetings and there were no answers for parking, crime, bathrooms, litter.		
5/18/2022	Community Survey 2 - May 2022	General Comment	The community engagement has been extremely poor. The community input has been misrepresented. The change of plan from White Bear Lake to White Bear Township was not transparent and incredibly unfair.		
5/18/2022	Community Survey 2 - May 2022	General Comment	We reside here due to the relatively quite peaceful nature of the area and routing a regional trail through this residential neighborhood will destroy that feel. The impacts from the loss of parking, construction and environmental impacts from removal of trees, runoff, trash, etc. the estimated high volume of users and lack of engagement of affected residents when determining the current alignment back in 2019 leave a foul taste in my mouth about the government entities involved in this boondoggle.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Stop it. You ran a public information meeting during a pandemic - thus limiting attendance. The second meeting was scheduled during MEA, again assuring limited attendance. The last meeting Mr Yarusso declined to answer questions or address concerns because of "community engagement" he didn't wish to hear. We don't want this.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Overall the proposed route is an absurd idea. Shelve it.		



			There has not been a true 3rd party feasibility study done on the projected route. The county did their own study which we all know can then be biased, and continue to refer to this study as this is the best route. I implore the county to do a 3rd party feasibility study as there are alternative routes with less neighborhood disruption. At the last meeting of impacted residence, not one person supported this. Of course you will have support from unaffected people, they have nothing to loose. It's not a fair survey to ask unaffected people. If you did a survey of just the affected people, I believe you'd have a 90%+ opposition to this project.		
5/18/2022	Community Survey 2 - May 2022	General Comment			
5/18/2022	Community Survey 2 - May 2022	General Comment	Invasive to residential area.		
5/18/2022	Community Survey 2 - May 2022	General Comment			
5/18/2022	Community Survey 2 - May 2022	General Comment	Oppose going along this current proposed route, should allow for time to use RR ROW as originally intended		
5/18/2022	Community Survey 2 - May 2022	General Comment	I think a REGIONAL TRAIL through these neighborhoods is very wrong.		
5/18/2022	Community Survey 2 - May 2022	General Comment	No trail. We live on Whitaker St near Lincoln. There will be too many people using this and parking will be horrendous. We love our nice quiet neighborhood. NO TRAIL! Leave White Bear Lake as it is, a lovely village like community.		
5/18/2022	Community Survey 2 - May 2022	General Comment	This is another wasteful government project affecting the White Bear neighborhoods. This, like the proposed bus, is not wanted yet is being forced upon residents. Stop the project.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Use the railroad corridor, if it is not currently available then wait. What is the reason for the push? If it is available funding, then it is completely misguided to push it through when the timely is not there. I am sure there are many other projects, especially those in more need, that can be worked on in the meantime. Those of us who have lived in the neighborhood for a long time, see projects come and go, this is one that needs to be tabled, first until the high school project from hell is done. Things change, maybe by then the train bed may be available.		
5/18/2022	Community Survey 2 - May 2022	General Comment	I am an avid bike rider, and I would not likely use a trail that was shared with walkers, too dangerous. Bikes belong on the road, it has never been difficult to plan a ride in the area. It appears this trail would not be a good fit where this plan intends, and appears to be a lot of overspending tax payer dollars.		
5/18/2022	Community Survey 2 - May 2022	General Comment	Quit showing this down our throat. Do what the people of this town want to you stupid ass designers taking people's existing blvd and making it a night mare		
May-22	Wikimap Comments	Concern or Opportunities	I biked and drove Bald Eagle Blvd. It is too narrow for bike safety, and not worth the detour on the lake compared to just continuing the path down or up Hugo Road and Long Avenue to 8th to get to and from downtown businesses, schools and churches. Plus Long Avenue has the Art, Children's and Theatre for families to access via the trail with this route.		
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May-22	Wikimap Comments	Concern or Opportunities	Continue to evaluate a Rails with Trails option with experts. I biked the corridor from 96 to 8th and there seems to be enough access for a trail 25 feet from the rail centerline. Highway 61, parking lots, crosswalks and the cobblestone path by the depot and Chamber building are all within 5-15 feet of the existing track centerline in places. This seems to provide a precedent to consider a Rails with Trails option from 96 to 8th, separated by fencing if needed. There could also just be a scheduling agreement, that the trail is open during daylight, and the train runs at night. The original Bruce Vento Trail Plan always had the rail route as the preferred trail path.		
May-22	Wikimap Comments	Concern or Opportunities	Improve crossing safety from 96 to Lake Ave Trail		
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May-22	Wikimap Comments	Community Destinations	Ensure the trail provides as direct as possible routes to the White Bear Lake business districts, including destinations like Cup & Cone.		
May-22	Wikimap Comments	Community Destinations	White Bear Lake High School campus entrance.		
May-22	Wikimap Comments	Community Destinations	Arts Building		
May-22	Wikimap Comments	Community Destinations	Children's Performing Arts		
May-22	Wikimap Comments	Community Destinations	Theater		
May-22	Wikimap Comments	Community Destinations	Antique Stores		
May-22	Wikimap Comments	Preferred Trail Entrance	Rails with Trails from 96 to 8th, per original Bruce Vento Master Plan, with direct access to the business district. <a href="#">a€Donk™ Bypass the White Bear Lake Business District</a> ™ the destination families and students are traveling to and from.		
May-22	Wikimap Comments	Preferred Trail Entrance	Rails with Trails from 96 to 8th, per original Bruce Vento Master Plan, with direct access to the business district. <a href="#">a€Donk™ Bypass the White Bear Lake Business District</a> ™ the destination families and students are traveling to and from.		
May-22	Wikimap Comments	Preferred Trail Entrance	Rails with Trails from 96 to 8th, per original Bruce Vento Master Plan, with direct access to the business district. <a href="#">a€Donk™ Bypass the White Bear Lake Business District</a> ™ the destination families and students are traveling to and from.		
May-22	Wikimap Comments	Preferred Trail Entrance	Continue to follow Hugo Road to Long Ave to 8th, next to the rail line, closest to the original Bruce Vento Master plan.		
May-22	Wikimap Comments	Preferred Trail Entrance	Continue to follow Hugo Road to Long Ave to 8th, next to the rail line, closest to the original Bruce Vento Master plan.		
May-22	Wikimap Comments	Community Destinations	Playground		
May-22	Wikimap Comments	Community Destinations	Park, baseball field.		
May-22	Wikimap Comments	Community Destinations	Cup & Cone		
May-22	Wikimap Comments	Community Destinations	Subway		
May-22	Wikimap Comments	Community Destinations	City Hall		
May-22	Wikimap Comments	Community Destinations	Eagle Brook Church		
May-22	Wikimap Comments	Concern or Opportunities	The Bald Eagle route bypasses the preferred White Bear Lake Business District destination and goes behind the school, not the entrance. This detour up to Bald Eagle Lake is not preferred by bicyclists and walkers accessing downtown White Bear Lake or traveling to and from Hugo.		
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May-22	Wikimap Comments	Preferred Trail Entrance	Trail close to rail line path through woods or the park.		
May-22	Wikimap Comments	Preferred Trail Entrance	Trail through the woods and park if possible.		
May-22	Wikimap Comments	Concern or Opportunities	Improve 61 and intersections crossing, with flashing beacons on Hugo Road.		
May-22	Wikimap Comments	Preferred Trail Entrance	Access to and from Lake Links Trail to Bruce Vento to the park or Hugo.		
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May-22	Wikimap Comments	Preferred Trail Entrance	Rails with Trails <a href="https://www.railstotrails.org/trailblog/2021/june/25/new-usdot-report-provides-25-years-of-lessons-learned-for-rails-with-trails/">https://www.railstotrails.org/trailblog/2021/june/25/new-usdot-report-provides-25-years-of-lessons-learned-for-rails-with-trails/</a>		
May-22	Wikimap Comments	Preferred Trail Entrance	Rails with Trails resources, contacts and best practice resources: <a href="https://www.fhwa.dot.gov/environment/recreational_trails/publications/rwt2021/">https://www.fhwa.dot.gov/environment/recreational_trails/publications/rwt2021/</a> Peter Dahlberg Minnesota Department of Transportation		
May-22	Wikimap Comments	Preferred Trail Entrance	Rails with Trails - Twin Cities Examples <a href="https://streets.mn/2020/12/07/rail-with-trails-of-the-twin-cities/">https://streets.mn/2020/12/07/rail-with-trails-of-the-twin-cities/</a>		
May-22	Wikimap Comments	Community Destinations	MarketFest		
May-22	Wikimap Comments	Community Destinations	Downtown Restaurants and Businesses, including Big Wood, Cobblestone, Brickhouse, Washington Square, Grandma's Bakery, Anchor Coffee, etc.		
May-22	Wikimap Comments	Community Destinations	Library		



May-22	Wikimap Comments	Community Destinations	Church		
May-22	Wikimap Comments	Community Destinations	Lake Ave & Lake Links Trail, lake, beaches and parks		
May-22	Wikimap Comments	Community Destinations	Park, restroom, boat launch, picnic tables and benches		
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May-22	Wikimap Comments	Community Destinations	Beach swimming, restrooms		
May-22	Wikimap Comments	Community Destinations	Park - picnic tables and restroom		
May-22	Wikimap Comments	Community Destinations	Beach, swimming, restrooms and picnic tables. Parking for bicyclists and cars.		
May-22	Wikimap Comments	Community Destinations	Lake Links Trail around White Bear Lake - access to and from Bruce Vento Trail to or from Hugo and Bald Eagle Park		
May-22	Wikimap Comments	Concern or Opportunities	Improve crossing 61 at 7th or 8th for families and children going to the beaches, downtown White Bear Lake &/or the arts centers.		
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May-22	Wikimap Comments	Concern or Opportunities	Improve crossing safety and length of walk lights, no right turns for 61 & 4th, this is the major highly used intersection for families to cross between the business districts. Install bridge in future.		
May-22	Wikimap Comments	Community Destinations	provide direct connection to WB high school to allow students to bike to school easily		
May-22	Wikimap Comments	Concern or Opportunities	Is Lake Avenue not even being considered for the Vento trail? It already has a walking/bike lane.		
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May-22	Wikimap Comments	Concern or Opportunities	This portion of East Bald Eagle Blvd is used by walkers, dog walkers, and it is not wide enough. If adding bicycles to this, the speed limit should be reduced. Cars go fast and I often have to step onto the steep side of the road until they have passed. Bike groups use this route now and it is sometimes scary when they are coming downhill. Families cross this road to access their docks. Please keep these things in mind.		
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May-22	Wikimap Comments	Concern or Opportunities	The proposed segment on Bald Eagle Blvd. E. from Park to Taylor has many geometric constraints which would make it unsafe for trail users. These include a narrow right of way with steep grades on either side and many vertical and horizontal curves which limit sight distances. A much safer alignment would be Park Avenue to Hugo Road then north to Co. Rd. J		
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May-22	Wikimap Comments	Concern or Opportunities	Bald Eagle Blvd is the logical location for the trail given its proximity to and views of the lake. Large numbers of people walk and bike along the lake today despite very unsafe conditions. Itâ€™s time we provide some basic safe accommodations for this undeniable demand and enable the entire community to enjoy this tremendous resource.		
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May-22	Wikimap Comments	Community Destinations	Even if the Bruce Vento Trail isnâ€™t constructed on Bald Eagle Blvd, that is where walkers and bikers will continue to go because the views of the lake are the magnet that attracts the use.		
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May-22	Wikimap Comments	Concern or Opportunities	The loss of trees, which create a windbreak and keep the banks from eroding, especially on the south side where the ice blows in in the winter is a major no for me.		



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May-22	Wikimap Comments	Community Destinations	This configuration seems the best. people could cross G1 at traffic control at Buffalo and continue on to Gateway if they wanted. There is a park/launch at Bald Eagle/Otter Lake Launch that has facilities, fishing, picnic tables and trash disposal.		
May-22	Wikimap Comments	Concern or Opportunities	East Bald Eagle is quite narrow and shorelines are steep. If we make this an official leg of the bike trail, it should be rezoned to a one way along this stretch of Bald Eagle Blvd East to allow adequate space and safety for bikers and walkers, alike.		
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May-22	Wikimap Comments	Concern or Opportunities	How do they plan to add a lane to this already narrow road without taking out the railroad tracks? There is limited parking on this road already and no room for expansion without taking peoples yards and driveways?		
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May-22	Wikimap Comments	Concern or Opportunities	There is barely room for 2 lanes of traffic. If they took property from the homeowners to make the additional lane, some would not have room on thier driveway for a car/truck to fit without sticking out into traffic.		
May-22	Wikimap Comments	Concern or Opportunities	They could run the trail on the East side of the tracks. It could then easily connect to the current trail running North to Hugo.		
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May-22	Wikimap Comments	Concern or Opportunities	I hope there is no plan to remove the trees that buffer the Highway 61 noise on Hugo Rd. Also, the road is already narrow as you approach the Park going north, how can you fit in a trail? Now maybe if you make Hugo a one way it might work?		
May-22	Wikimap Comments	Concern or Opportunities	This is a narrow road that many vehicles speed on right now, making it unsafe for walkers and bikers. How are they going to add a bike/walking trail to the already narrow road and still have parking available for residents if needed? I would be open to making it a one way and taking one lane for the trail and the other lane for vehicle traffic. Better yet, make it a dead end with a spot for the trail to go through but not vehicular traffic. Boat traffic is supposed to go around to J to get to the boat launch...this would solve that problem as well.		
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May-22	Wikimap Comments	Community Destinations	Many people already use this section daily, including walking, running, and biking. Even though the road can be narrow, the road could easily be turned into a one way as this road is lightly traveled and best and there are many options to connect to other roads for those few residents with driveways on E Eagle Blvd.		
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May-22	Wikimap Comments	Concern or Opportunities	Tons of bike, walking, and running traffic on this portion of Bald Eagle Blvd already, it would be silly to put the trail somewhere else, people will still follow this path due to much better views. You cannot, however, put the trail here without making this road a one way, there is not enough room to be safe. I live on the lake and regularly bike with my kids on this portion, hugo road as well as Bald Eagle Blvd are way too narrow to also add a bike path.		
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May-22	Wikimap Comments	Community Destinations	This is where the existing Bruce Vento Regional Trail ends.		
May-22	Wikimap Comments	Community Destinations	I realize the current plans have the trail extension going north and not East, but it would be great to have a safer extension to Century College either from Bruce Vento or the Gateway. Right now, bike lanes are not very wide or non-existent and the college continues to serve larger populations from the city of St. Paul. Thanks!		
May-22	Wikimap Comments	Concern or Opportunities	Bald Eagle Blvd E. and Avenue is extremely busy with local traffic and anticipated to increase with the expansion of the WBL High School project. The Bruce Vento Trail along this route is NOT recommended due to this increase in traffic and very limited shoulder along Bald Eagle Blvd E. The railway would provide a much safer route for biking/walking on the Bruce Vento Trail. I am against the proposed route on Bald Eagle Blvd E. and Bald Eagle Avenue. This is NOT safe and should be avoided.		
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May-22	Wikimap Comments	Concern or Opportunities	One way on E Bald Eagle is a terrible idea. We have fought this idea over and over. The DOT has found no oncoming traffic makes drivers more confident to speed. If the Blvd. is expanded many homeowners will have the trail practically on their porch. I counted at least 25 trees that are within 4 feet of the road that would have to be cut down if the Blvd was widened. The trail needs to go elsewhere.		
May-22	Wikimap Comments	Concern or Opportunities	Making E Bald Eagle Blvd one way is a terrible idea that the DOT has studied already. They found no oncoming traffic made drivers more likely to speed. There are not enough cross streets for homeowners to have convenient access to their homes. Widening the roadway would mean at least 25 trees would have to be cut down and some homes would have the trail almost to their porch. The trail needs to find another path.		
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			I live at this location. The road speeds are not safe enough to support a trail designation without a change in infrastructure. Ramsey County did a speed study on the road in the fall of 2020 and found the 85th tile to be 39 MPH in a 30 MPH posted zone. There is no room to have a 2 way road and a bike trail next to the RR tracks. If a 1 way is being considered on E 86 Blvd then why not here? With a lack of right of way due to already narrow lots and RR tracks makes the a N. bound 1 way with separate bike path the safe and logical option, it could be configured to preserve existing parking for residents along with needed natural greenery to block noise from RR and 61.		
May-22	Wikimap Comments	Concern or Opportunities			
May-22	Wikimap Comments	Concern or Opportunities	Why would we replace a 104' strip of permeable shoreline and remove tree root structure that protects against shoreline erosion with a 104' wide non-permeable asphalt path. We need to protect the lake as a resource not create a 400 yard viewing area for bike riders. This trail route is not the preferred trail route. This is an alternate route after they were denied their original route by the railroad. They are looking for a place to put it and it does not belong in a residential neighborhood		
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May-22	Wikimap Comments	Concern or Opportunities	Bald Eagle Ave and Stillwater Road is a high traffic intersection that is only going to get busier with increased traffic from WB high school. Another example why this trail should not be bright through a residential neighborhood and a high traffic roadway.		
May-22	Wikimap Comments	Concern or Opportunities	Bald Eagle Ave and Stillwater Road is a high traffic intersection that is only going to get busier with increased traffic from WB high school. Another example why this trail should not be bright through a residential neighborhood and a high traffic roadway.		
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May-22	Wikimap Comments	Concern or Opportunities	It appears utility poles will need to be moved closer to residents homes. Does anyone want a power line moved close to their house ? This is probably the only area along the 13.3 mile trail where this is happening. Will the homeowners pay an assessment for something they don't want or need ? Another reason this does not belong in a residential neighborhood		
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May-22	Wikimap Comments	Concern or Opportunities	This appears to be the third entrance into the HS and athletic fields. The is also an active RR crossing here. This is the main school bus route plus a significant increase in student traffic.		
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May-22	Wikimap Comments	Concern or Opportunities	This appears to be the third entrance into the HS and athletic fields. The is also an active RR crossing here. This is the main school bus route plus a significant increase in student traffic.		
May-22	Wikimap Comments	Concern or Opportunities	Very busy intersection.		
May-22	Wikimap Comments	Concern or Opportunities	Very busy intersection.		
May-22	Wikimap Comments	Concern or Opportunities	For everyone who thinks it's obvious and easy to make E Bald Eagle a one way they need to realize you move that traffic to other residential streets & it doesn't just go away. The decision to create a one way has an affect on other people and neighborhoods. Also, you can create a one way with or without a trail route. If it's so necessary why has it not been done ? Ask your Township. It's because you need to consider how it impacts people who live in the area not just where people want to walk and bike		
May-22	Wikimap Comments	Concern or Opportunities	For everyone who thinks it's obvious and easy to make E Bald Eagle a one way they need to realize you move that traffic to other residential streets & it doesn't just go away. The decision to create a one way has an affect on other people and neighborhoods. Also, you can create a one way with or without a trail route. If it's so necessary why has it not been done ? Ask your Township. It's because you need to consider how it impacts people who live in the area not just where people want to walk and bike		
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May-22	Wikimap Comments	Concern or Opportunities	In general I don't think people understand the difference between making E Bald Eagle Blvd safer for walking / biking vs the creation of the Bruce Vento Trail through a residential neighborhood where it was never intended to go. One can be accomplished without the other . People deserve to not have a 104' wide asphalt trail come through their property, a trail 304' from their front door, power poles moved closer to their homes etc.		



			In general I don't think people understand the difference between making E Bald Eagle Blvd safer for walking / biking vs the creation of the Bruce Vento Trail through a residential neighborhood where it was never intended to go. One can be accomplished without the other . People deserve to not have a 10ft wide asphalt trail come through their property , a trail 30ft from their front door , power poles moved closer to their homes etc .		
May-22	Wikimap Comments	Concern or Opportunities			
			There are numerous pedestrians that use this corridor (and further to the SW along the lake) in an unsafe existing condition, so pedestrian facilities would be welcomed.		
May-22	Wikimap Comments	Concern or Opportunities	What are the alternatives for typical sections for this area? Currently drainage is handled by ditches, which could be mitigated with C&G and storm sewer and could allow for the widening of the section, but could be costly, if this becomes a one-way, how would the signal at County J be revised to accommodate the additional capacity?		
May-22	Wikimap Comments	Concern or Opportunities	This portion of the trail has a private school , a mortuary and St Mary's of the Lake Church , all which utilize on street parking and there is little room for a trail		
May-22	Wikimap Comments	Concern or Opportunities	This portion of the trail has a private school , a mortuary and St Mary's of the Lake Church , all which utilize on street parking and there is little room for a trail		
May-22	Wikimap Comments	Concern or Opportunities	Sharp narrow curve with homes close to the road. The trail is too close to homes. Side streets in this area cannot accommodate traffic diverted into the neighborhood to support a one way		
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May-22	Wikimap Comments	Concern or Opportunities	Need to have parking for cars for visitors who will want to walk only this section. Where is that parking going to be?		
May-22	Wikimap Comments	Concern or Opportunities	Need to have parking for cars for visitors who will want to walk only this section. Where is that parking going to be?		
May-22	Wikimap Comments	Concern or Opportunities	This is a great place for walkers and hiking. Nice green space and wildlife. Good views of the lake.No traffic. No asphalt path required. No need to place a Regional Trail through peoples property or close to their homes. It's an under utilized resource for walkers		
May-22	Wikimap Comments	Concern or Opportunities	Where do all these people expect to park. The streets are already standard size.		
May-22	Wikimap Comments	Concern or Opportunities	Put the trail along Long Ave to increase access to Arts District. This district is trying to decide how to get more pedestrian and bicycle traffic to their area.		
May-22	Wikimap Comments	Preferred Trail Entrance	Have the trail continue after Hoffman Road to cross Hwy 61 at White Bear Ave. then can continue around S. Shore Blvd or into town to go along Lake Ave. that already has an existing path.		
May-22	Wikimap Comments	Preferred Trail Entrance	Prefer to have the trail to enter here to go along the already existing bike/ walking trail on Lake Ave. this is easy access to get to town, beaches, and other trails. Do not go north up Bald Eagle Ave., it is out of the way to get to anything.		
May-22	Wikimap Comments	Preferred Trail Entrance	Prefer to go this route to enter along Lake Ave. near parks beaches and town		
May-22	Wikimap Comments	Preferred Trail Entrance	Prefer to enter at Ramsey Beach to go along Lake Ave. this avoids a lot of traffic on Hwy 61 then can enjoy Lake Ave. and connect on Hwy 96 to get to Gateway trail		
May-22	Wikimap Comments	Preferred Trail Entrance	The trail can follow the route from Hoffman Road then can utilize the already existing path along Lake Ave. to get to Hwy 96 near Ramsey Beach, that can reach the Gateway trail, and Lake Links. The crossings can be improved at White Bear Ave to accommodate bikers. This avoids traffic along Hwy 61 and is easy access to town if people want to go there.		
May-22	Wikimap Comments	Preferred Trail Entrance	Use already existing bike path on Lake Ave for preferred route to access town, trails and Ramsey beach		
May-22	Wikimap Comments	Preferred Trail Entrance	Since the BVT is supposed to connect to county Rd J then why not use Portland, it would connect all the way to Hwy 96 then can ride to gateway and to Ramsey beach, the mark sather trail along Lake Ave.		
May-22	Wikimap Comments	Concern or Opportunities	Yes, use the route along Lake Ave as the preferred route. It connects to Lake links, and to future Vento trail at Hwy 61 and white bear ave., and to south shore drive. Also bonus to go north to Ramsey beach, Hwy 96, and reach county J along Northwest or Portland where the road is already wide enough to accommodate bikers.		
May-22	Wikimap Comments	Concern or Opportunities	Need an all-way stop sign at Taylor Avenue and Hugo Road. This is needed whether or not the bike trail extension is built.		
May-22	Wikimap Comments	Concern or Opportunities	I suggest keeping Hugo Road two-way, but end it for motorists at the south entrance to the park. Make it bikes-only from there to the boat access road. That would finally get the boat trailers off this stretch of road. I live in the Overlake neighborhood. I already drive north to get out because of the terrible condition of Hugo Road going south.		
May-22	Wikimap Comments	Concern or Opportunities	This is a busy section of Bald Eagle Avenue with a pizza & ice cream shop and frequent on street school & church parking (with a funeral home across the street), which could pose an issue.		
May-22	Wikimap Comments	Concern or Opportunities	The trail is isolated from other connecting trails and the best use would be local. It would better to connect it to the current Vento trail that ends at county road E. HWY 96 is difficult to cross and not a bike friendly route. I use bike trails to travel through congested areas. I ride about 4,000 miles a year and live in Vadnais Heights.		
May-22	Wikimap Comments	Concern or Opportunities	Stillwater St is not designed for high traffic volume, no shoulder or stop signs. High risk of pedestrian harm. High School will make side streets busier. Regional trail not needed at this time.		
May-22	Wikimap Comments	Where you would go along trail	Please use Hugo Road and Long Avenue to follow close to the original Bruce Vento Trail Master plan to 8th Street. Then use Hugo Road and Long Avenue to follow close to the original Bruce Vento Trail Master plan to 8th Street. Then 8th to Bloom (High School entrance), Bloom to 4th. This gives students, walkers and bikers the most direct and safest path to travel from Bald Eagle Park to the High School campus and the west business district, like to Cup & Cone, etc. This route also supports access to the arts and theatre, and businesses on Long Ave. The trail path could be part of the road resurfacing.		
May-22	Wikimap Comments	Where you would go along trail	Please use Hugo Road and Long Avenue to follow close to the original Bruce Vento Trail Master plan to 8th Street. Then 8th to Bloom (High School entrance), Bloom to 4th. This gives students, walkers and bikers the most direct and safest path to travel from Bald Eagle Park to the High School campus and the west business district, like to Cup & Cone, etc. This route also supports access to the arts and theatre, and businesses on Long Ave. The trail path could be part of the road resurfacing.		
May-22	Wikimap Comments	Where you would go along trail	Please don't bypass the business district. People want to bike and walk to Downtown and the west business district, including students. Routing the bike trail going north from 96 on Bald Eagle, to 4th provides access to the business district and family destinations like Cup & Cone. Using Bloom provides a safe path to the front of the school campus (not the back, like Bald Eagle). Following 8th Street back to Long Ave/Hugo Road gives riders the safest, most direct reduced intersection path north to Bald Eagle Park, closest to the original Bruce Vento Trail Master Plan, preferred by bicyclists traveling to and from downtown White Bear Lake businesses, churches and schools.		
May-22	Wikimap Comments	Where you would go along trail	Avoid the narrow boulevard. This route passes by a nice, shaded park on Eagle. This road however, is heavily used during the school year, along with Division and Bald Eagle Avenue.		
May-22	Wikimap Comments	Where you would go along trail	Connect this route to the Main Bruce Vento trail.		
May-22	Wikimap Comments	Where you would go along trail	Have the trail go from Hoffman Road to cross at White Bear Ave. or Hwy 96, or Whitaker to connect to Lake Ave. then along Lake Ave to Ramsey County Beach to connect to Hwy 96 at the Beach.		
May-22	Wikimap Comments	Where you would go along trail	At the end of Hoffman Road Have a crossing for the trail to go to South Shore Blvd. and to get to town along the Lake there is another trail at Lake Ave.		
May-22	Wikimap Comments	Where you would go along trail	The trail should follow Lake Ave, an already existing trail with easy access to town, Ramsey beach, and other trails.		
May-22	Wikimap Comments	Where you would go along trail	The trail can continue along Portland North from Hwy 96 and Ramsey Beach to reach other connecting trails		
May-22	Wikimap Comments	Where you would go along trail	BVT should continue from Hoffman road to cross Hwy 61 (with an improved crosswalk) to allow bikers to either follow South Shore Blvd. or continue north reaching Lake Ave. then traveling to Ramsey Beach Park can continue on to Hwy 96 connecting to Lake Links and Gateway.		
May-22	Wikimap Comments	Where you would go along trail	This is the easiest most beautiful way to go and the way we bike for 25 years since we live in the Benson Bay area on Bald Eagle and my parents lived in old White Bear 6th and Johnson.		



**Appendix K - GARE Worksheet**



# Racial Equity Tool Worksheet

## Step #1

### What is your proposal and the desired results and outcomes?

1. Describe the policy, program, practice, or budget decision (for the sake of brevity, we refer to this as a “proposal” in the remainder of these steps)

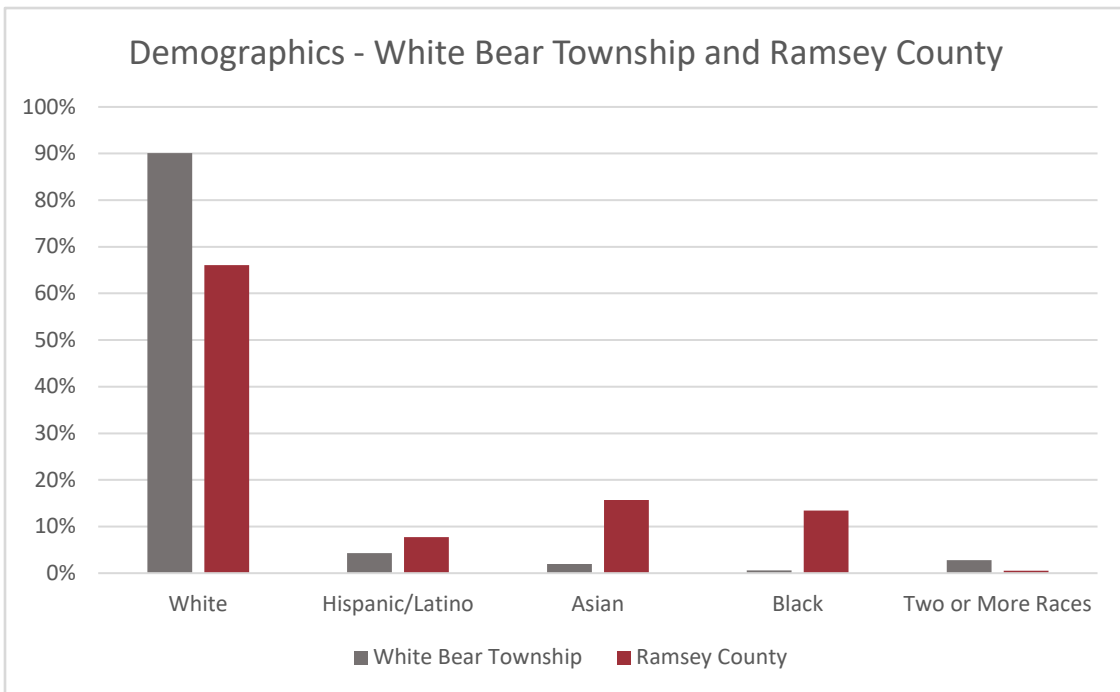
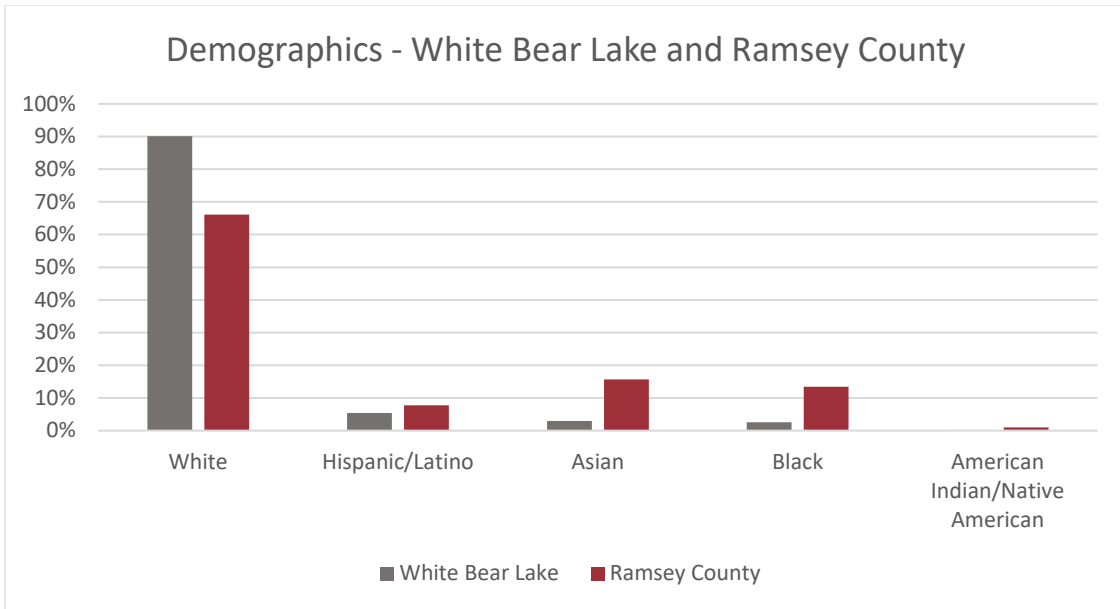
Ramsey County is seeking assistance to develop a critical link of the Bruce Vento Regional Trail to connect pedestrian and bicycle facilities in Ramsey County between Whitaker Street and County Road J. The Bruce Vento Regional Trail is envisioned to be a 13-mile trail from downtown St. Paul to the County boundary in White Bear Township.

2. What are the intended results (in the community) and outcomes (within your own organization)?

This project would complete a major gap in National US Bike Route 41 (USBR 41) by furthering the design process for the final 3.5 miles of the corridor. The approach to engagement builds on Ramsey County’s goal established by the Board of Commissioners to cultivate economic prosperity through *“inclusive initiatives that engage all communities in decisions about our future”*.

Engagement for the Bruce Vento Regional Trail preliminary design was conducted broadly to connect with a wide variety of stakeholders, including people of color, people with low incomes, other commonly underrepresented residents like youth and the elderly, as well as local business owners. About 10 percent of White Bear Lake residents are people of color, with 5.4 percent identifying as Hispanic or Latino, 2.6 percent identifying as Black/African American and nearly 3 percent identifying as Asian. In addition, 6.3 percent of households have incomes under the federal poverty line.





The project team provided accessible engagement opportunities for these groups to identify their existing needs and understand the potential benefits and impacts of the proposed trail alignments and future trail connections (I.e., in-person meetings, online meetings, pop-up events, online surveys, online comment map, project videos and meeting materials available online). **Given the diversity of residents and the underrepresented communities along the route, the project team provided information in ways that are approachable to multiple audiences.**

Engaging community members was an important part of the planning process. The project ensured an accessible opportunity to participate at each phase of the County’s



robust public engagement efforts and integrated the input from multiple agencies and community perspectives into the design. **Through conversations with stakeholders and the public, we can better understand the needs of the community and respond with creative, constructible solutions that will help to ensure success for the Bruce Vento Regional Trail.**

The project team is using the following engagement methods.

- Virtual Open House (Lunch and Evening)
- In-Person Open House – Fall 2021
- In-Person Open House – Spring 2022
- Surveys (2)
- Wikimap
- Project Video

These methods were developed in part to ensure people could have their voice heard even during COVID-19 restrictions, while also providing opportunities to meet people in the community, educate, receive comments, and talk about the project.

Successful engagement helped ensure that regional trail improvements provide high quality access to healthy recreation and transportation options for all people, all ages and all abilities.

A successful project will also deliver improved access and accessible design to the community. These improvements help ensure all residents have access to active recreation and transportation options both on the trail and on all the trails to which it connects.

### **3. What does this proposal have an ability to impact? (bold all that apply)**

**Children and youth**

**Community engagement**

Contracting equity

Criminal justice

**Economic development**

**Education**

**Environment**

Food access and affordability

Government practices

**Health**

**Housing**

Human services

**Jobs**

**Parks and recreation**

**Planning / development**

**Transportation**

**Utilities**

Workforce equity

Other:



## Step #2

### What's the data? What does the data tell us?

- 1. Will the proposal have impacts in specific geographic areas (neighborhoods, areas, or regions)? What are the racial demographics of those living in the area?**

The proposal is specific to White Bear Lake and White Bear Township, particularly the approximately 4 miles between Whitaker Street and County Road J. Residents in this area will experience increased access to healthy recreation and transportation opportunities with the addition of the trail. In addition, residents from other areas will be able to use the trail to connect to amenities and destinations in White Bear Lake and beyond. The [demographic composition](#) of White Bear Lake is 90.63% White/Caucasian, 2.73% Asian/Asian American /Pacific Islander, and 2.59% Black or African American. Additionally, 86.7% of the White Bear Lake population is White/Caucasian alone, and 5.5% [also/instead] identify as Hispanic or Latinx. In White Bear Lake, 20.2% of residents are under the age of 18, and 20.9 percent of residents are Seniors over the age of 65.

- 2. What does population level data, including quantitative and qualitative data, tell you about existing racial inequities? What does it tell you about root causes or factors influencing racial inequities?**

Population data indicates that White Bear Lake and White Bear Township are mostly white communities. However, over time these communities are becoming increasingly diverse as more Asian, African American, and Latino households move to White Bear Lake and White Bear Township. Beyond the specific project extent within White Bear Lake, the broader communities that are served by this trail connection are diverse. To the south in Maplewood and East St Paul, there are large populations of Latino, African-American, Karen and Hmong residents. To the north in Hugo and Lino Lakes, approximately 5 percent of the population is made up of people of color.

Community	2010	2020
White Bear Lake	93.25% -2010 Census	87.28% - 2020 Census
White Bear Township	88.15% -2010 Census	81.12% - 2020 Census

This stratification of demographics from one side of White Bear Lake to another is reflective of larger geographic stratifications trends across race and ethnicity in Ramsey County and the Twin Cities as a whole. It shows how communities can be disconnected and segregated on racial and economic lines. Creating more connections across these stratified communities is a small step that can help address those geographic inequities.

- 3. What performance level data do you have available for your proposal? This should include data associated with existing programs or policies.**



According to Ramsey County's 2018 Walking and Biking use report, about 1.07% of White Bear Lake residents use walking as their mode of transportation, and less than .05% of residents use a bike as their primary mode of travel. The report also notes that White Bear Lake performs better than some of its other suburban peers in having an integrated pedestrian network: "White Bear Lake offers a model for smaller communities within Ramsey County, an integrated network of sidewalks in the downtown, supporting walking and short trips within a highly walkable defined area."

**4. Are there data gaps? What additional data would be helpful in analyzing the proposal? If so, how can you obtain better data?**

There are demographic/population data gaps for those who currently utilize the Bruce Vento Regional Trail in White Bear Lake and White Bear Township. It would be helpful to get an understanding of the types of pedestrians who use the trail and for what purposes (ex: families for recreation, commuters for work, students for school, etc.). It is important to understand how the public will interact with new/extended transportation facilities. The project attempted to address data gaps by collecting information from project participants by adding a question to the community survey asking what purpose(s) the respondent uses the trail.

## Step #3

### How have communities been engaged? Are there opportunities to expand engagement?

**1. Who are the most affected community members who are concerned with or have experience related to this proposal? How have you involved these community members in the development of this proposal?**

Neighborhood residents, White Bear Lake businesses, White Bear Township businesses, bicyclists, youth/parents, elected officials, school officials. These groups have been engaged through Open House meetings, project promotion, pop-up, mailers, community survey, interactive community comment map, virtual meeting, email outreach, website updates, project videos, as well as opportunities for translated materials.

**2. What has your engagement process told you about the burdens or benefits for different groups?**

Some of the burdens of the project include:  
Potential loss of trees/vegetation  
Railroad crossing and safety concerns



Concern about project cost  
Potential loss of property  
Potential one-way conversion  
school commute concerns

Benefits include:

Safety improvements for bicyclists/pedestrians  
Accessibility to schools and businesses via biking and walking  
More connected regional trail system

**3. What has your engagement process told you about the factors that produce or perpetuate racial inequity related to this proposal?**

Avoiding impacts to neighborhood comes as a cost of lack of access to trail users. While the demographics suggest a low percentage of racial minorities in the area, limited representation of these groups in the process could lead to results that reduce access to schools and businesses for low income and minority populations.

## Step #4

### What are your strategies for advancing racial equity?

**1. Given what you have learned from research and stakeholder involvement, how will the proposal increase or decrease racial equity? Who would benefit from or be burdened by your proposal?**

Racial equity improves with safe and accessible trail access, opportunities to travel from downtown St. Paul to County Line, connections to transit, and better pedestrian connections to education centers and community spaces. Most of the direct burdens heard from stakeholder engagement are impacts on adjacent property owners, resistance to change (i.e. loss of privacy) and changes in vehicle and pedestrian traffic patterns in the neighborhoods.

**2. What are potential unintended consequences? What are the ways in which your proposal could be modified to enhance positive impacts or reduce negative impacts?**

Potential unintended consequences identified in the process include possible changes in routing school commuter traffic, lack of connections to downtown White Bear Lake, environmental impacts to Bald Eagle Lake, a one-way conversion that has the potential to lead to a jurisdictional transfer of the roadway, loss of parking, and changes in school access and vehicle access to/from the school campus.

The addition of a trail has the potential to improve health and physical fitness of youth, seniors, and adults as well by improving safe and accessible trail access for all. Improved



pedestrian connections to regional amenities, including parks, transit stops and local businesses. Wayfinding and signage can help to bring more patronage to downtown White Bear Lake businesses. Significant improvements to bicycle and pedestrian safety by providing dedicated space outside of the roadway. Opportunity to improve pedestrian safety at school intersections.

**3. Are there complementary strategies that you can implement? What are ways in which existing partnerships could be strengthened to maximize impact in the community? How will you partner with stakeholders for long-term positive change?**

This phase of planning identified multiple ways to enhance positive impacts and reduce negative unintended consequences through additional stakeholder coordination, community, broader community education and further emphasis on vehicle and pedestrian safety. Specific opportunities identified during the process that can lead to long-term positive change include additional school planning, efforts to achieve municipal consent, outreach to build community buy-in, emphasis on neighborhood improvements (i.e. safety, wayfinding, traffic volumes, visualizations/renderings), address Bald Eagle Lake impacts and demonstrate potential improvements, partnership and connections to the Lakes Link Trail Project.

**4. Are the impacts aligned with your community outcomes defined in Step #1?**

Yes, through conversations with stakeholders and the public, we better understand concerns and opportunities identified by the community and worked to respond with creative, constructible solutions that will help to ensure success for the Bruce Vento Regional Trail.

This project would complete a major gap in National US Bike Route 41 (USBR 41) by furthering the design process for the final 3.5 miles of the corridor. A successful project will also deliver improved access and accessible design to the community. These improvements help ensure all residents have access to active recreation and transportation options both on the trail and on all the trails to which it connects.

## Step #5

### What is your plan for implementation?

**1. Describe your plan for implementation.**

The project encompassed preliminary design of a Phase 2 trail extension. The preferred route for the regional trail corridor following Bald Eagle Avenue, Bald Eagle Boulevard, Taylor Avenue and Hugo Road to County Road J was determined by the feasibility study and master plan amendment that preceded this project. Additional design and public engagement will continue through the design development and final design phases of



the project. Further engagement with local agencies will also be needed to help secure funding for implementation and construction.

## **2. Is your plan:**

### **Realistic?**

The plan for implementation is TBD but is several years away as funding has yet to be secured for construction. Community engagement will continue throughout design development and final design and community support is an important contributor to successful funding applications.

### **Adequately funded?**

Funding was adequately provided for the initial phase 2 design phase. The County received the \$1.3 million federal grant to advance preliminary and final design, further engagement with stakeholders and WBL and WBT community. Funding for construction has not been secured.

### **Adequately resourced with personnel?**

Yes, a Project Management Team lead by Ramsey County Parks & Recreation includes a variety of key local stakeholders. Continued collaboration amongst Ramsey County staff and agency partners (White Bear Lake, White Bear Township, Watersheds and the school district will be crucial to the continued advancement of the project.

### **Adequately resources with mechanisms to ensure successful implementation and enforcement?**

N/A

### **Adequately resourced to ensure on-going data collection, public reporting, and community engagement?**

Yes, future trail design will include additional engagement with stakeholders and community members. All engagement materials used to facilitate preliminary are available for review on the County website.

If the answer to any of these questions is no, what resources or actions are needed?



## Step #6

### How will you ensure accountability, communicate, and evaluate results?

- 1. How will impacts be documented and evaluated? Are you achieving the anticipated outcomes? Are you having impact in the community?**

The engagement process has been documented from start to finish with summaries and meeting materials available on the project website. Anticipated outcomes have been achieved through a transparent public input process, including surveys and summary of engagement, public notices, and promotional strategies such as mailers, meeting materials online, options for translation, presentations to City Council, Project Management Team, walking tours and one-on-one stakeholder conversations, timely response to public comments as well as a project video and public facing engagement summaries.

- 2. What are your messages and communication strategies that are will help advance racial equity?**

The project worked to present project information in plain language and accessible formats to build community awareness and gather meaningful community input. This includes the use of simple language, iconography, visuals, and project videos to share information with community members in a variety of ways. As the project advances it will be important to continue using plain language messaging as well as to promote engagement events in a variety of ways to reach a broad audience. Additionally engagement and outreach efforts should work to meet people in the places they live, work and recreate in order to more effectively reach audiences who may not typically participate in traditional planning processes.

- 3. How will you continue to partner and deepen relationships with communities to make sure your work to advance racial equity is working and sustainable for the long-haul?**

Ramsey County Parks & Recreation will continue to engage with residents, stakeholders, and elected officials as the project advances to more detailed design. All Phase 2 meeting materials, including project videos, will remain available on the project website.



**Appendix L - Bruce Vento Regional Trail Planning Timeline**



## **Bruce Vento Regional Trail Corridor History**

Below is a high-level summary regarding the history and development.

### Corridor Overview:

The Bruce Vento Regional Trail corridor is thirteen miles in length and extends from the east side of downtown Saint Paul to the north county line in White Bear Township. The southern seven-mile segment of the regional trail was completed in 2005 from downtown Saint Paul to Buerkle Road in White Bear Lake on the former Burlington Northern Santa Fe (BNSF) railway.

### Original Master Plan:

- In 1992, the Ramsey County Regional Rail Authority purchased abandoned BNSF right-of-way from downtown St. Paul to Buerkle Road.
- The Bruce Vento Regional Trail was originally established in 1993 as a 13-mile regional trail corridor from downtown Saint Paul to County Road J.
  - St. Paul section: Kellogg Ave to Larpenteur
  - Ramsey County section: Larpenteur to County Road J.
- The 1993 plan proposed the location of regional trail on railway right-of-way since the southern seven miles was abandoned and there were plans to abandon the remaining 6-miles in the future.
- The 1993 plan also identified a need for additional planning activities for the remaining regional trail corridor north of Buerkle Road.

### Development from Kellogg Ave to Buerkle Road:

- Through the 1990's, portions of the regional trail were developed from Kellogg Avenue to Beam Avenue, except the Phalen Blvd section.
- By 2005, the Phalen Blvd segment and the section from Beam Ave to Buerkle Road was completed creating the contiguous southern 7-mile trail section.

### North Six-mile section (Buerkle Road to County Road J):

- The remaining six-mile segment has remained undeveloped due to active railway use.
- For planning and development activities, Ramsey County split the remaining six miles up into two phases.
  - Phase 1 Section: Buerkle Road to Highway 96
  - Phase 2 Section: Highway 96 to County Road J.
- In 2013, Ramsey County received funding to complete a design study for the Phase 1 section to establish an alternate trail corridor from Buerkle Road to Highway 96.
- From 2014-2016, Ramsey County completed a preliminary design study to establish a corridor out of railway right-of way, develop schematic concept plans, review potential impacts, development cost estimates and engage the community.
- In 2018, Ramsey County refined the Phase 1 and Phase 2 sections based on constructability and alignment with the Rush Line BRT project (Purple Line).
  - Phase 1 section: Buerkle Road to intersection of Hoffman Road/U.S. Highway 61.
  - Phase 2 section: Whitaker Street to County Road J.



- The small section between Hoffman and Whitaker Street was incorporated with the Rush Line BRT project for proposed construction along Highway 61 with Rush Line improvements.
- In 2018, Ramsey County received additional funding to continue design and engagement with the community and launched three additional projects.
  - Complete preliminary and final design plans for the Phase 1 section.
  - Initiate a feasibility study for the Phase 2 section between Whitaker and County Road J.
  - Initiate a master plan amendment for the Ramsey County section of the Bruce Vento Regional Trail between Larpenteur to County Road J.
- In 2021, Ramsey County initiated the Phase 2 Design Study project between Whitaker and County Road J to further study the preferred corridor, develop schematic concept plans, review potential impacts, development cost estimates and engage the community.

#### Feasibility Study (Whitaker to County Road J) (2018-2020):

- Ramsey County completed an exhaustive evaluation of potential routes between 2018 and 2020 through White Bear Lake (WBL) and White Bear Township (WBT). Agency staff included in the feasibility study analysis consisted of Ramsey County Parks and Recreation, RC Public Works, WBL and WBT.
- Purpose of the Feasibility Study was to evaluate an alternate corridor for Phase 2 (Whitaker Street to County Road J) including a public engagement process.
  - 2018 – initiate feasibility study with agency partners for review of corridors through WBL and WBT.
  - 2018 Open House – October 4th and 25th to solicit feedback regarding potential corridors.
  - 2019 – Re-evaluate WBL downtown again including Long Avenue as an outcome from 2018 meetings including extensive engagement with railway and agency partners.
  - 2019 Open House – Present 3 options for the preferred corridor alignment. Direct mailings was sent to individuals within the project corridor at ¼-mile buffer.
  - 2020: Preferred trail alignment for Phase 2 of the Bruce Vento Regional Trail determined early 2020 as a result of feedback received from previous engagements.
  - Preferred corridor from Whitaker to County Road J (Lincoln, Bald Eagle Ave, Bald Eagle Blvd, Taylor, Hugo Road)

#### Master Plan Amendment (2019 – 2021):

Ramsey County coordinated planning efforts for long-term Bruce Vento Regional Trail improvements through a public, stakeholder, and agency involvement process. There were three key elements for which Ramsey County and others sought public input to determine the future of Bruce Vento Trail:

- Phase 1 Section: Document outcomes for the preliminary design study, design features and re-alignment of trail from Buerkle Road to Hoffman Road/U.S. Highway 61.
- Phase 2 Section: Document outcomes for the feasibility study, design features and re-alignment of trail from Whitaker Street to County Road J.



- Purple Line (Rush Line BRT): Document outcomes for integration of the regional trail and Purple Line components between Larpenteur and Buerkle Road including design features and amenities, trail design, connections to BRT station stops, and visual quality.
  
- Master plan amendment for the Bruce Vento Regional Trail was initiated in mid-2019 and the feasibility study provided the basis for the master plan amendment.
  - 2020: Ramsey County prepares a Draft master Plan Amendment for long-term improvements to the Bruce Vento Regional Trail
    - August 2020 - 30-day Public Review
    - Letters of Support – Gem Lake, White Bear Lake, White Bear Township
  - 2021 (January) – Recommendation for approval by the Ramsey County Park & Rec Commission
  - 2021 (May): Draft Amendment approved by Ramsey County Board of Commissioners



## **Feasibility Study - Route Analysis and Elimination:**

An evaluation of potential routes through White Bear Lake and White Bear Township occurred between 2018 and 2020. Agency staff included in the feasibility study consisted of Ramsey County Parks & Recreation and Public Works, the City of White Bear Lake and White Bear Township.

Results of engagement activities completed in 2018 included a request by the public to re-evaluate downtown White Bear Lake for the use of the original route identified in the 1993 plan. Re-evaluation included areas adjacent to or near the railway corridor, east and west sides of Highway 61 and Long Avenue. The study also evaluated additional routes east of Highway 61 through the east downtown section of White Bear Lake and the potential use of the Lake Avenue Trail (Lake Links Regional Trail), Clark Avenue, Stewart Avenue, Pacific Avenue, Buffalo Street, Northwest Avenue, Portland Avenue, and Highway 61 north of Highway 96. After significant evaluation, it was determined that these corridors were not viable options for the following reasons.

Downtown White Bear Lake (Near or adjacent to the railway, west and east side of Highway 61, Clark Avenue):

- Burlington Northern Santa Fe Railway would not support the route adjacent to the railway tracks due to spacing requirements from an active railway and impact to railway property.
- Lack of available right-of-way space throughout the downtown area. Significant redevelopment of existing buildings, parking lots, public roadways and other public facilities would be required to allow space for the trail.
- Greater impact on public infrastructure and adjacent private property.
- Impact on existing building infrastructure adjacent to the railway.
- Lack of connectivity to Long Avenue.
- High amount of turns north of Fourth Street to Long Avenue.
- Greater impact on existing buildings, public infrastructure and adjacent private property along Clark Avenue. Additional signal crossings would be required on Highway 61 at Seventh or Eighth Street.
- Lack of right-of-way space, harsh trail environment and ability to provide proper trail maintenance adjacent to Highway 61 (east and west side) and significant impact to adjacent properties, public infrastructure and building infrastructure.

Routes West of Highway 61:

- Routes 2 and 3 scored the lowest in the route survey at the November 19, 2019, community meeting.
- Lack of available right-of-way space along routes 2 and 3.
- Routes 2 and 3 do not provide high-quality trail use and atmosphere for a regional trail.
- Routes 2 and 3 do not fit into the overall regional trail context for planning alignment routes.
- Numerous turns for Route 2 and 3 through established urban areas.
- Greater impact with existing road-right-of-way and public infrastructure on Routes 2 and 3.



- Greater impact on existing mature canopy trees on Routes 2 and 3 do.
- Route 2 does not work with the proposed White Bear Lake High School development. It would create back turns to get to Long Avenue.

Routes East of Highway 61 (Lake Avenue, Stewart Avenue, Highway 96, Pacific Avenue, Northwest Avenue, Portland Avenue, Buffalo Street and Highway 61 north of Highway 96):

- Does not provide high-quality trail use and atmosphere for a regional trail and does not fit into the overall regional trail context for planning alignment routes.
- High amount of turns in urban areas.
- Greater impact with existing road right-of-way, public infrastructure, and adjacent private property.
- Greater impact on existing canopy trees.
- Additional acquisition or easements.
- Too many crossing points with Highway 61.
- Lake Avenue already has many trail users.
- Adding more to the Lake Avenue Trail (Lake Links Regional Trail) would create too much congestion.
- Utilizing the Lake Avenue Trail (Lakes Link Regional Trail) does not meet regional trail separation distance requirements.
- Due to significant wetlands, extensive boardwalk structures would be required north of Pacific Avenue.
- Lack of trail connectivity and available right-of-way on Highway 96.
- Creation of trail back turns for routing to crossing points on Highway 61.
- Harsh trail environment on Highway 61, north of Highway 61 to Buffalo Street and County Road J.
- Greater impact to existing right-of-way, wetlands, drainage ways, mature canopy trees and adjacent private property along Portland Avenue, Buffalo Street, and Northwest Avenue.

The evaluation analyzed design intent, usability and effectualness for both bikers and walkers, impacts, connections to downtown and trail user experience. Results of the feasibility study were incorporated into the master plan amendment and the county received letters of support/resolutions from White Bear Lake and White Bear Township in late 2020. The master plan amendment was approved by the Ramsey County Board of Commissioners in May 2021 and by the Metropolitan Council in August 2021.



## SEGMENT 4: FUTURE NORTHERN EXTENSION SEGMENT

After the Rush Line BRT realignment at Whitaker Street, the Bruce Vento Trail will travel north through White Bear Lake and White Bear Township to the Ramsey County/Washington County line at County Road J. Because the original trail alignment considered sharing the right-of-way with active railroad service, two feasibility studies for the northern extension starting at Highway 96 were conducted by Ramsey County, the first in 2016 and the second in 2018. The goal of the studies were to determine an alternative trail alignment outside of railway right-of-way. Initially there were multiple alignments under consideration determined by the first feasibility study, which are shown in Figure 9.

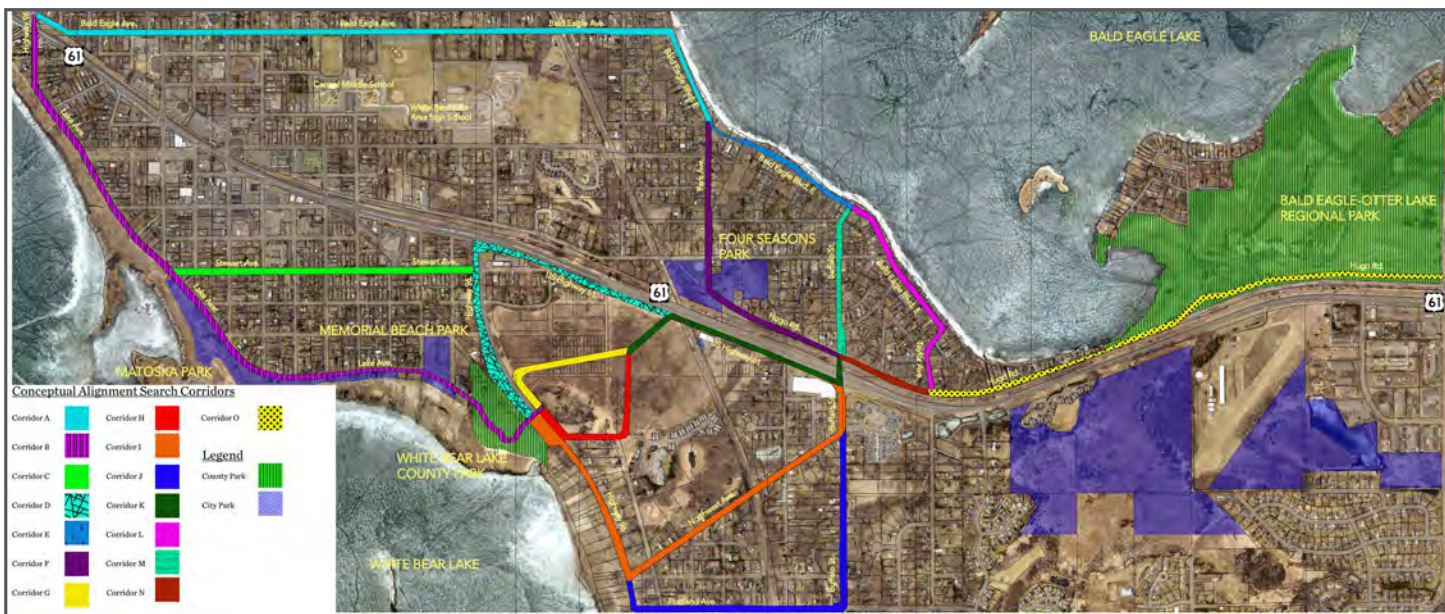


Figure 9. Northern Extension Potential Alignments from the 2017 Study

Based on public engagement comments and coordination with other agencies, the multiple alignments shown in the figure above were narrowed down to three main options for the Bruce Vento Trail in the later feasibility study. They are shown in Figure 10, with justification for the selection of Route 1 as the preferred route below:

- Route 1: Route 1 was identified as the preferred route through the downtown area for connection to Hugo Road. The route would extend along Bald Eagle Avenue to Bald Eagle Boulevard to either Taylor Avenue or Buffalo Street and make connection to Hugo Road to reach County Road J. Buffalo Street could be considered as alternative trail corridors for connection onto Hugo Road depending one-way consideration of Bald Eagle Boulevard. In the event Bald Eagle Boulevard is not feasible, an alternate sub route utilizing White Bear Schools property, Division Avenue, and Park Avenue can be utilized to connect the trail from Bald Eagle Avenue to Hugo Road.
  - Scored the highest in the routes survey at the November 19, 2019 community meeting.
  - Provided the most appealing trail alignment and atmosphere for a regional trail.
  - Less turns.
  - Good connections to the High School.
  - High quality viewing to Bald Eagle Lake.
  - Potential partnership/coordination with the White Bear Lake and Ramsey County Public Works for railway crossing improvements.
  - White Bear Lake desire to incorporate trail connections from Bald Eagle Avenue to downtown area.
  - Sections of Bald Eagle Boulevard should be reviewed for possible 1-way road configuration east of Bald Eagle Avenue.



- Could provide long-term trail loops between Bruce Vento Trail and Lakes Link Regional Trail. This would require partnership for potential city connections to create this loop.
- Route 2 and 3: Both routes were not favored due to the amount of turns through the residential areas for connection into either Division Avenue or Long Avenue. It was anticipated this route may be confusing to users of the regional trail corridor.
  - Scored the lowest at the November 19, 2019 community meeting.
  - Does not provide high quality trail use and atmosphere for a regional trail.
  - Does not fit into overall regional trail context for planning alignment routes.
  - High amount of turns.
  - Get you closer to downtown area.
  - Potential impact with existing road right-of way and public infrastructure.

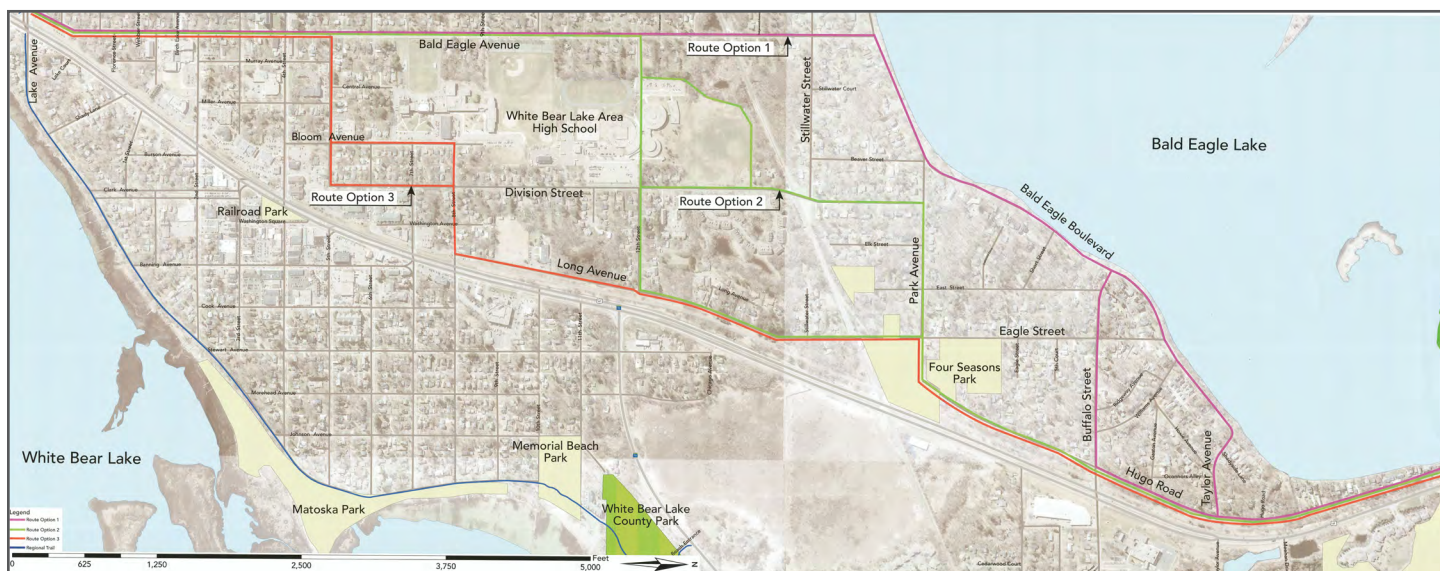


Figure 10. Three Route Options from the 2019 Community Meeting

Section 2.3 provides more detail on the specific plan for this segment, based on the findings of the feasibility study. Route 1 is the preferred route that is shown but may be altered in the future due to continued studies in downtown White Bear Lake for potential long-term development of the school, Rush Line BRT project, and surrounding business development on Long Avenue. Investigations are anticipated to be completed over the summer of 2020 which will determine the impact of these studies on the Bruce Vento Trail northern extension alignment and provide a basis for an alternative to the preferred route or a reevaluation of the proposed alignment.

A map showing existing conditions in the northern extension feasibility study area and the preferred alignment is shown in Figure 11, with existing conditions images following.





Figure 11. Segment 4, Whitaker Street to County Road J





1 Cul-de-sac at the southern end of Bald Eagle Avenue facing the intersection of Highway 61/County 96.



4 No shoulders or separated facilities exist along the narrow stretches of Bald Eagle Boulevard.



2 Parking and pedestrian facilities along Bald Eagle Avenue.



5 Existing conditions of Hugo Road.



3 The shoulder along Bald Eagle Avenue narrows as the road extends north.